

Rocky Creek

CONCEPTUAL SCHEME

Amendment February 2022



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Rocky Creek

CONCEPTUAL SCHEME

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Introduction

The Rocky Creek Conceptual Plan (CS) was prepared on behalf of Melcor Developments Ltd. pursuant to the requirements of the Balzac West Area Structure Plan (ASP). The plan provides an overall development rationale to guide development, and a policy framework to inform ongoing decision making for future land use redesignation and subdivision applications for these lands. This document includes a Development Concept showing proposed land uses, supported by development densities, and development and design elements pertaining to open space, road design, servicing (water and wastewater), and stormwater management. The overall buildout of Rocky Creek is described through the determination of future phasing, and implementation of the Plan.

Ultimately, Rocky Creek will be a complete community, providing residential, commercial and industrial land uses, designed with an ultimate objective of preserving and respecting the natural landscape. This will provide for an extensive open space network with naturalized pathways, resulting in an urban style, but rural feeling community. Rocky Creek will accommodate the best of both worlds – urban servicing, densities and amenities with a rural feel including ample open space, dark skies and larger lots.



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Site Context

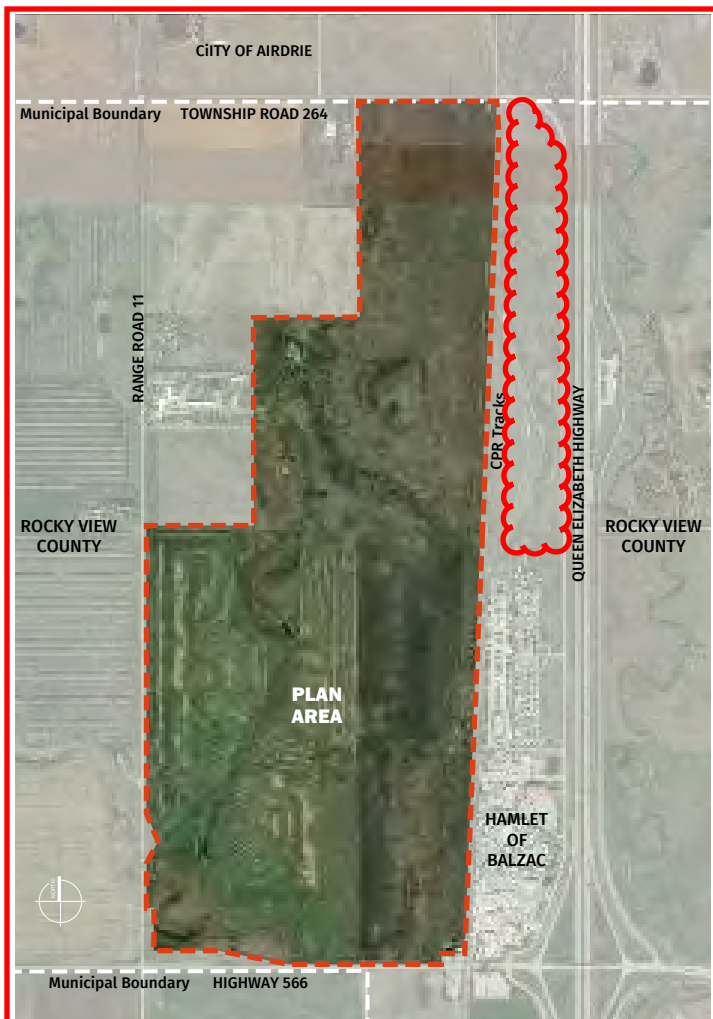


FIGURE 1. Location Plan

Figure amended to include Business Commercial lands

2.1 Plan Location & Ownership

The Plan area is located within Rocky View County (RVC), approximately 1 km south of the municipal boundary of the City of Airdrie and abuts the municipal boundary of the City of Calgary to the south. The Plan area is situated adjacent to the historic Hamlet of Balzac and is owned by Melcor Developments Ltd. Ownership of the subject site and surrounding lands is outlined in **Figure 2**.

The legal description of the Plan area includes:

- Plan 0311824; Block 5; Lot 1
- Portion of the lands known as NE 24;26;1 W5M and SE 24;26;1 W5M
- Lands within portions of 5;1;26;24; NE, SE and SW quarter sections

These lands combined form the Plan area, comprising approximately ~~259~~ ²⁸⁷ hectares (~~639~~ ⁷⁰⁹ acres).

The subject lands are bounded by Queen Elizabeth II Highway to the east, Highway 566 to the south, Range Road 11 to the west, and Township Road 264 to the north. The site is directly adjacent to the Canadian Pacific Railway (CPR) right-of-way that runs north/south, along the east side of the Plan area.



Figure amended to include Business Commercial lands



FIGURE 3. Context Plan

Figure amended to include Business Commercial lands

2.2 History of Balzac

1920-1929

The history of the Hamlet of Balzac dates back to 1910 when a Canadian Pacific Railway (CPR) train station began operating in the area. The name, named after a favorite French novelist, Honoré de Balzac, was established by the president of CPR at the time, William Cornelius Van Horne.

- 1910: CPR train station begins operating along the tracks in the area
- 1920: the Balzac Trading Company is established by a group of local farmers and loading platform is developed along the CP rail
- 1925: Grain elevator is built
- 1928: Development of the Balzac Community Hall
- 1929: Location of the Alberta Wheat Pool in Balzac

2000

Since the 2000's, the greater area of Balzac has seen a significant amount of commercial and industrial development with a number of business parks being developed over the years, including:

- The Nose Creek Business Park
- The Hopewell Business Park, and;
- Industrial developments in the Wagon Wheel Business Park and High Plains Industrial Park.

Below is a list of significant developments within these business parks that have been built in the past 10 years, and future development, that contribute to this area's identity as a significant regional employment area;

2009

- CrossIron Mills is built, the first enclosed mall to be built in Alberta in over 20 years

2010

- The development of the East Balzac Water Treatment Plant and Reservoir 20km east of Balzac (operational in 2011, servicing the area)
- Walmart Food Distribution Centre – one of Canadas largest refrigerated buildings
- Costco Wholesale – retail shopping centre

2016

- Sobeys Distribution Centre

2018

- Amazon Fulfillment Centre – processing orders from Western Canada
- New Horizon Mall – indoor marketplace operated by local, small businesses

2021

- Lowes Distribution Centre

2.3 Existing & Surrounding Land Use

The Rocky Creek lands are currently designated a combination of Direct Control - 80 (DC80) and Agricultural General (A-GEN). The intent of the DC80 land use was for the development of a community comprised of residential, commercial, an assisted living facility and recreation amenities including a golf course. This was based on the previously submitted Rocky Creek Conceptual Scheme adopted in 2002. Development of that plan was never undertaken.

The surrounding land uses are a combination of the following, as shown in **Figure 4**.

- **East of Plan Area**
 - B-REC (Business, Recreation District)
 - C-LRDs (Commercial, Local Rural District)
 - C-LUD (Commercial, Local Urban District)
 - DC118 (C-PT: Point Commercial)
 - DC117 (I-IS: Storage and Sales Industrial)
 - DC72 (B-HF: Highway Frontage Business)
 - DC150
 - I-LHT (Industrial, Light District)
 - S-PUB (Special, Public Service District)
- **West of Plan Area**
 - Agriculture, Small Parcel District (A-SML)
 - Agricultural General (A-GEN)
 - R-RUR (Residential, Rural District)



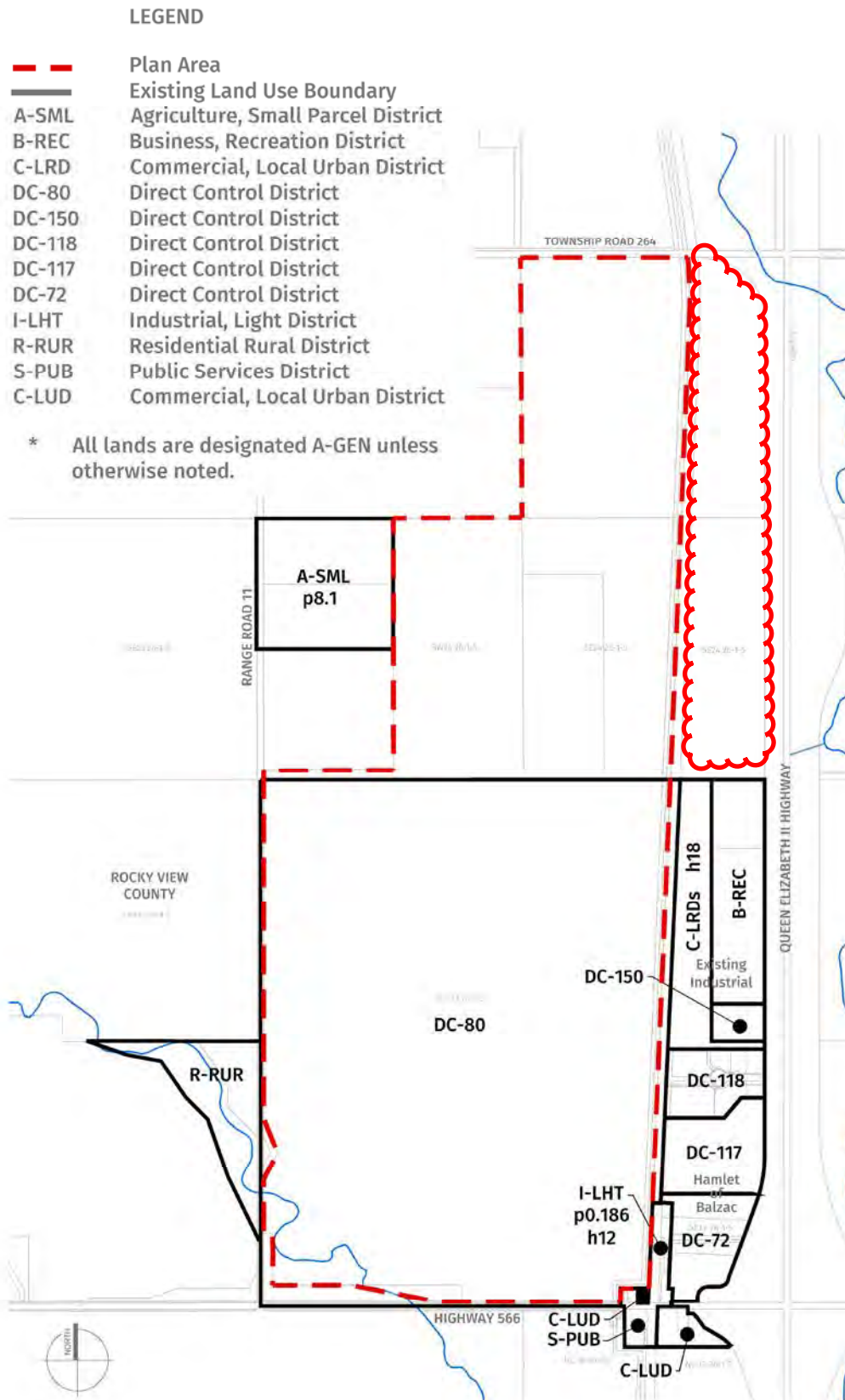


FIGURE 4. Existing Land Uses

Figure amended to include Business Commercial lands

2.4 Site Conditions

The lands are bordered by 3 major roadways: Highway 566 to the south, Range Road 11 to the west and Range Road 264 to the north. The eastern most portion of the site is intersected by the Canadian Pacific Railway right of way, with the Queen Elizabeth Highway II (QEII) running parallel to the east. There are currently only agricultural access points into the subject lands from the bordering roadways.

The site is comprised of rolling topography, the highest point in the north eastern portion of the plan area, providing clear, 360-degree views across the highways and towards the mountains to the west. These high points are adjacent to a natural drainage course that runs north/south, providing a natural corridor throughout the site. These natural areas will be preserved and enhanced to provide for an extensive open space network with opportunities for public amenity spaces connected by a pedestrian network.

The Plan area is located within the Grassland Natural Region (Foothills Fescue Subregion) where native vegetation of this natural region is associated with the wetlands, watercourses and riparian areas. Much of the upland native species have been cultivated. There are several natural connected wetlands, drainages and an

unnamed Crown claimed watercourse in the southwest corner of the Plan Area. The watercourse flows southeast and is a tributary to Nose Creek. There are currently no existing buildings or structures that are situated on the lands.

The lands to the west are characterised by agriculture uses, with the lands to the east characterized by primarily industrial uses capitalizing on the proximity to the highway. The Plan area is located directly west of the existing Hamlet of Balzac, comprised of:

- The Balzac Community Hall - established in 1928, providing a community gathering space for Balzac and Airdrie residents for over 90 years.
- The Anglican Parish of St. Clement
- The Balzac United Church; and a
- Gas station

The site is approximately 2.5 km away from the regional commercial node, CrossIron Mills, the largest enclosed shopping mall in Alberta. The CrossIron Mills area also includes big box retail sites and industrial uses to the east, making the area a very important employment base and destination for residents and visitors in the region.

The existing conditions are highlighted in **Figure 5**.



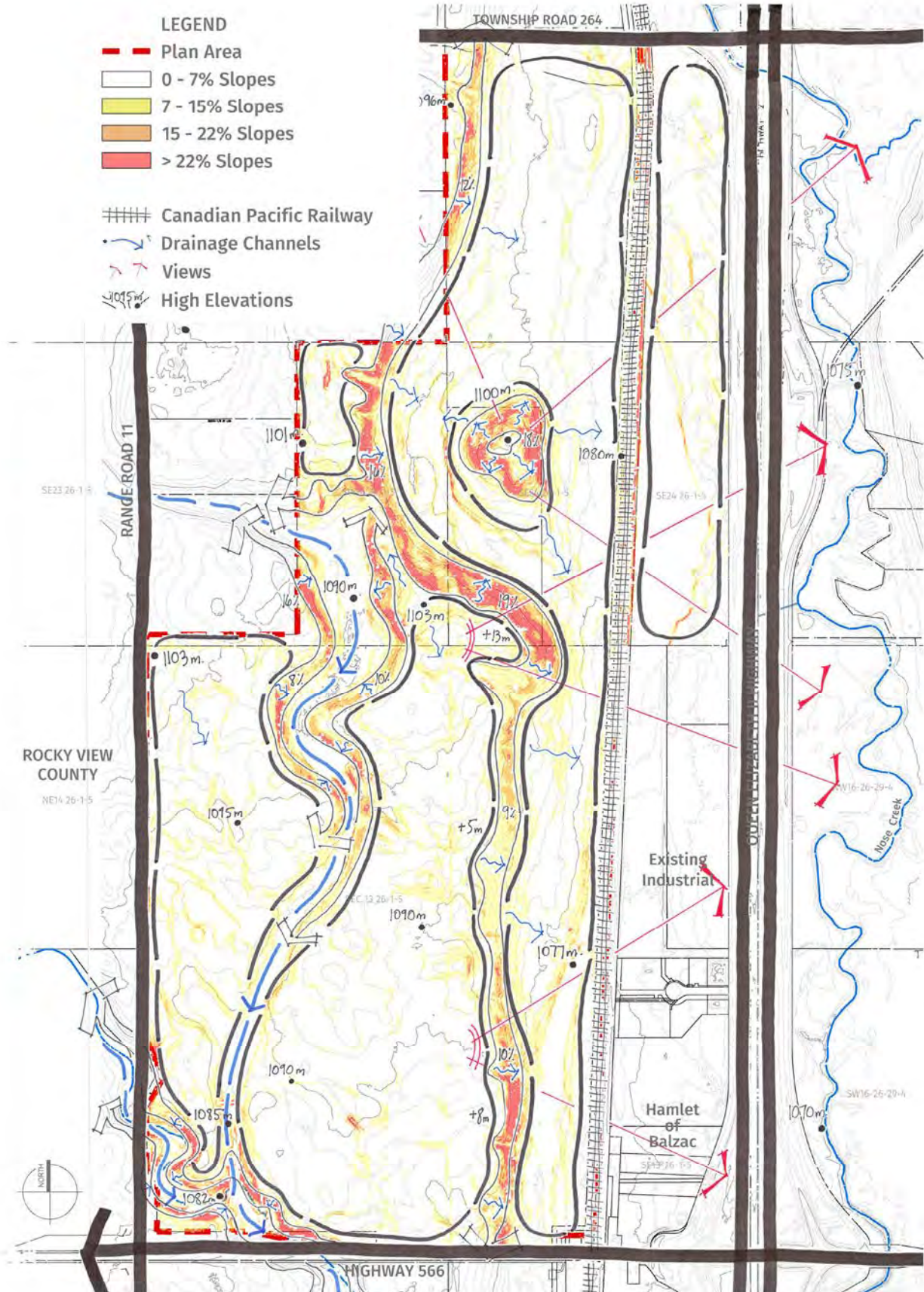


FIGURE 5. Site Conditions

3 Policy Review

3.1 Regional Statutory Plans

The following section outlines the relevant regional planning documents that apply to the Rocky Creek lands within the context of the greater region.

3.1.1 South Saskatchewan Regional Plan

The South Saskatchewan Regional Plan provides a framework for the management of the province of Alberta's land and natural resources by establishing land-use regions and development plans for each area in the region. It is required for statutory plans that fall within these land use regions to comply with the South Saskatchewan Regional Plan.

3.1.2 Calgary Metropolitan Regional Board (CMRB) - Interim Growth Plan

The Calgary Metropolitan Regional Board was established in 2018 and is made up of the Calgary Metropolitan Region's 10 member municipalities, which includes Rocky View County. The Interim Growth Plan (IGP) was developed by the CMRB to guide land-use, growth and infrastructure planning on an interim basis, prior to the approval of the long-term Growth Plan and Servicing Plan that is currently being developed. The Growth Plan and Servicing Plan will align with the policies of the South Saskatchewan Regional Plan. Statutory plans, such as Area Structure Plans, submitted in the CMRB region are subject to approval by the CMRB.

The proposed Rocky Creek Conceptual Scheme aligns with its corresponding Area Structure Plan (ASP), the Balzac West ASP, by providing a land use strategy to guide growth and determine opportunities for residential and business development of a fully sustainable community, including provisions for open space, within the County.

3.1.3 Intermunicipal Development Plans

Each intermunicipal development plan (IDP) that applies to the Rocky Creek lands outlines how these two municipalities, the City of Airdrie and the City of Calgary, will each work together with Rocky View County in the development of adjacent lands of interest. More specifically, these plans indicate how to coordinate the provision of services across borders such as transportation, water, utilities, parks, recreation and open spaces.

The Rocky View County/City of Airdrie Intermunicipal Development Plan (IDP) was adopted in August 2001 and identifies the northern portion of the Rocky Creek plan area as being within the IDP Policy Area. This means that the policies contained in the Airdrie IDP apply to the northern portion of Rocky Creek. The Conceptual Scheme proposes development of future industrial uses within these lands. This aligns with the objectives and relevant policies of the Airdrie IDP by;

- Providing business development opportunities for the region that will diversify the municipality's economic base
- Locating industrial development that compliments surrounding land uses, and including interface design policies to help achieve development with seamless transition between land uses
- Providing supporting technical studies that demonstrate how water and sanitary systems will be handled and provisions for adequate stormwater management for this area; and
- Considering the access points and how this future industrial will interact with provincial and intermunicipal transportation system

The Rocky View County/City of Calgary IDP was adopted in January/February 2012 and identifies the southern half of the Rocky Creek lands, adjacent to the shared border between municipalities, as part of the "Policy Area". This means that policies contained in the Calgary IDP apply to the southern portion of Rocky Creek.

More specifically, the Calgary IDP identified the subject site to be within the Queen Elizabeth II Highway (Queen Elizabeth II Highway) North Corridor, and as such, the Rocky Creek Plan aligns with these policies outlined by;

- Including Interface Design section and policies that encourage certain design measures to create a suitable interface between future development and the Hamlet of Balzac
- Including policy that all development must be circulated to Canada Pacific Railway (CPR) for commenting, to ensure the development does not compromise the function of the railway line
- Preserving the natural Crown claimed unnamed watercourse with a riparian setback aligning with the Nose Creek Watershed Water Management Plan. The unnamed creek is a direct tributary to Nose creek.

The Plan also takes into consideration section 6.0 Interface Planning of the Rocky View County/Calgary IDP and uses it as a framework to include specific policy that requires design considerations to ensure smooth transitions between land uses.

3.2 The County Plan

In October 2013, the County Plan was adopted by Rocky View County, providing an overall vision and framework for development to occur within the County over the long term. This statutory plan shows the Rocky Creek lands and surrounding area (West Balzac) under Residential Communities Hamlet-Full Service, and a future Highway Business Area.

The vision for Hamlets within the County plan is to:

- Support hamlets in maintaining and developing a strong sense of community identity and in carrying on their role as service hubs to the surrounding agricultural regions
- Support hamlets in developing and maintaining attractive, high quality, built environments and distinct, safe residential neighbourhoods

The County Plan includes policy that is supportive of the development of this existing Hamlet of West Balzac. The Hamlet is defined as having a range of land uses, primarily residential development, with variety of housing types, that includes a main street, crossroads, or central gathering area, and may have an associated business park with commercial/industrial uses. The Rocky Creek lands will align with the vision to create a community that is fully serviced with managed growth over time to respect the rural character of the County.

The County Plan also identifies the south eastern portion of the Rocky Creek lands as a Highway Business Area, where the majority of commercial and industrial development should locate within the County. The Rocky Creek plan proposes to focus its industrial uses within this determined Highway Business Area to take advantage of the provincial highway system (i.e., QEII and Highway 566) and provide regional business services and local employment opportunities.

The County Plan is a statutory plan that must be complied with for applications for development within the County. The Balzac West Area Structure Plan (ASP) adopted in 2007 was reviewed for its compliance with the County Plan. By aligning with the Balzac West ASP, the Rocky Creek plan shall also comply with those corresponding vision/policies of the County plan.

3.3 Balzac West Area Structure Plan (ASP)

The Balzac West ASP was adopted by Rocky View County Council in April 2007. This plan outlines the vision, goals and policies for the lands sharing a border with the City of Airdrie to the north and the City of Calgary to the south, in between the QEII to the east and Range Road 13 to the west. Highway 566 runs through the Plan area and connects the existing Hamlet of Balzac to the surrounding road network. The staging plan as outlined in Section 7.1 of the ASP identifies the plan area as requiring a Conceptual Scheme prepared to provide a framework for future development, implemented through land use redesignation and subdivision.

The Rocky Creek Plan area falls within the ASP policy areas; Existing Hamlet of Balzac and Queen Elizabeth II Highway Corridor. Below is an overview of how the Conceptual Scheme addresses the subsequent policies from these policy areas to guide development within the subject lands.



The Queen Elizabeth II Highway Policy Area:

This policy area of the ASP plans for regional commercial, retail employment, office and light industrial business park(s) land uses. The purpose of the lands within this policy area are to capitalize on the physical access and visual exposure to the regional road network, namely, the Queen Elizabeth II Highway (QEII). The northern most portion of the Rocky Creek Plan falls within this policy area and it is proposed that it will be a continuation of approved highway commercial and light industrial corridors, aligning with the intended purposes outlined in the QEII policies of the ASP.

The remainder of the Rocky Creek lands to the north identified as part of the QEII policy area includes the highest point of the Plan area, creating a challenging topography to accommodate the development of commercial and or industrial development. This would necessitate a clearing of the natural landscape which the Rocky Creek Plan seeks to avoid; the overall framework of this Conceptual Scheme is to preserve the natural areas and use them as opportunities to inform design. To address this, the Rocky Creek Plan proposes industrial land uses at the south eastern most portion of the subject lands in which the ASP identifies as the existing Hamlet of Balzac policy area. This area possesses the same qualities as the lands in the QEII corridor in the north by being contiguous with the highway commercial/industrial east of the CPR tracks, and visible and physically accessible by the QEII (by way of Highway 566). The future realignment and interchange proposed at both Highway 566 and

Range Road 264 will improve access to the lands, both north and south, adding to their suitability for industrial development.

In order to meet policies 6.2.9 and 6.2.10 of the QEII policy area regarding the mitigation of environmental and nuisance impacts of commercial/light industrial (such as unsightly areas, outdoor storage, noise, contaminates, odors, lighting), the plan includes interface design policies in **Section 11** that require developers to employ specific design measures to mitigate potential impacts to adjacent residential development. Examples of these measures may include landscaping and architectural elements to enhance the visual appeal along the QEII highway corridor.

3.3.1 Balzac West Area Structure Plan (ASP) Policies

- 3.3.1.1 Subject to further definition of land uses for later phases of the Conceptual Scheme contained within the Queen Elizabeth II Highway policy area of the Balzac West Area Structure Plan, an application to amend the Area Structure Plan may be required to accompany future land use amendments to guide the proposed development within that area.
- 3.3.1.2 Prior to land use redesignation or subdivision, a Conceptual Scheme amendment (or new Conceptual Scheme) shall be prepared to guide the development of the area identified as Future Industrial.

Existing Hamlet of Balzac Policy Area:

For this policy area of the Balzac West ASP, the Rocky Creek Plan meets:

- **Policy 6.4.1** *"The predominant land uses in the Existing Hamlet of Balzac policy area include the Rocky Creek Conceptual Scheme, institutional and community uses, commercial and light industrial business park uses focused on the Queen Elizabeth II Highway, and business uses that require highway access" by:*
 - Including residential land uses, opportunities for local commercial, and an open space network with public amenity spaces and a school site, aligning with the existing Rocky Creek Conceptual Scheme, and placement of predominantly light industrial land use in areas contiguous with the existing industrial to the east, and to take advantage of access to the Queen Elizabeth II Highway
- **Policy 6.4.2** *"Commercial and light industrial business park densities should be determined by the capacity of the regional road network" by:*
 - Ensuring the area dedicated to these types of uses can be accommodated by the existing and planned regional transportation network. Any additional upgrades required will be the responsibility of the developer.
- **Policy 6.4.3** *"Any lands subject to land use redesignation or subdivision within the Existing Hamlet of Balzac policy area shall be required to connect to municipal utility servicing systems (i.e., sanitary sewer and water)" by:*
 - Connecting to the County's water and wastewater services
- **Policy 6.4.4** *"New open space systems within the Existing Hamlet of Balzac policy area are encouraged to incorporate linked linear networks of trails and pathways, connecting the Hamlet with existing or proposed regional open space systems, particularly within the Nose Creek valley." by:*
 - Creating opportunities to connect to a regional open space network, if/when available
- **Policy 6.4.7** *"To mitigate negative visual impacts, consideration should be given to screening of any outdoor storage, display, or unsightly areas on sites within view of the Queen Elizabeth II Highway." by:*
 - Including policy in the Interface Design Section 11 of the Conceptual Scheme to ensure that developers implement adequate screening and buffering of sites with outdoor storage, display, or unsightly areas within view of the Queen Elizabeth II Highway to reduce any negative visual impacts
- **Policy 6.4.8** *"The boundaries of the Existing Hamlet of Balzac may be expanded, provided such Hamlet expansion takes place in a logical, coordinated, and integrated fashion" by:*
 - Providing a phasing plan in Section 14, Figure 24, based on elements such as servicing capacity to demonstrate logical expansion to the north





PUBLIC CONSULTATION

As outlined in Section 5.3 Conceptual Schemes, the Balzac West ASP states that Conceptual Scheme submissions may include public engagement with all directly and indirectly affected landowners within and adjacent to the Plan area.

Over time, discussions have taken place with all landowners in the area and previous open houses have taken place regarding development of the subject lands which have all informed the design and implementation strategy for Rocky Creek.

4 Community Vision



The overarching vision for the community of Rocky Creek is to provide a complete residential community, along with commercial and industrial uses providing local services and employment centres, all carefully positioned to respect and enhance the natural landscape. The objective of the Rocky Creek community design is to preserve and enhance the natural environment and provide an extensive network of naturalized/organic linkages that connect residents within the community, joining community members from park to park. This coupled with an appropriate density provides a rural residential character that creates a welcomed escape from higher intensity urban living.

The vision for the community of Rocky Creek is:

An escape from the traditional urban setting, and getting back into nature, through a fully sustainable community with a rural way of living by:

- Enhancing and preserving the natural environment, prioritizing naturalized pathway, open space, and roadway design
- Complementing and responding to the natural topography, capitalizing on opportunities for residents to have views of the mountains from their homes
- Providing commercial opportunities for provision of local services and industrial areas to create opportunities for local employment and regional economic development

The Rocky Creek Conceptual Scheme incorporates the following elements from the Balzac West ASP vision:

- To create a comprehensively planned, developed and fully serviced community offering a range of housing, employment, retail, community service and recreational opportunities
- To plan for residential densities slightly lower than the urban residential densities developed in the surrounding areas to offer more variety in the urban landscape, and a greater choice of housing options for citizens in the region
- To develop transportation and utility servicing at the highest standards
- To provide public access readily available to the citizens of the Balzac area
- To take advantage of the Plan areas strategic location along Queen Elizabeth II Highway and proximity to surrounding business parks, to foster both community and economic development
- To take advantage of the Plan areas strategic location along Queen Elizabeth II Highway and proximity to surrounding business parks, to foster both community and economic development

Business Commercial

Please refer to Appendix A. for a complete description of the lands east of the Canadian Pacific Railway line. These lands are proposed to support Business Commercial development. Please note, each section in the appendix corresponds to the relevant subsequent section of the Conceptual Scheme. All policies contained within the Conceptual Scheme shall apply, as appropriate, to these lands as described in Appendix A.



5 Development Concept

The Rocky Creek community will include residential, local commercial, industrial, and open spaces connected by a pathway network. The concept is a result of the Plan's community vision, site location and characteristics, regional context and guiding policy documents such as the Balzac West ASP. Natural features on the landscape formed a basis for the development of the community concept. The rolling topography was preserved and capitalized upon to create opportunities for views of the mountains and creation of a variety of open spaces. These open spaces also act as separation from land uses/built form to help achieve a seamless transition.

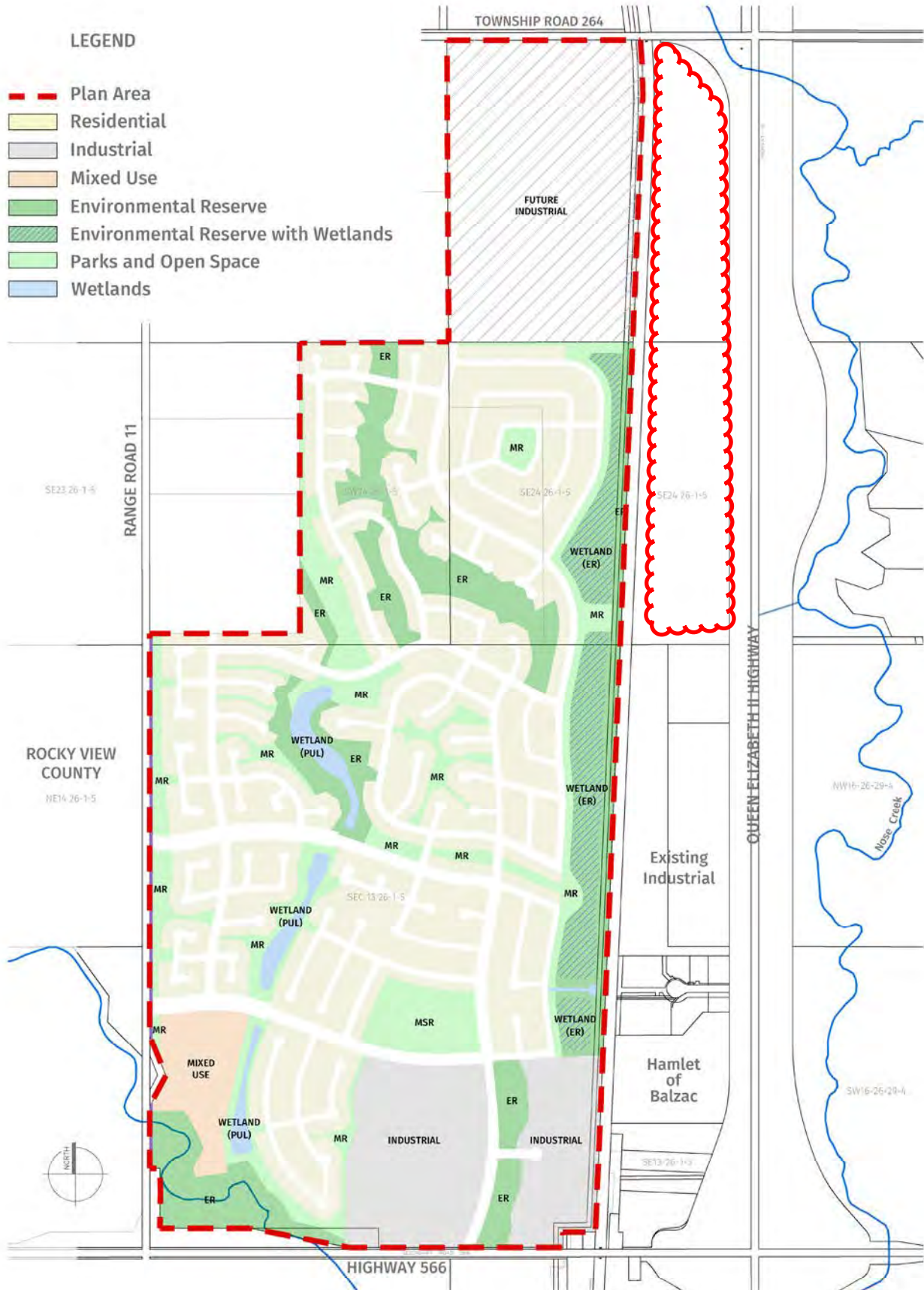


FIGURE 6. Land Use Concept

Figure amended to include Business Commercial lands

5.1 GENERAL COMMUNITY FEATURES

Cul-de-sacs



The inclusion of residential cul-de-sacs produces less traffic and provides for safer, quieter streets for children to play. These residential enclaves will encourage neighbourly interaction and thereby contribute to the overall connectedness of community.

Open Space System



The open space network was created by respecting the existing natural features, enhancing areas as public amenity spaces while preserving the environmentally sensitive areas to be maintained and left in their natural state. This creates an extensive green space network with opportunities for homes to back directly onto them, providing visual and physical access to an enjoyable, back to nature experience right outside residents' backyards.

Back of Lot Walkway System



The community of Rocky Creek will be connected through a unique system of walkways that run between blocks and behind residential properties to provide for a naturalized, rural-feeling pedestrian experience. These walkways will consider safety design elements such as lighting and walkway width to accommodate for comfortable and safe ease of use.

Pedestrian Focused



With an abundance of open space distributed throughout the plan area, this community will focus on the off-street pathway system that not only connects residents to the different amenity spaces, and the commercial and industrial areas, but also provides for an enjoyable, naturalized experience at a comfortable scale.

Organic, Naturalized Design



The community is designed by enhancing the natural landscape, following the natural topography, and providing street and pathway design with added landscaping to evoke a rural feeling of walking the countryside.

Home Owner Association



A Home Owners Association (HOA) will be established in order to manage the maintenance and operation of certain community amenities, including the open space and pathway network, and management of solid waste pick up. When developing the HOA, the developer will also need to establish and enforce architectural guidelines that provide guidelines for aesthetically appropriate development.

Grande Boulevard Gateway



The main entry into the community will introduce the open space, rural theme and set the tone for the green network through the incorporation of wide boulevards, a landscaped median, street trees, and native landscape design that complements the natural character of the area.

Architectural Guidelines



Architectural guidelines will be established by the developer and enforced by restrictive covenants registered with the subdivision of residential lots. This includes guidelines for the design of the pedestrian oriented back of walk walkway and open space network to ensure fences on private lots backing onto open spaces or pathways are built to specified standards.

Mixed-Use



The plan proposes a mixed-use development situated at the southern most east/west entry point into the community. This area is located along a community collector, and on one of the two entry points to the community from the west along Range Road 11. The provision of mixed-use land use will provide opportunities for a variety of housing types mixed with commercial uses, all surrounded by an open space, creating a node of density, activity and community amenity spaces.

Dark Sky



Development within the community will follow the County's Dark Sky Policy, with the intent of cutting down on energy usage, and reducing the effects of unnatural lighting and light pollution on the environment and surrounding community.

Industrial Employment Areas



The portion of lands dedicated to industrial development is distributed within the Plan area to maximize proximity/ access to major transport routes. The industrial uses will provide ample employment opportunities for local residents and those within the greater region, creating a local and regional employment hub that contributes to the County's growing economy and reinforces the community's identity as a fully sustainable, complete community.

5.2 Land Use Statistics

TABLE 2. Land Use Statistics

CONCEPTUAL SCHEME	HECTARES	ACRES	%
Gross Developable Area	304.20	751.07	
(ER) Environmental Reserve	44.87	110.86	
Road Widening	1.00	2.47	
Net Developable Area	258.33	638.34	100
Residential	105.73	261.27	41
Mixed Use Site	6.86	16.95	3
Industrial (Future)	40.85	100.94	16
Industrial	27.27	67.38	11
PUL - Pond West	4.35	10.75	2
Municipal Reserve	20.67	51.07	8
Municipal School Reserve	4.89	12.08	2
Roads	47.56	117.52	18
Road - Primary Divided Collector (45.00m)	3.64	8.01	1
Road - Collector (32.00m)	4.84	11.96	2
Road - Rural Collector (25m)	8.21	20.29	3
Road - Rural Industrial (30m)	0.74	1.83	0
Road - Rural Industrial (24m)	0.98	2.42	1
Road - Pond Collector (20.0m)	1.72	4.25	1
Road - Rural Residential (20.0m)	9.92	24.51	4
Road -RVC Residential (15.0m)	17.51	43.27	6
Net Developable Area	258.33	638.34	100

Note: Areas are approximate based on the current concept plan.

Please refer to Appendix A for proposed Business Commercial lands.

6 Residential Development

The proposed residential development accounts for 41% of the Rocky Creek Plan area, with a focus on development of cul-de-sacs and blocks backing onto open spaces or connected to them through a back of lot walkway system. A lower density and grouping of residential areas vs. a grid network will allow for a greater provision of public open space, and the creation of naturalized pathways to connect residents, reinforcing a rural feel. This unique offering will attract residents of different ages and stages in life who wish for an escape from traditional urban living.

The residential cells have been designed by taking advantage of the rolling topography of the site, locating lots in areas that provide opportunities for views of the Rocky Mountains to the west. The steeper slopes and environmentally significant areas are to be left in their natural state, with enough buffering between built form to allow for the creation of significant open spaces with public amenities and pathway systems to connect residents throughout the greater community. This includes a formalized open space situated at the top of highest point within the Plan area, providing a unique space to enjoy the views, and opportunities for interesting lot types.

The gross density for this community is 4-5 units per acre determined based on the servicing capacity for the area, as outlined in **Section 13 Servicing**. The community will be characterized by laneless, front-drive homes, reinforcing a rural feel vs. the higher densities seen in laned neighbourhoods within urban municipalities.

The housing types within Rocky Creek will be primarily single detached dwellings, on a variety of lot sizes. The average depth of these lots will be 100 feet (30 metres) with lot widths from 48 feet (15 metres) to accommodate double car garages, to 58 feet (18 metres) to accommodate triple car garages. The residential zoning will provide opportunities for additional housing types such as semi-detached and duplexes, providing a variety of choices for the neighbourhood. This will also include opportunities for more attainable housing options such as manufactured dwellings, tiny dwellings and

accessory dwelling units. Lastly, the mixed-use area of the community will provide opportunities for multi-unit housing, such as row housing, all contributing to a greater variety of housing options available to residents in the community.

The commercial area within the mixed-use lands in the south western portion of the community will provide important neighbourhood services, and act as a destination for residents in surrounding areas due to its direct connection to Range Road 11. The intent for this area is to allow for residential housing developments, such as multi-unit housing, comprehensively designed to ensure appropriate integration with the surrounding commercial and open space, creating an area where residents can live, work, and meet their daily needs, all within walking distance.

6.1.1 Residential Policies

- 6.1.1.1 Residential development shall be generally consistent with the land use concept shown in **Figure 6 Development Concept**.
- 6.1.1.2 Residential development should be designed to be sensitive to adjacent acreage and agricultural development by incorporating complementary interface and transition treatments.
- 6.1.1.3 All residences in the Plan area should be located within a 400 metre radius (or 5 minute walk) of an open space or pathway.
- 6.1.1.4 All residential development directly adjacent to industrial land uses or major roadways (Highway 566 and Range Road 11) should follow policies as outlined in **Section 11 Interface Design**.
- 6.1.1.5 At the time of subdivision, the developer shall prepare and implement Architectural Guidelines for residential development that reflects the character of the community and ensures the high quality, natural aesthetic of the development.

7

Open Space & Pathways

The Rocky Creek community is characterized by close to 200 acres of open spaces, broken down into environmental reserve, municipal reserve and municipal school reserves. The subject site presents many characteristics that are consistent with a typical Alberta Prairie landscape including rolling topography, native grassland vegetation, and natural drainage courses. This existing combination of physical geography and biology establishes the foundation for an open space network that respects and enhances this natural landscape and creates a holistic community amenity through the vehicle of design.

The primary intent of the open space design for this development is to embrace and accentuate the prairie landscape with a focus on connectivity, proximity, view corridors, and a meaningful interface of man-made and natural amenities. This will create a strong connection to the landscape and a natural experience for all residents.

CONNECTIVITY

The overall intent of the open space system is to promote walkability and physical activity through a series of amenity nodes and an extensive pathway system that binds them together. The comprehensive pathway network will be designed to appeal to a wide range of uses and users, with a design strategy that complements both the natural terrain and manmade landscape through the integration of varying trail widths and surface treatments. The pathway network will seek to connect residents to different open spaces and connect those open spaces with each other. The pathway network design considered possible future connection to the County's broader pathway network when it may be available in the future.





Figure amended to include Business Commercial lands

WATER

Water is ever present in the natural prairie landscape as it will be in the context of this open space system. Constructed wetlands will be integrated throughout as a nod to the native landscape as well as a means of stormwater management for the community. These water courses and the associated open space will also create opportunities to integrate amenity value in the form of active or passive recreational uses, and pathway integration where appropriate, in addition to creating habitat and promoting biodiversity.



NATURALIZATION & THE LANDSCAPE

Entry points to the community, as shown in **Figure 7**, will introduce the open space theme and set the tone for the green network through the incorporation of wide boulevards, a landscaped median, street trees, and native landscape. The unique Grand Boulevard street layout will provide opportunities for design to contribute to the community's overall identity.

The overall planting strategy in a general sense is to respect naturally occurring vegetation patterns, with native trees and shrubs located at lower elevations and adjacent to water bodies and drought tolerant species including naturalized grasses being established at higher elevations. Plant selection will focus on native, non-invasive, Chinook-hardy tree and shrub species.

The planting will be fundamentally integrated with the surface stormwater management system, as grasses and riparian species in low-lying areas will help prevent erosion, slow water flow, increase permeability, and help naturally treat water prior to entering the constructed wetlands. There will be strategic areas that require a more decorative plant palette to frame and/or accent specific site elements.

BUFFER ZONES

Buffer areas will utilize berming and denser planting as appropriate both as a visual screening mechanism as well as a method of noise attenuation, specifically along the east perimeter of the site, separating residential from the railway and existing industrial to the east, as well as in areas where there is residential adjacent to future industrial land uses and/or major roadways. Fencing options will also be considered as a buffering method and will be utilized for the purposes of both spatial definition and aesthetics intended to contribute to the character of the site.

SCHOOL SITE

The Rocky Creek Plan dedicates a portion of land towards a school site for the community that meets the anticipated size and configuration requirements of the school boards. This is located central within the community, along two collector roads, and allows a developable site for the required school building envelope and required playfield(s). This site will provide community opportunities for active recreation uses such as baseball diamonds, playfields etc. as per the school boards' site programming. Specific layout of the school site is to be determined by the school authority at the subdivision stage.



AMENITY SPACES

The open space network provides opportunities for public amenity spaces where passive and or active recreation activities can occur (shown in **Figure 7** as Activity or Passive Amenity Location). The existing Hamlet of Balzac south east of the community contains the Balzac Community Hall, a community institution that has been servicing residents in the surrounding region for over 90 years. The Rocky Creek Plan does not propose a community building in order to preserve the vitality of this existing historical landmark. The community seeks to compliment the Balzac Community Hall by providing local commercial services and additional residents in the area to continue the long-standing tradition of the Hall by serving the community.

7.1.1 Open Space Policies

- 7.1.1.1 Municipal Reserve dedication as shown in **Figure 7** is to be dedicated to Rocky View County in accordance with Section 666 of the Municipal Government Act.
- 7.1.1.2 The Municipal Reserve lands shall be maintained and operated by the Homeowners' Association via a license arrangement with Rocky View County.
- 7.1.1.3 The development of open spaces should be coordinated with the area's Recreation Boards and the development of the school site should be in consultation with the local school authority.
- 7.1.1.4 A Landscaping Plan for public and private open spaces should be required prior to the endorsement of the applicable plan of subdivision or the issuance of a Development Permit.
- 7.1.1.5 Open space amenities throughout the plan should be programmed to service a wide variety of ages and abilities, providing active and passive activities that are ideally year-round.
- 7.1.1.6 Design of open spaces should seek to enhance human comfort by maximizing solar exposure and providing protection from prevailing winds.
- 7.1.1.7 Provisions should be made by developers for the prevention and control of invasive noxious weeds (as listed by Alberta Weed Control) on both private and public lands.

7.2 Pathway Network

7.2.1 Back of Lot Walkways

The community will be characterized by an innovative network of walkways that will be located in the rear of residential properties with the purpose of connecting all residents to the open space network (as shown in **Figure 7**). At the time of subdivision, a right-of-way will be established with a Public Access Easement registered on each property to allow for the public access and use of these walkways that are not public MR, but instead located on private property. This will be accomplished by either locating the entire right of way on one property or splitting it onto backing adjacent properties. The Public Access Easement will also allow an established Home Owners Association to access the walkways for operations and upkeep, which where appropriate, will allow for shallow-utility servicing, along with public access.

7.2.2 Back of Lot Walkway Policies

7.2.2.1 At the time of subdivision, the developer shall register appropriate easements and utility rights-of-ways along properties that include back of lot walkways to provide public and utility access on private property.

7.2.3 Pathway Network Policies

Please note the following policies apply to the entire pathway network, including the back of lot walkways.

- 7.2.3.1 The pathway network shall provide naturalized connections between open spaces, significant community features and residential areas.
- 7.2.3.2 The pathways should be constructed by the developer as shown in the corresponding cross section design outlined in **Section 12.3**.
- 7.2.3.3 The pathway networks proposed should be constructed to minimize impacts on the natural environment.
- 7.2.3.4 The pathway network should incorporate design elements that allow for safe, comfortable use for pedestrians and cyclists.
- 7.2.3.5 The pathway network shall be maintained by a Home Owners' Association and shall provide public access for all Rocky View County residents.



7.2.3.6 The pathway connections shown to the south (**Figure 7**) should provide for connection to future regional pathways in the area. The exact location of these connections should be determined at the subdivision stage.

8

Home Owners Association

A Home Owner’s Association (HOA) will be established to dispense a variety of community elements of the Rocky Creek development. The Home Owner’s Association will also be required to enter into license arrangement with the County for the maintenance of public open space and facilities including Municipal Reserve lands.

In summary, the HOA will be responsible for the maintenance and operation of:

- Open space network and pathway network
- Management of solid waste pick up, i.e. garbage and recycling
- Enforcement of architectural guidelines

8.1.1

Home Owner’s Association Policies

- 8.1.1.1
- A Home Owner’s Association shall be established by the developer at the time of subdivision, and each property owner shall be a member of the Home Owner’s Association pursuant to a restrictive covenant registered against all titles on the subject land.
- 8.1.1.2
- The Municipal Reserve lands, pathways and walkways, shall be maintained and operated by the Home Owner’s Association via license arrangement with Rocky View County.
- 8.1.1.3
- A Home Owner’s Association shall be responsible for the following:

a. Implementation and enforcement of the Architectural Guidelines as established by the developer at the subdivision stage;

b. Operation and maintenance of both the publicly owned and privately owned open spaces, pathway system, and the associated amenities which include public realm waste and recycling amenities; and

c. Solid waste management and recycling services for the residential development in accordance with the County’s Solid Waste Master Plan and Solid Waste Servicing Strategy.

9

Service & Employment Sector

9.1 Commercial

The Rocky Creek community includes a mixed-use area for the combining of residential and local commercial services as shown in **Figure 6**. This combination will allow residents to use local services to meet daily needs, and provides opportunities for local employment, all within the immediate area where community members can also live. This provides for an area that encourages social interaction and builds local community character. Additional to the commercial node within the Plan area, the Rocky Creek community is in close proximity to one of the largest commercial hubs in Alberta, CrossIron Mills. The combination of new and existing commercial uses will provide opportunities for residents to shop for their daily needs through local community services and other shopping and employment opportunities outside of the community all within close proximity.

9.1.1 Commercial Policies

- 9.1.1.1 The exact size of commercial parcels shall be determined at the subdivision stage and shall comply with the designated land use standards as per the Rocky View County Land Use Bylaw.
- 9.1.1.2 Applications for commercial development should align with the County's Commercial, Office, and Industrial Design Guidelines.
- 9.1.1.3 All commercial developments should follow the principles of Crime Prevention Through Environmental Design (CPTED).

9.2 Industrial

The Rocky Creek community will include various portions of land dedicated to industrial development as shown in **Figure 6**. Business and employment uses proposed will align with the Balzac West ASP which proposes light industrial and light industrial business park(s) along with residential and commercial development. The industrial districts within Rocky Creek are located in areas that are in close proximity to, and are well accessed by, major roadways Highway 566 in the south and Township Road 264 in the north. These ultimately provide each area a strong connection to the QEII, a major regional transportation corridor. By focusing industrial development in these locations, it contributes to orderly growth and desired economic efficiencies in the development of transportation and infrastructure systems, which is in alignment with the County Plan. Additionally, both east/west roadways Highway 566 and Range Road 264 have a planned future interchange that once developed, will improve the access from the north and south industrial lands to the QEII.

With close to 170 acres of industrial lands included within the community, these industrial developments will provide economic diversity for the County realized through additional tax revenue, increased spending dollars being retained within the County, and the provision of employment opportunities for the immediate community and surrounding region. The industrial areas are designed to accommodate a wide variety of industrial clients with parcel sizes determined at the subdivision stage. Uses in these areas will be required to follow the Rocky View County Land Use Bylaw, where the manufacturing and/or operation is contained entirely within the building/structure with little to no impacts on adjacent land uses. These uses include business such as distribution logistics, warehousing, construction, processing and manufacturing. In addition to locating the industrial next to the major transportation networks, the Rocky Creek community design has utilized the natural green spaces in various areas as a natural buffer between residential and industrial land uses. **Section 11** Interface Design also includes policy to ensure that in areas where industrial is adjacent to residential land uses, sufficient screening design methods are employed to mitigate against possible negative offsite impacts (i.e., visual, audible and or smell/contaminates), to achieve an appropriate transition.

9.2.1 Industrial Policies

- 9.2.1.1 Industrial development should be generally consistent with the land use concept shown in **Figure 6 Development Concept**.
- 9.2.1.2 The size of industrial parcels shall be determined at the subdivision stage and should comply with the designated land use as per the Rocky View County Land Use Bylaw.
- 9.2.1.3 Uses proposed in industrial areas should be businesses occurring within enclosed buildings and/or within outside areas that are appropriately buffered and screened from adjacent properties.
- 9.2.1.4 Applications for development within the industrial areas should align with the County's Commercial, Office, and Industrial Design Guidelines.
- 9.2.1.5 All industrial developments should follow the principles of Crime Prevention Through Environmental Design (CPTED).

10

Dark Sky

10.1 delete

All lighting within Rocky Creek will convey a cohesive community identity and should align with both the County's Dark Sky policy and the International Dark Sky Association guidelines. In adherence to these policies, Rocky Creek will be designed to be as inconspicuous as possible while still providing enough lighting to maintain visibility for a safe, high quality travel experience, no matter the mode. The scale of lighting should be considered along pathways to respect the pedestrian scale, for example, by installing low mounted lighting. Street lighting will adhere to both the County and Alberta Transportation design standards at the time of installation.

10.1.1 Dark Sky Policies

- 10.1.1.1 Low impact lighting should be incorporated to mitigate light pollution.
- 10.1.1.2 Flood lights, spot lights, or any other large-area, high intensity lighting is discouraged.
- 10.1.1.3 At the subdivision stage, an outdoor lighting plan should be completed to the satisfaction of Rocky View County and shall demonstrate;
 - a. Lighting techniques to reinforce community character and maintain visibility for safety; and
 - b. Rationale for how the outdoor lighting meets the Rocky View County's Dark Sky policy.



Interface Design

The Rocky Creek community will be comprised of primarily residential development with a portion of the land being dedicated to industrial development. Interface design measures should be employed between residential and industrial lands to achieve a seamless transition and mitigate against any potential negative impacts, such as noise, odor, or unsightly areas. Additional to buffering between industrial and residential, these design measures should be considered in the design of residential areas that are adjacent to major roadways, Highway 566 and Range Road 11, to mitigate from the sound and visual impacts of vehicular traffic.

Interface design measures that align with the community's overarching principle of preserving and enhancing the natural landscape will be encouraged, for example, utilizing natural vegetation buffers, open spaces, landscaping and or berming where possible.

Examples of interface design measures that may be used to mitigate impacts and provide an appropriate transition include;

- Strategic Phasing
- Open space
- Landscaping
- Berming
- Screening
- Fencing and;
- Architectural elements to enhance visual appeal

Sufficient interface design should be implemented in the following areas;

- Where industrial land use abuts residential, examples include;
 - Between the future industrial for the northern most portion of the Plan area and the adjacent residential

- Between the industrial in the south eastern portion of the Plan area and the residential/ open space that surrounds it on the north and west sides
- Where residential is adjacent to existing agricultural lands
- Where residential lots face the Canadian Pacific Railway and existing industrial land use to the east
- Land uses adjacent to major transportation corridors Range Road 11 (to the west), Range Road 246 (to the north) and Highway 566 (to the south)

11.1.1 Interface Design Policies

- 11.1.1.1 At the subdivision or detailed design stage, the developer should implement sufficient interface design measures in the following areas.
- a. Where residential is adjacent to and or facing existing industrial to the east
 - b. Where residential is adjacent to industrial and future industrial
 - c. Where industrial and future industrial is visible from the Queen Elizabeth II Highway corridor
 - d. Where residential is adjacent to major roadways Highway 566 and Range Road 11
 - e. Where residential is adjacent to agricultural lands by consulting with the County's Agricultural Boundary Design Guidelines
- 11.1.1.2 Development along the eastern portion of the Plan area shall mitigate visually from, and not interfere with the function of the Canadian Pacific Railway line.
- 11.1.1.3 Developers are encouraged to implement natural vegetation buffers where appropriate.

12 Transportation

The Rocky Creek transportation network will preserve the natural landscape, following the existing topography where possible, and includes street design that efficiently moves vehicles, provides adequate access in and around the community, and utilizes natural landscaping to enhance a rural atmosphere.

12.1 External Road Network

External road connections to Rocky Creek are to be made via two accesses from Range Road 11 to the west, and one access from Highway 566 to the south. Access to the north industrial lands will be longer-term in nature, but will eventually access Township Road 264; as shown in **Figure 8**.

At the south limit of the project, the development will be accessed from Queen Elizabeth II Highway via a future interchange constructed at Highway 566. This interchange has been designed and is currently awaiting funding before proceeding into construction.

A Transportation Impact Assessment (TIA) was completed by ISL Engineering and Land Services Ltd. in support of this Conceptual Scheme and will be provided to the County under separate cover. The TIA outlines specific off-site upgrades that may be required based on traffic volumes generated by way of this development.

12.2 Internal Road Network

Rocky Creek will be accessed by three main access points to and from the community, including;

- To/from the south at Highway 566
- To/from the west at Range Road 11 – one in the southern portion of the plan, and one approximately 435 metres north

The Grand Boulevard is the northern east/west access point originating from Range Road 11 to the west, providing the main point of entry to the community. This boulevard will be designed to be a wide, meandering access with a landscaped median and then taper to two lanes with no median as it meets the open spaces in the centre of the Plan area. This will then branch off north and south, with the southern portion circling back to the integrate with the second access point from Range Road 11, as shown in **Figure 9**. The location of the southern access point is to provide the industrial uses with access to the external highway connections.

The road network has been designed to work with the existing topography and preserve the natural landscape, where possible. Wide boulevards with ditch drainage have been incorporated throughout the road network wherever possible. This will enhance the rural feel of the community and provides significant benefits for stormwater treatment. There will be three main spine north/south collectors as shown in **Figure 9**.

While there is currently no regional public transit system proposed, community development will consider connection points with Rocky Creek should this occur in the future.

Road naming in accordance with approved municipal policy will be determined at subdivision stage, pending branding and theming of the neighbourhood.

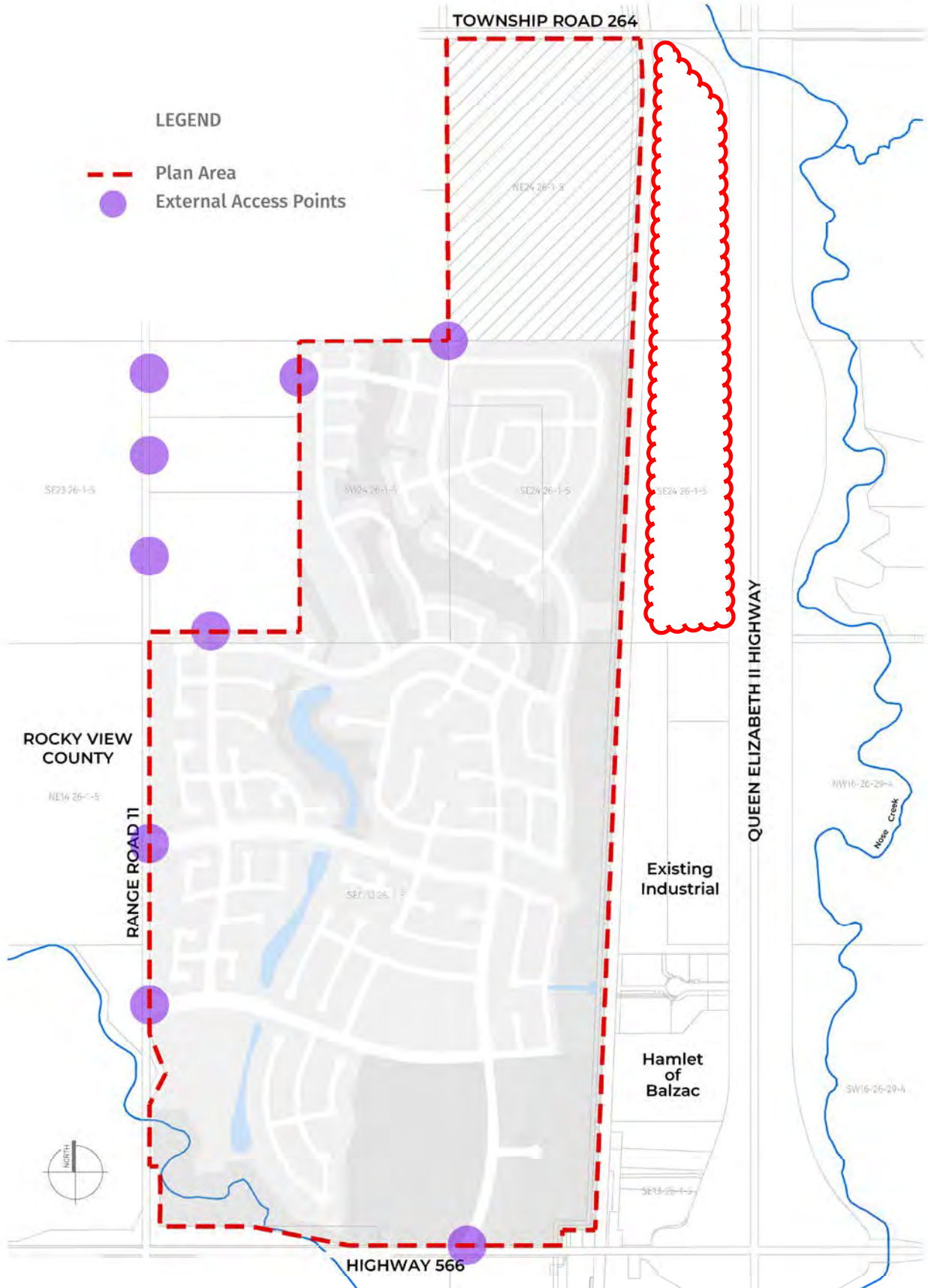


FIGURE 8. External Road Network

Figure amended to include Business Commercial lands

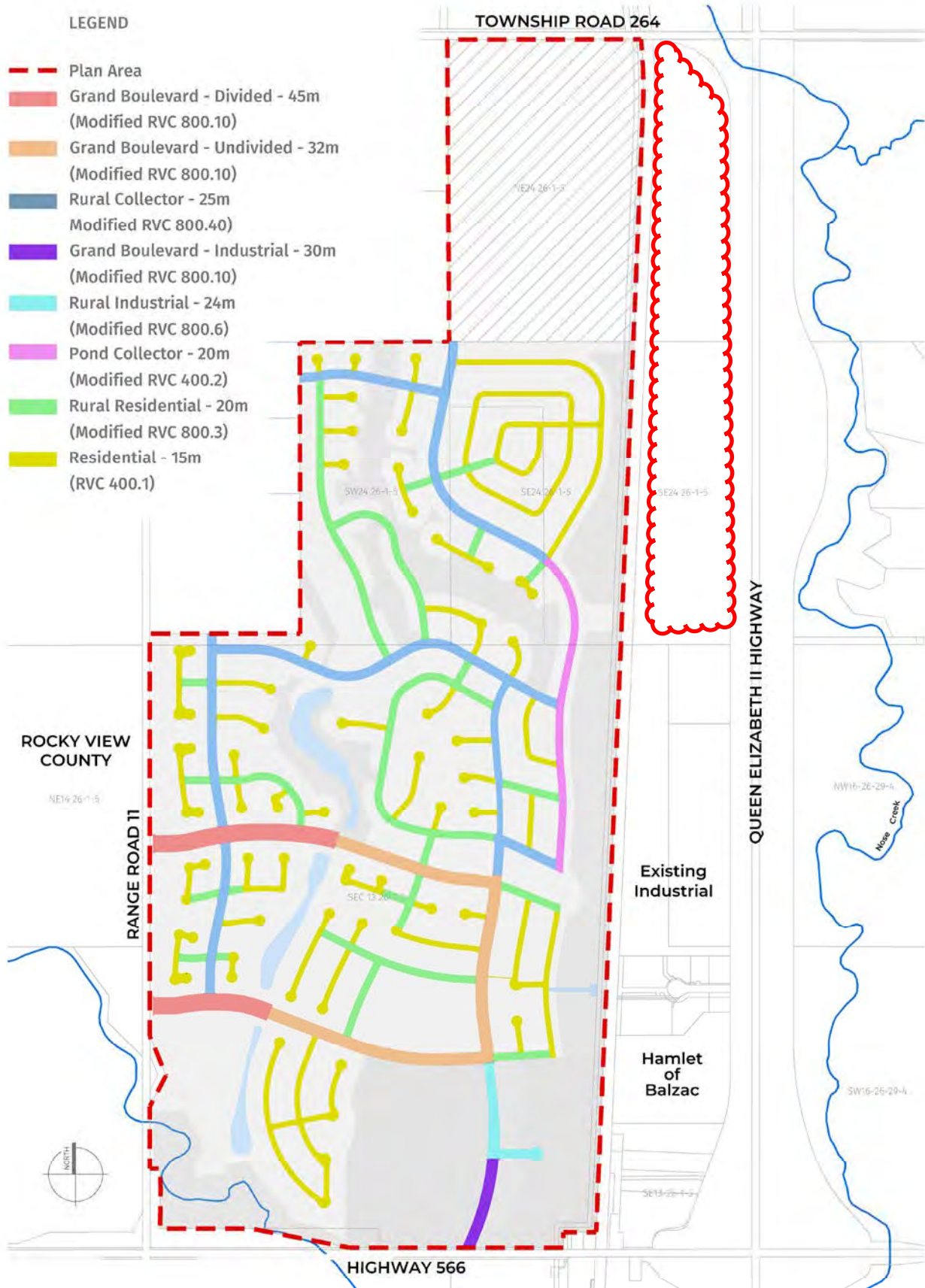


FIGURE 9. Internal Road Network

Figure amended to include Business Commercial lands

12.3 Road Design

The Rocky Creek Conceptual Scheme has designed custom roadways that are in accordance with, or exceed the County Servicing Standards. An overview of each proposed roadway design and their corresponding cross section is outlined in this section below.

Grand Boulevard - Divided

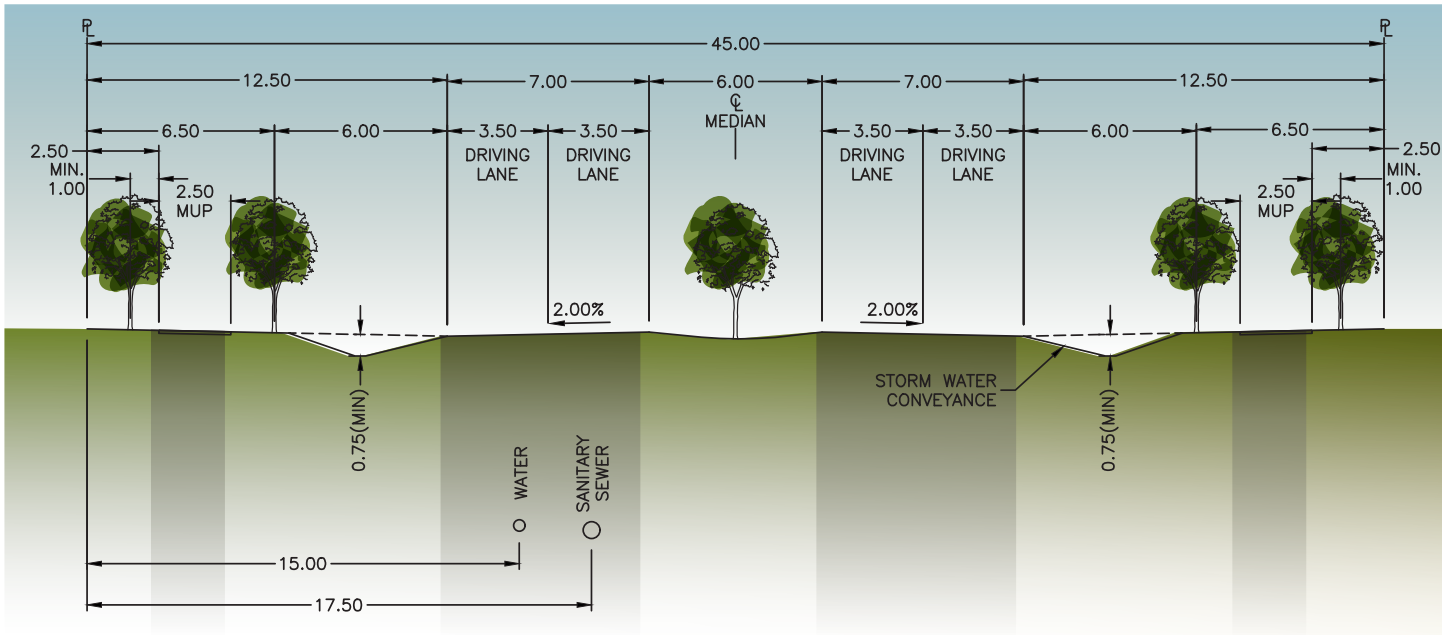


FIGURE 10. Grand Boulevard - Divided

This roadway design features a wide landscaped median with trees, multi-use pathways and a dual row of trees in both boulevards. Informed by Rocky View County's standards for a grand boulevard, this section is intended to serve as a grand entrance into the community with the enhanced landscaping and wide boulevards. As is the case with all rural sections to follow, no curb and gutter is proposed which enhances the rural character and capitalizes on the road ditches for storm water conveyance and treatment.



Grand Boulevard - Undivided

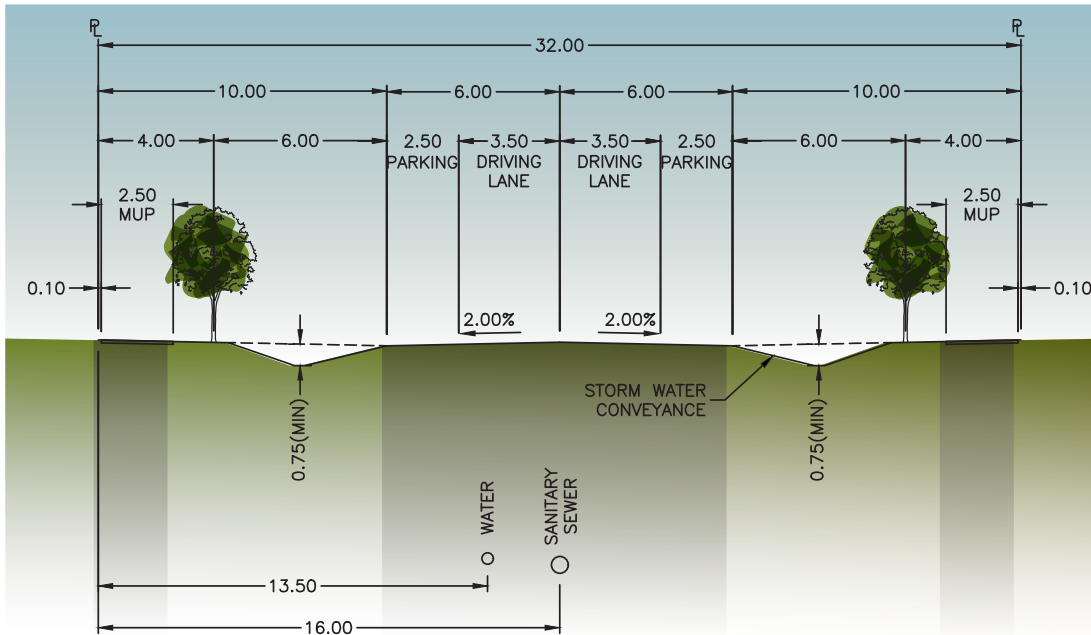


FIGURE 11. Grand Boulevard - Undivided (with Parking)

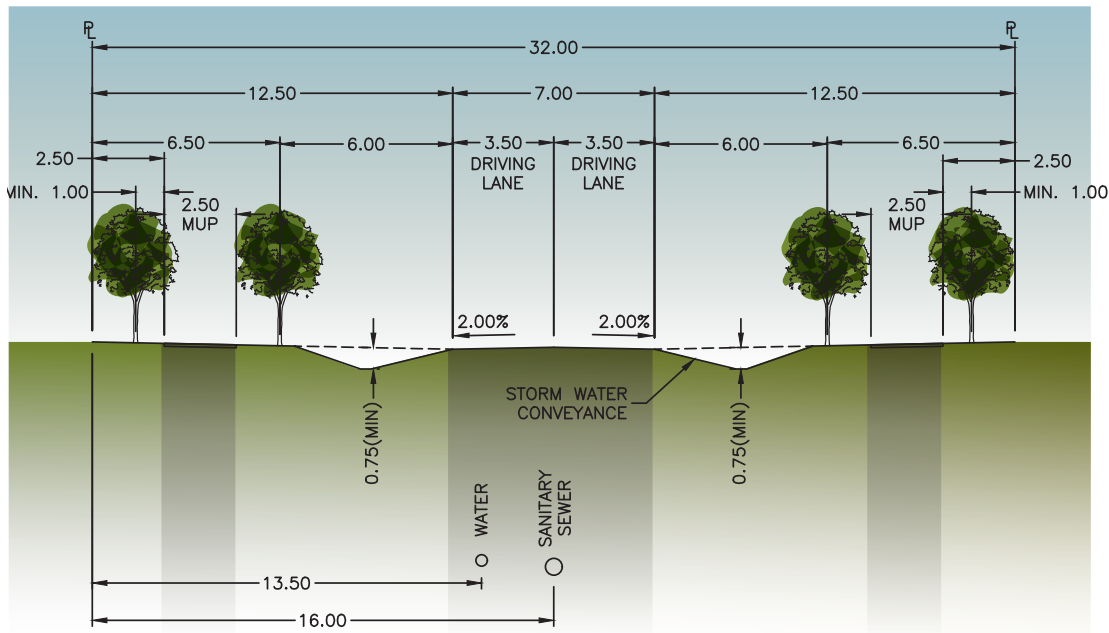


FIGURE 12. Grand Boulevard - Undivided (without Parking)

Three separate versions of this rural cross section are proposed based on varying parking requirements along the corridor loop for this section. Each version maintains a consistent 32 metre right of way which allows for seamless transitioning between each. Multi-use pathways are maintained throughout. Where no parking is required, the row of trees on either side of the pathway is also maintained.

- Undivided with Parking
- Undivided no Parking
- Undivided Parking One Side

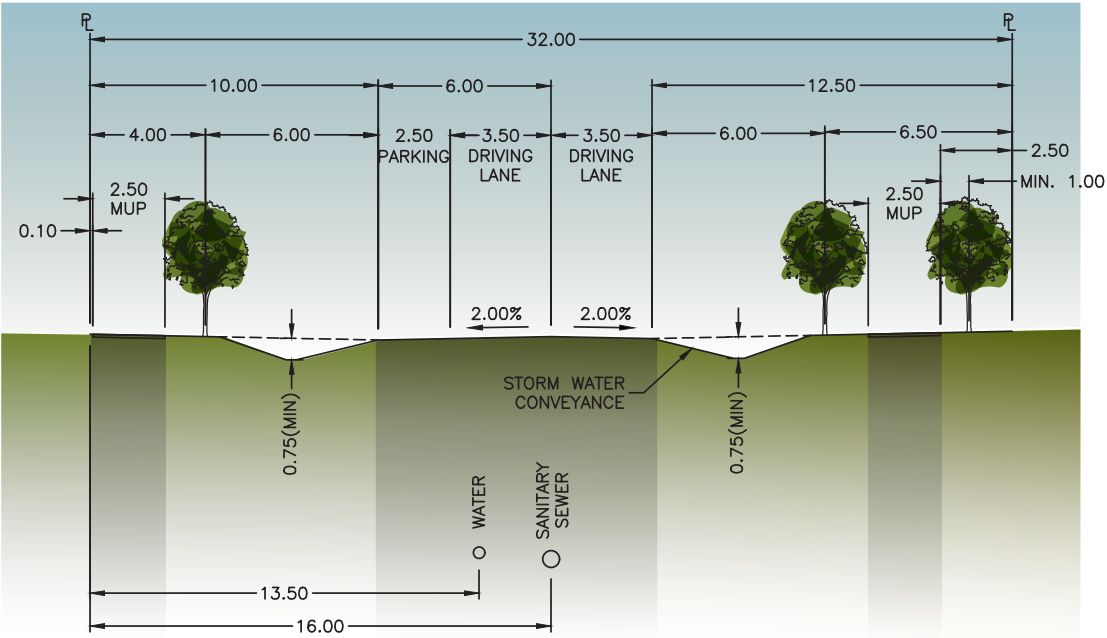


FIGURE 13. Grand Boulevard -Undivided (Parking on one side)



Industrial

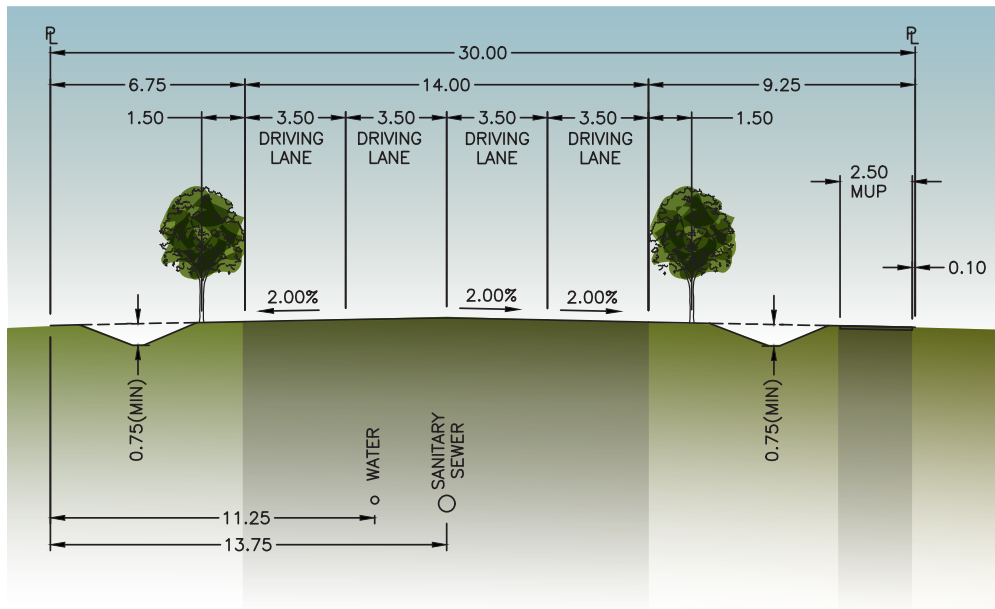


FIGURE 14. Grand Boulevard - 30m Industrial

The goal of designing this roadway is to strike a balance between the overall right of way width and access requirements for the community. The section provides a large carriageway with four lanes of travel given the higher volumes of traffic from the adjacent industrial uses. At the same time, this section provides a unique experience for pedestrians by proposing a multi-use pathway in parallel to the escarpment along the east side of the road. Boulevard trees have been added within the right of way to provide grand appeal.

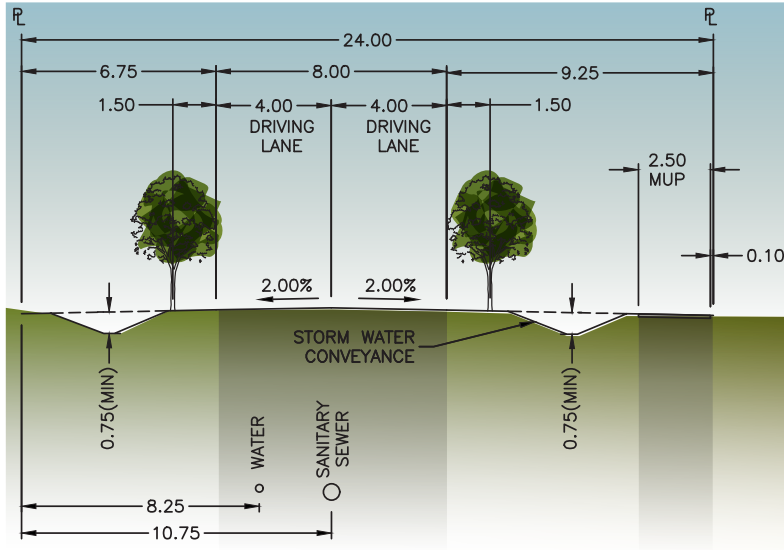


FIGURE 15. 24m Rural Industrial

This cross section provides two wide lanes of travel to support truck and heavy traffic movement and proposes to maintain boulevard trees throughout plus the multi-use pathway in parallel to the escarpment. The section is based on the standard neighbourhood business street but proposes to remove curb and gutter throughout.

Collector

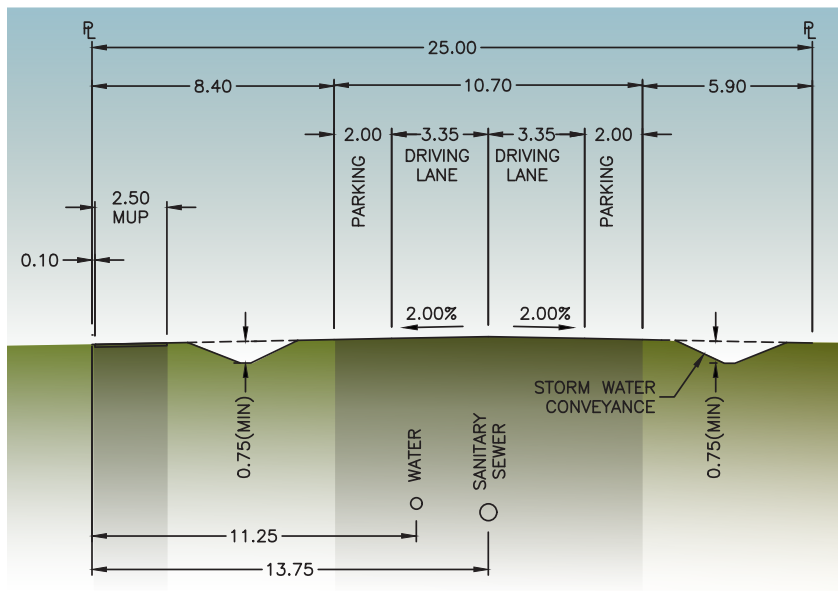


FIGURE 16. 25m Rural Collector

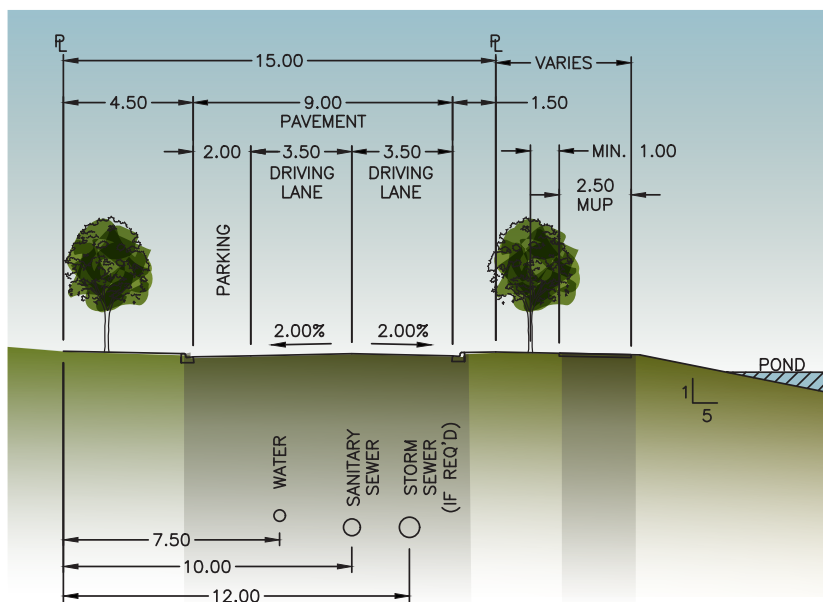
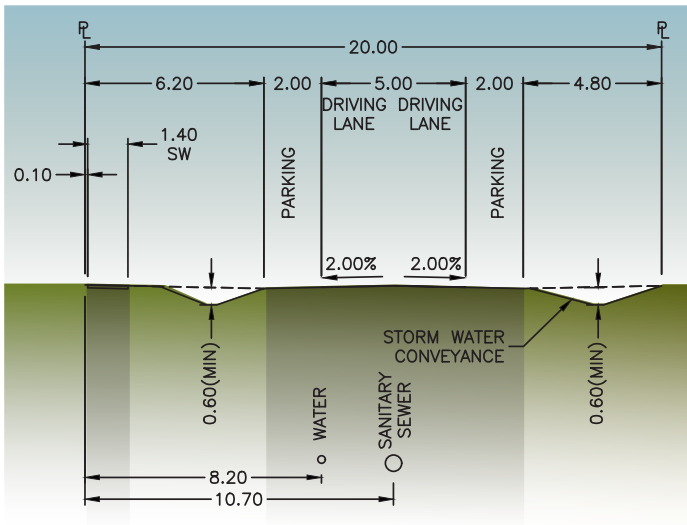


FIGURE 17. 15m Pond Collector

This cross section achieves standards for a residential collector aligning with the lane widths and space for parking. Proposed modifications include the removal of curb and gutter throughout and the addition of boulevard trees. Upgraded pedestrian facilities are proposed with the multi-use pathway instead of the standard separate sidewalk.

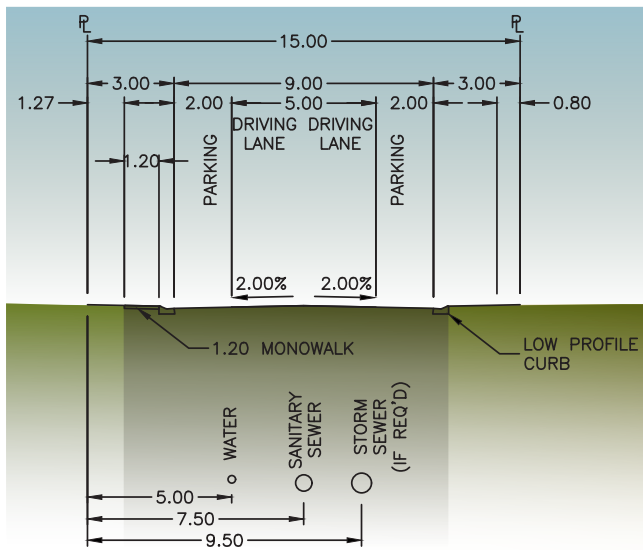
The pond collector is based on the standard for a typical urban residential collector but has proposed unique modifications given its adjacency to the pond. Typical urban boulevards are proposed to facilitate easy access onto adjacent residential lots. The carriageway width has been slightly narrowed to facilitate traffic calming. Boulevard trees are proposed to help with screening adjacent sites and nearby roads. Additionally, the right of way has been strategically narrowed to allow for enhanced pedestrian activity within the adjacent pond.

Residential



This cross section achieves standards for a residential local road aligning with the lane widths, and including parking, and pedestrian access. Similar to the rural collector, curb and gutter are proposed to be removed throughout.

FIGURE 18. 20m Rural Residential



This cross section proposes to match the standards for a typical residential road. The benefit of the standard boulevards (with no ditches) and curbs is to assist with driveway accesses.

FIGURE 19. 15m Residential

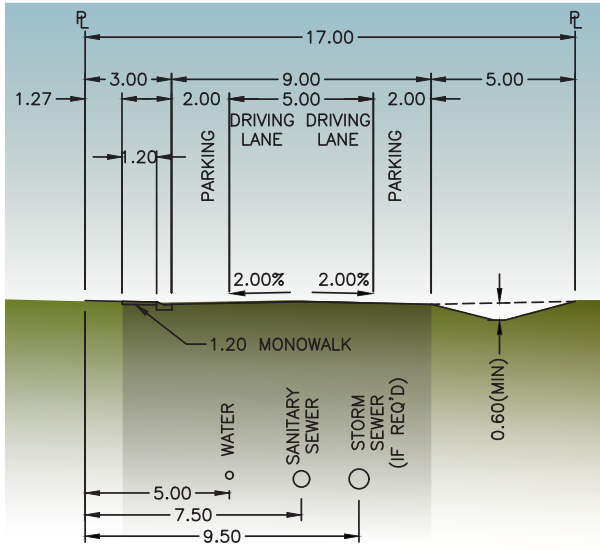


FIGURE 20. 17m Modified Residential

This roadway design is intended as a hybrid between the rural and urban residential sections and is anticipated to be proposed through detailed design wherever there are side yards across from standard lot frontages. The rural side of the section (i.e., the ditch side) is envisioned to be located along the side yard where no driveway access is required and where ditch conveyance can be directed towards storm facilities.

12.3.1 Transportation Policies

- 12.3.1.1 Access to the development shall generally be in accordance with **Figures 8 & 9**.
- 12.3.1.2 All road systems necessary to service the development shall be constructed by the developer to the satisfaction of Rocky View County, in accordance with the County's Servicing Standards for Roadways.
- 12.3.1.3 All road names shall be determined at the subdivision stage and shall be named in accordance with the approved municipal addressing policy, to ensure proper identification of distress calls and property locations in times of emergency.
- 12.3.1.4 Road cross sections shown in Figures 14-20 (inclusive) are conceptual in nature and should be refined at future detailed design stages. Consideration should be given to the County's Servicing Standards for Roadways.
- 12.3.1.5 At the detailed design stages, appropriate locations for snow storage should be investigated and incorporated into the plan, as appropriate.
- 12.3.1.6 At the subdivision stages, appropriate land dedication shall be provided for future Range Road 11 widening purposes, as the development progresses.



- 12.3.1.7 Consideration for transit readiness should be determined during detailed design at the subdivision stages, in collaboration with adjacent municipalities.

13 Servicing

Rocky Creek will be a fully serviced development and will include watermain, wastewater collection systems, shallow utilities and a stormwater conveyance system connecting to several storm water management facilities throughout the development. Servicing strategies have been reviewed and summarized in the Water/Wastewater Servicing Design Brief and Sub-Catchment Master Drainage documents completed by Urban Systems Ltd.

13.1 Shallow Utilities

Shallow utilities will be provided to the subject lands through the extension of existing networks in the surrounding areas. Shallow utility services will include electricity, natural gas, telephone and cable. Shallow utility feeds will be extended into the development on a phased basis and will be installed along roadways in 2.4-3.5m rights of way (as required). Wherever possible, servicing to residential cells is proposed from the back of lots through easements and rights-of-way as described in **Section 7.8**. This servicing strategy will minimize the amount of shallow utility trenching and cabling required throughout the development and is currently being explored in more detail in consultation with all utility providers.

13.1.1 Shallow Utilities Policies

- 13.1.1.1 Shallow utilities shall be provided within the Plan area at the sole expense of the developer and shall be located within the appropriate utility right-of-way established at the subdivision stage.
- 13.1.1.2 Rights-of-ways will be established at the subdivision stage to provide for utilities to be located within the rear of residential properties, where feasible.

13.2 Water Servicing

Potable Water servicing to the Rocky Creek development will be provided by an extension of the East Balzac water system by Rocky View County. A dual 400mm trunk main feed is proposed at the connection point which will extend to the west and north extents of the Rocky Creek development, and will feed smaller distribution mains throughout development.

The proposed servicing concept is shown on the following page, **Figure 21**. The proposed concept will allow for extensions to the west of Range Road 11 in the future as well as to future industrial lands to the north as required. All potable water servicing will occur in a logical, sequenced manner and will be in conformance with the current Rocky View County Servicing Standards.

The East Balzac water system is fed by the existing East Balzac Reservoir and Pump Station. Based on the pump discharge pressure, it is anticipated that the development will see pressures exceeding 80 PSI therefore pressure reducing valves will be required at buildings throughout the development.

The East Balzac extension and dual 400mm feed connection is currently in process for construction by Rocky View County. Construction completion is anticipated in 2021. Based on anticipated demands for water usage within Rocky Creek, water capacity is available for the development. Further detail has been provided in the supporting Servicing Design Brief completed by Urban Systems Ltd.

13.2.1 Water Servicing Policies

- 13.2.1.1 Potable Water servicing shall be provided by existing regional water utility services.
- 13.2.1.2 The potable water distribution system shall be designed to ensure adequate fire protection throughout the development, as per section 606.5 of the Rocky View County Servicing Standards. Details of Fire Flow Storage requirements will be confirmed at the detailed design stage.

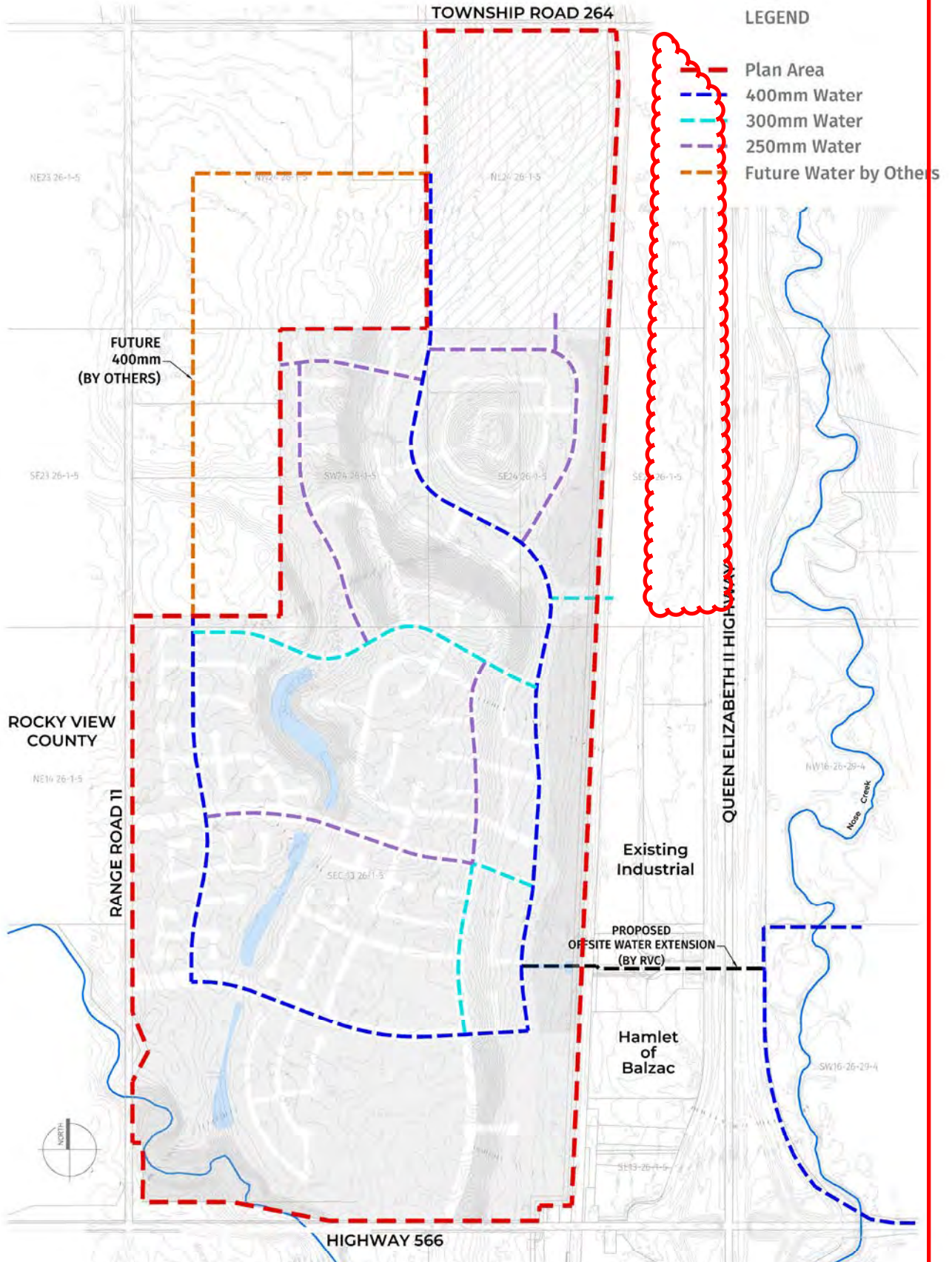
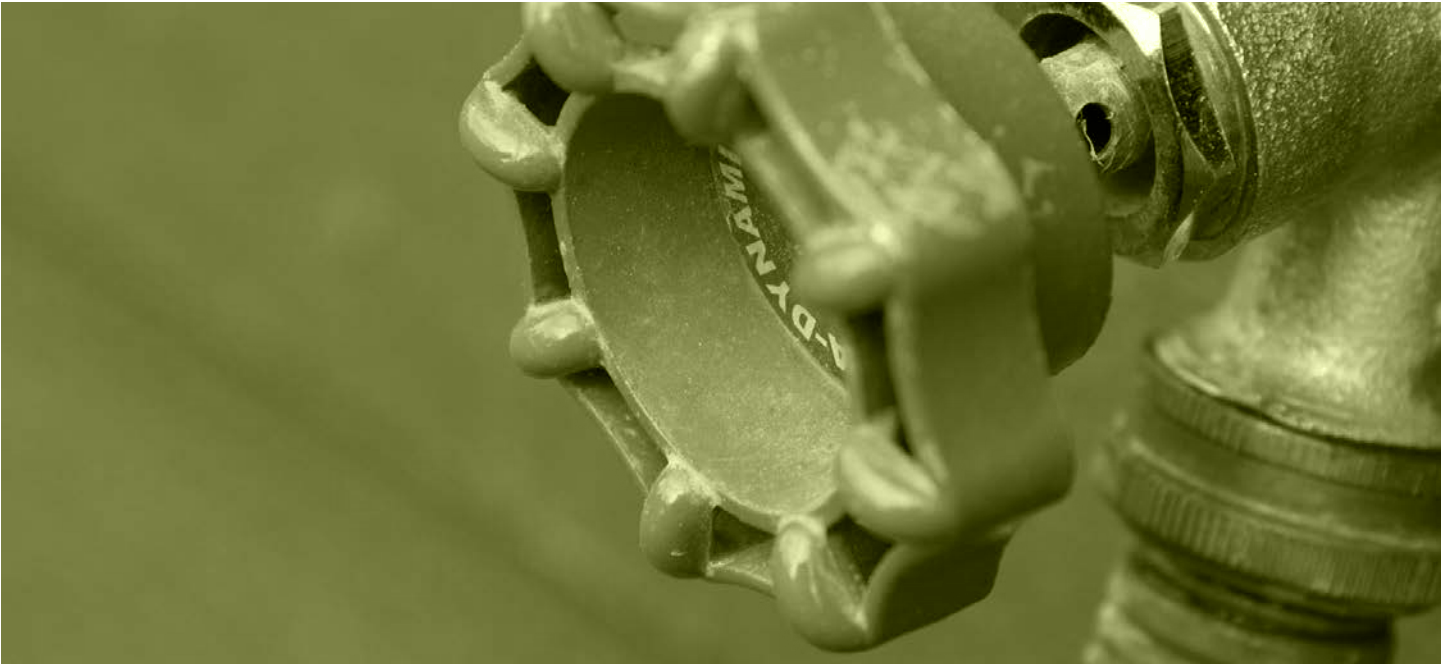


FIGURE 21. Water Servicing

Figure amended to include Business Commercial lands



13.3 Wastewater Servicing

Wastewater flows from the Rocky Development will be collected within a centralized sanitary sewer main system. The system will drain south and east by gravity towards a connection point at the West Balzac sanitary lift station where the flows will be conveyed through the existing sanitary system in East Balzac.

As per the Preliminary West Balzac Servicing Review completed by Sedulous Engineering Inc. in September 2019, the existing East Balzac Sanitary Collection has been sized to accommodate up to 190 L/s of flow. 150 L/s of this flow is allocated towards ultimate buildout of the West Balzac ASP area which includes the Rocky Creek development. Design of the lift station is currently being refined by Sedulous Engineering Inc. using the 150 L/s flow assumption and construction of the force main crossing is underway. Completion of all construction is anticipated in 2021.

Based on updated flow generations for the Rocky Creek development, 115 L/s of total flow is anticipated at full build out. There is therefore adequate capacity through the force main from the proposed West Balzac Lift Station and within the existing downstream sanitary mains in East Balzac. Further detail has been provided in the supporting Servicing Design Brief completed by Urban Systems Ltd.

It should be noted that coordination between Urban Systems Ltd. and Sedulous Engineering Inc. has occurred to confirm the proposed location of the lift station. The proposed location as well as the servicing concept for the development is shown on **Figure 22**.

13.3.1 Wastewater Servicing Policies

13.3.1.1 Wastewater Servicing shall be provided by piped utility services.



Figure amended to include Business Commercial lands

13.4 Stormwater

The Plan area is in the Nose Creek Watershed. The post-development discharge rate to Nose Creek of 1.257 L/s/ha will be maintained post-development. A stormwater design approach combining a “low energy release” and a volume target of 35 mm will be utilized to achieve the watershed health objectives. This is further described in the Sub-Catchment Master Drainage Plan (SMDP) prepared by Urban Systems Ltd. and submitted as a supporting document to the Conceptual Scheme, to provide details on a servicing strategy for stormwater. Natural features in the landscape (drainage course, wetlands) will be utilized for stormwater management.

Although a number of wetlands exist within the site, none are significant or slated for preservation in a natural state. An unnamed creek in the southwest corner is claimed by the Crown. Discharges to this creek will follow the same targets as discharges to Nose Creek.

Within the Plan area, runoff will be conveyed to the stormwater facilities through a combination of piped and overland drainage systems. In addition to storm sewer network, vegetated swales and ditches will be utilized for both residential and commercial/industrial land uses.

All stormwater facilities within the Plan area are reconstructed wetlands. A west catchment area is directed to a series of cascading wetlands that ultimately discharge to the unnamed creek in the southwest of the Plan area. An east catchment area is directed to a reconstructed wetland that discharges under Queen Elizabeth II Highway and directly to Nose Creek.

Water quality is provided by a treatment system including lot-level LID/BMP strategies, conveyance via vegetated channels, Oil and Grit Separator (OGS) units, and reconstructed wetland facilities. The treatment system will ensure that current regulatory requirements with respect to quality of stormwater discharges to Nose Creek are met.

13.4.1 Stormwater Policies

- 13.4.1.1 All stormwater management infrastructure shall be designed and constructed in accordance with Rocky View County's Servicing Standards.
- 13.4.1.2 Low-Impact Development (LID) measures should be implemented throughout the development to enhance water quality, reduce volume of stormwater discharges, and support the sustainable nature of the community.



- 13.4.1.3 The stormwater management system should include a treatment train with lot-level source controls, vegetated channels, oil and grit separators, and reconstructed wetlands.
- 13.4.1.4 A stormwater management report, based on the servicing principles stipulated in the Sub-Catchment Master Drainage Plan, shall be required at each subdivision phase of development, to the satisfaction of Rocky View County and Alberta Environment and Parks
- 13.4.1.5 Stormwater infrastructure shall be located within Public Utility Lots and dedicated to Rocky View County at the subdivision stage, to be owned, maintained, and operated by the County.
- 13.4.1.6 Development should follow the policies outlined in the Nose Creek Watershed Water Management Plan.

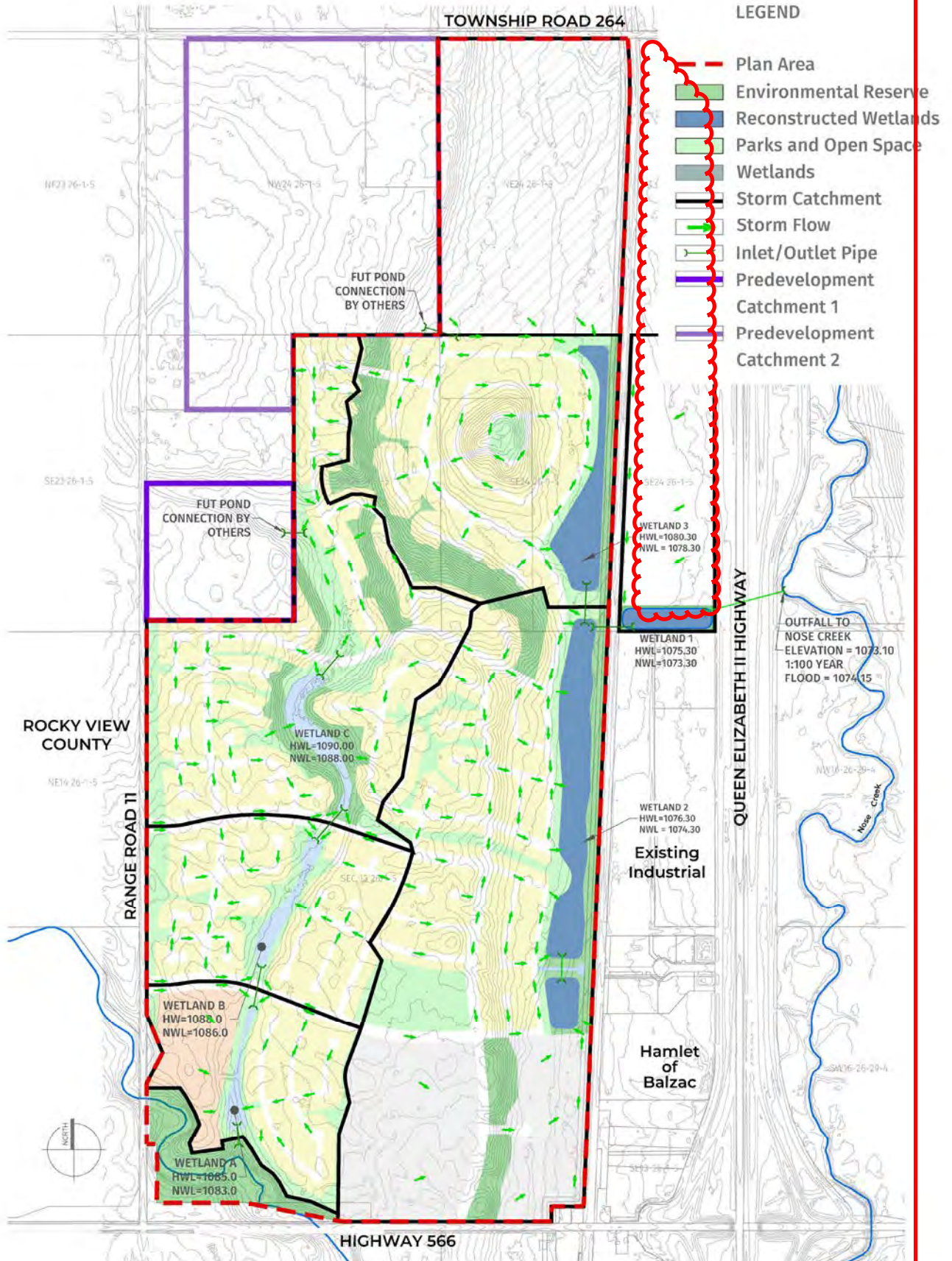


FIGURE 23. Stormwater Servicing

Figure amended to include Business Commercial lands

14

Development Phasing



Phasing of the Rocky Creek Plan area is anticipated to occur in fourteen (14) phases and will generally follow **Figure 24**. The timeline for the ultimate buildout of the community is influenced by several factors such as market conditions, provision of off-site infrastructure, and development costs.

Development will begin in the western portion of the plan, to benefit from the building of the Grand Boulevard from Range Road 11 as the main entry point into the community. The development will then move eastward in logical, sequential phases.

Ultimate phasing will depend on market demand and infrastructure requirements. The logical extension of open space amenities, roads and utilities will progress along with associated phases of development.

14.1.1 Development Phasing Policies

14.1.1.1 Residential, commercial and industrial development, including services and utilities should be developed in phases generally corresponding to the development phases shown in **Figure 24**, depending on market demand and availability of off-site infrastructure.

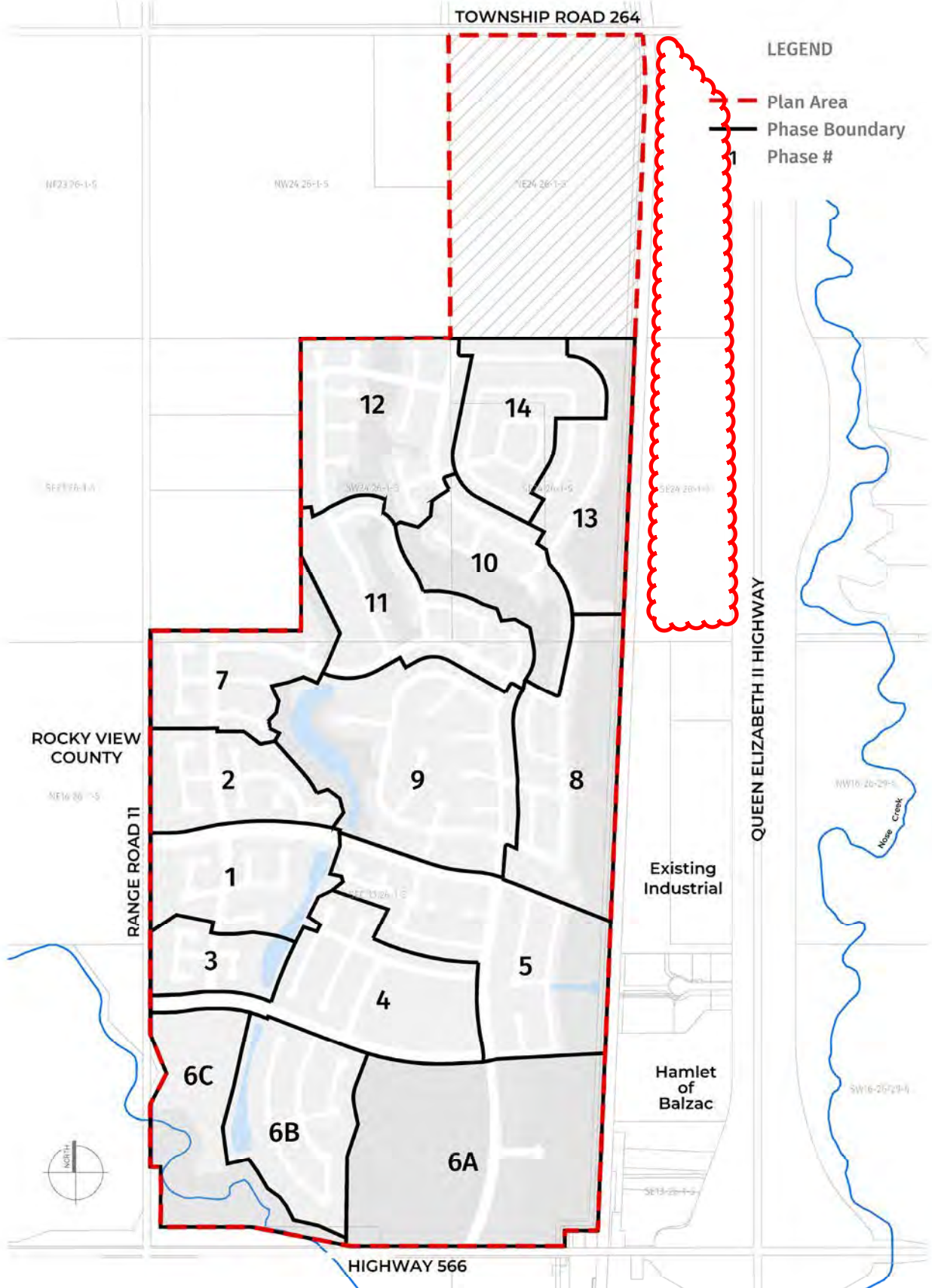


FIGURE 24. Proposed Development Phasing

Figure amended to include Business Commercial lands

15 Implementation

The Rocky Creek Conceptual Scheme is a non-statutory plan adopted by Rocky View County Council to provide policy direction for the development of the subject area. In order to proceed with development of the lands, a successful redesignation must occur to designate the lands with the appropriate land use district in accordance with the Rocky View County Land Use Bylaw.

15.1 Land Use Redesignation

For development of the Rocky Creek community to proceed, land use redesignation application(s) shall be submitted to the County’s satisfaction and should generally align with **Figure 6**. Development Concept. The phasing is an estimation, and each final stage will be established based on market conditions, therefore, the extent of each land use designation application will be determined at that time. Each land use application will be supplemented with a series of technical reports as deemed necessary by the County, addressing items including, but not limited to, traffic, stormwater management, water supply and sanitary sewer.

15.1.1 Land Use Designation Policies

- 15.1.1.1 Future land uses are expected to be assigned within the Plan Area as generally illustrated by **Figure 6**.
- 15.1.1.2 All applications for land use redesignation adjacent to the railway shall be circulated to Canadian Pacific Rail (CPR) for review.

15.2 Subdivision & Development Permits

Subdivision is generally anticipated to proceed in fourteen (14) phases as shown in **Figure 24**. It is possible that each phase may be broken down into one or more sub-phases and the subdivision may proceed in accordance with an alternative phasing pattern to better respond to market demand and infrastructure requirements with no requirement to amend the Conceptual Scheme. The subdivision applications should meet all Rocky View County subdivision application requirements, and are anticipated to include information that addresses elements such as lot configurations, dedication of Reserve(s), architectural guidelines, landscaping design, lighting design, relevant utility rights-of ways, interface design measures, stormwater management and any necessary updates to technical reports (i.e., Transportation Impact Assessment or Sub Catchment Master Drainage plan, etc.).

15.2.1 Subdivision & Development Permits Policies

- 15.2.1.1 All subdivision and/or development permit applications for development adjacent to the railway shall be circulated to Canadian Pacific Rail (CPR) for review.

16 Supporting Technical Studies

The following is a list of technical studies completed in support of the Conceptual Scheme submitted separately, under separate cover.

- Sub-Catchment Master Drainage Plan (SDMP), Urban Systems Ltd., December 2020
- Environmental Desktop Assessment, Urban Systems Ltd., December 2020
- Servicing Design Brief , Urban Systems Ltd., December 2020
- Waterbody Ownership, Boundary Assessment, and Research Program, Stantec Consulting Ltd. August 2017
- Phase 1 Environmental Site Assessment, Base Property Consultants Ltd., September 2010
- Historical Resource Impact Assessment (HRIA), Bison Historical Services Ltd., August 2006
- Preliminary Geotechnical Subsoil Investigation, Almor Engineering Associates Ltd., August 2002

Please note: A Transportation Impact Assessment (TIA) will be submitted in support of this Conceptual Scheme under separate cover.

****NEW CONTENT**

Appendix A

BUSINESS COMMERCIAL



****NEW CONTENT**



Business Commercial



The following is a overview of the lands within the Conceptual Scheme (CS) boundary that are located east of the CPR tracks.

A.1 Site Context

The subject lands are located in West Balzac along Balzac Boulevard and are highly visible from the Queen Elizabeth II Highway Corridor. The subject lands comprise 28.30 ha (69.93 ac) of the CS plan area, with legal description NE 24, 26, 1, W5M and SE24, 26, 1, W5M. The lands are currently designated Agricultural, General District (AG). The lands directly south of the subject site are designated a combination of C-LRD, B-REC and Direct Control Districts generally for commercial and industrial uses.

****NEW CONTENT**

A.2 Policy Review

The subject lands are located within the Queen Elizabeth II Highway Corridor land use policy area within the Balzac West Area Structure Plan. The purpose of the Queen Elizabeth II Highway Corridor is to provide for uses that value direct access and visual exposure to the Queen Elizabeth II Highway. The predominant land uses in the Queen Elizabeth II Highway Corridor policy area shall be commercial, retail, office, and light industrial business park uses that benefit from access to and visibility from the Queen Elizabeth II Highway.

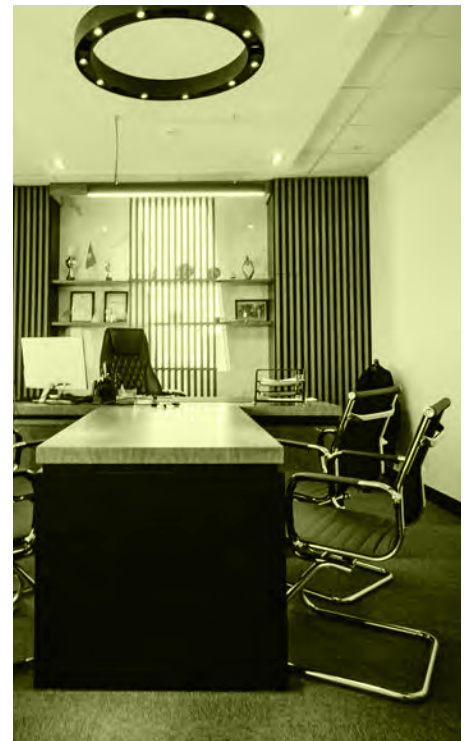
The proposed Business Commercial development types for the subject lands fall directly in line with the description and policy of the Balzac West ASP by accommodating a combination of office, light industrial, and business park development that will benefit from proximity to and visibility from the Queen Elizabeth II Highway.

A.3 Vision

The proposed Business Commercial area is intended to support development of businesses that will provide for the needs of the local community within Rocky Creek, create employment opportunities, and deliver speciality service to the greater region. Business Commercial uses are proposed in this area to take advantage of visibility from the Queen Elizabeth II Highway, existing frontage along Balzac Boulevard, and key transportation routes within the area. This will also result in a continuation of compatible land use and additional commercial and industrial users in the area.

Development will accommodate a combination of office, light industrial, and business park uses and will include a sensitive visual transition between the CPR Railway and open space located in Rocky Creek, west of the tracks.

The subject lands are intended to develop into one, or several smaller parcels containing commercial and light industrial uses which follow the direction provided in the Balzac West ASP.



****NEW CONTENT**



FIGURE A.1. Concept Plan - Business Commercial

****NEW CONTENT**

A.4 Concept

Development of the subject lands may take place as one/ several larger user of the site, or several smaller users depending on market interest. The final size of users/ lots shall be determined at subdivision stage and will be based on market interest and availability of infrastructure.

Figure A.1 shows a potential lotting configuration for the lands, but does not prevent other configurations from being considered.

The Business Commercial land use of the CS is comprised of 28.30 ha (69.93 ac).

A.5 Open Space

At this CS stage, it is anticipated that any reserves owing for the subject lands will be provided by way of cash-in-lieu, however, the dedication of Municipal Reserve for the subject lands must be determined at the subdivision stage. All relevant policies under Section 7.1.1 Open Space Policies of the CS shall apply to the subject lands.

A.6 ~~Interface Design~~

The Canada Pacific Railway as well as significant utility rights of way are located directly west of the subject lands. As a benefit, the CPR tracks also provide a buffer between the subject site and future development to the west. All policies under Section 11.1.1 Interface Design Policies of the CS shall apply to the subject lands.

Interface

(including a future interchange)



A.7 Transportation

As submitted with the CS, the Rocky Creek TIA (ISL Engineering and Land Services, Feb 2021) analyzed traffic generation for the subject lands. As demonstrated in **Figure A.2** Business Commercial Transportation, the subject lands are accessible by Balzac Boulevard which connects the subject lands to major transportation routes including Highway 566, Township Road 264 and Highway 2. Access to the subject site will be from Balzac Boulevard and be similar in nature, form and intensity, to other uses to the south.

If internal roadways are required to facilitate subdivision, an appropriate road cross-section will be selected from those contemplated within the CS. **Figure A.2** Business Commercial Transportation is conceptual in nature, and the ultimate internal road network and access points will be determined at subdivision stage. All relevant policies of Section 12.3.1 Transportation Policies shall apply to the subject lands.

****NEW CONTENT**



FIGURE A.2. Business Commercial Transportation

****NEW CONTENT**

A.8 Servicing

A.8.1 Water

Potable Water servicing to the subject lands will be provided by an extension of the East Balzac water system by Rocky View County, as demonstrated in **Figure A.3** Business Commercial Water Servicing. Capacity for the subject lands was accounted for within the Servicing Design Brief (Urban Systems, Dec 2020) submitted with the CS. Interim servicing may be contemplated in accordance with Rocky View County's Servicing Standards. All relevant policies under Section 13.2.1 Water Servicing Policies of the CS shall apply to the subject lands.

A.8.2 Wastewater

Wastewater servicing for the subject lands will be provided by an extension of the centralized sanitary sewer main system by Rocky View County as demonstrated in **Figure A.4** Business Commercial Wastewater Servicing. Capacity for the subject lands was accounted for within the Servicing Design Brief (Urban Systems, Dec 2020) submitted with the CS. Interim servicing may be contemplated in accordance with Rocky View County's Servicing Standards. All relevant policies under Section 13.3.1 Wastewater Servicing Policies of the CS shall apply to the subject lands.

A.8.3 Stormwater

The Sub-Catchment Master Drainage Plan (Urban Systems, Dec 2020) accounts for the subject lands and includes an integrated system of stormwater collection of management to be constructed to support, and be supported by, the Rocky Creek development. As outlined in Section 13.4 of the CS, and identified in **Figure 23**, Stormwater Servicing, stormwater discharge from the east catchment area (which includes the subject lands) is directed to a reconstructed wetland that discharges under Queen Elizabeth II Highway and directly to Nose Creek. All relevant policies under Section 13.4.1 Stormwater Policies of the CS shall apply to the subject lands.

Wetlands have been delineated within the subject lands for which the intent is to provide compensation and incorporate the lands into the development concept. The approach will follow wetland policy and guidelines and is to be approved by Alberta Environment and Parks.



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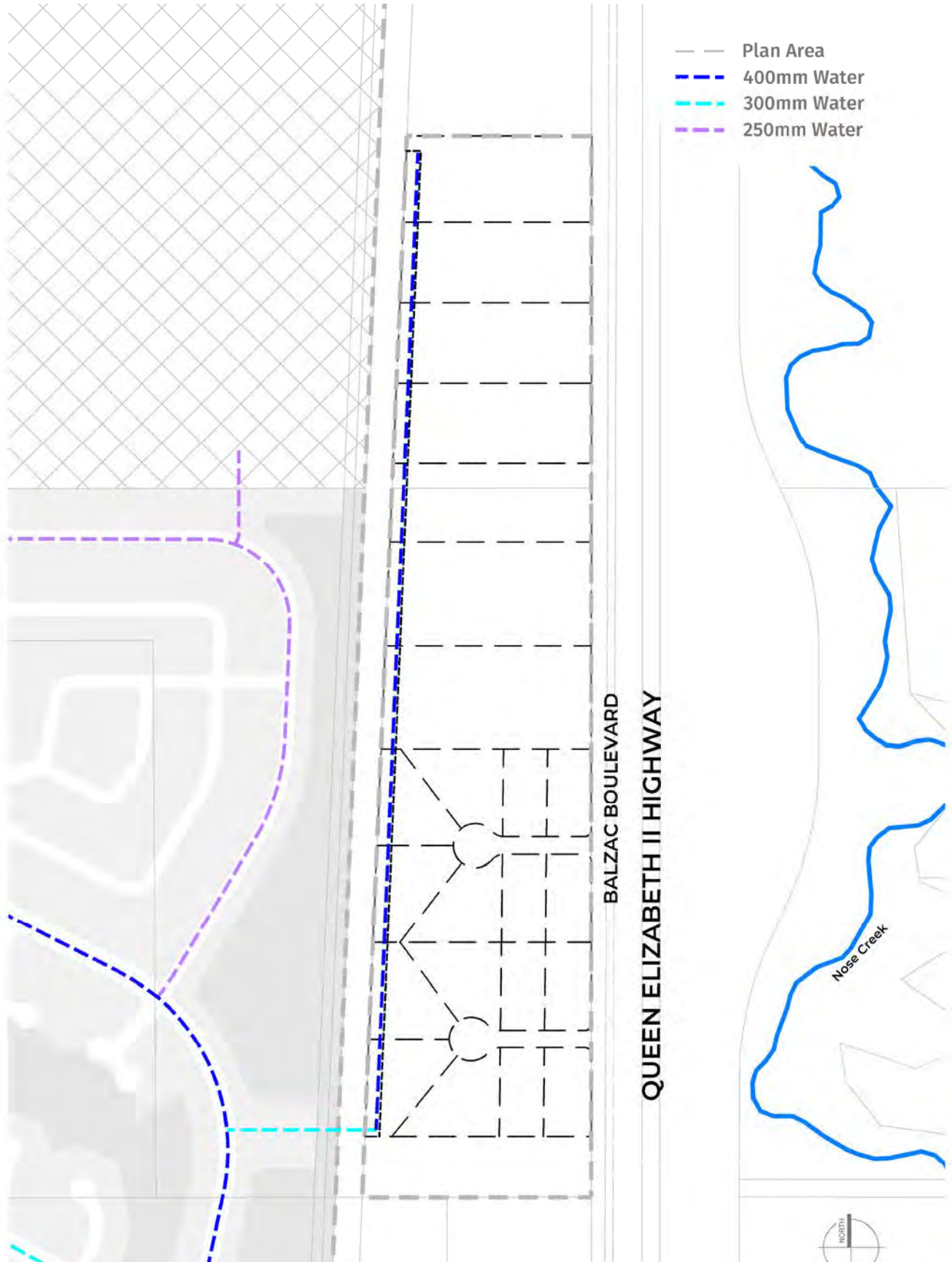


FIGURE A.3. Business Commercial Water Servicing

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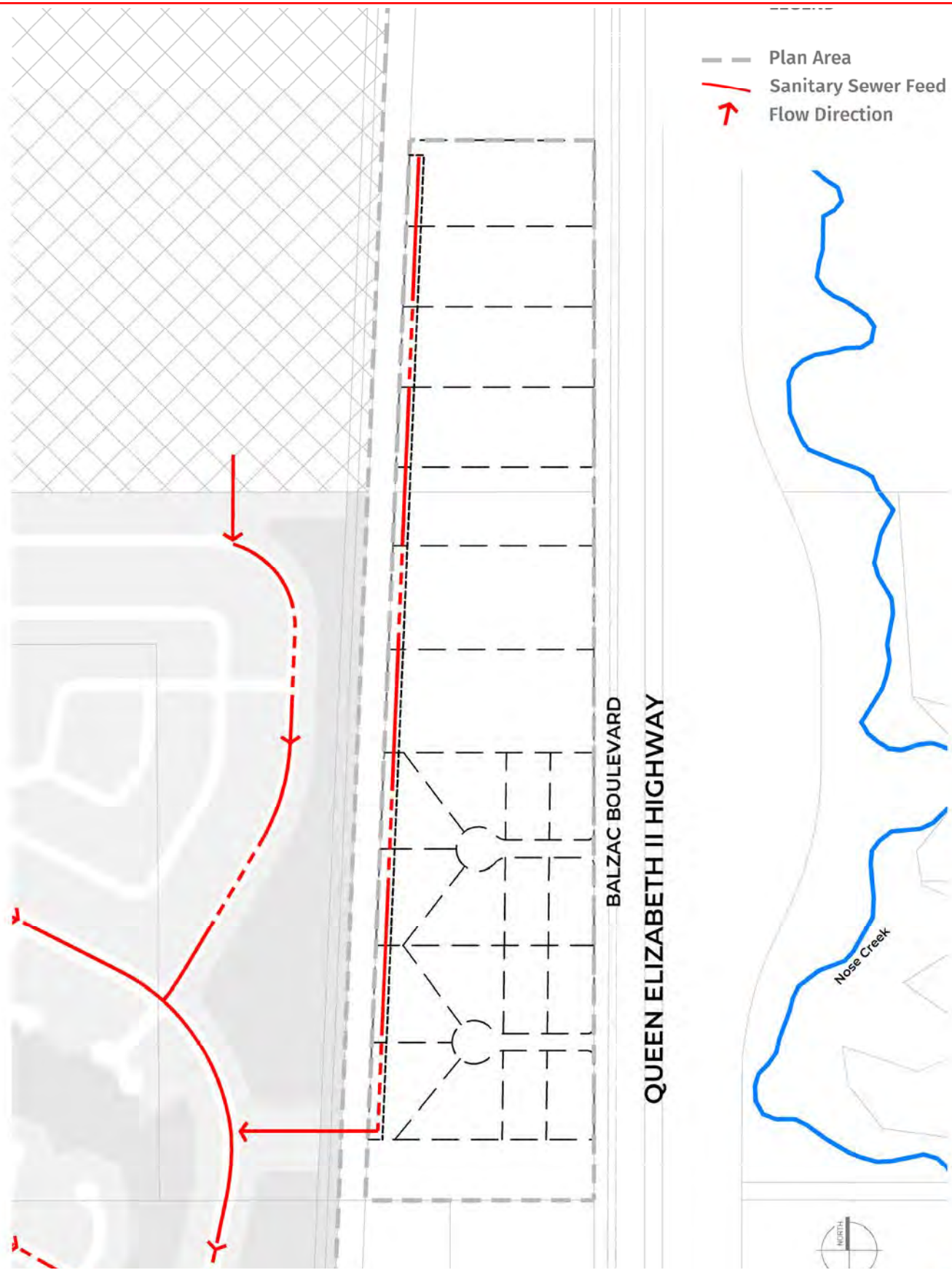


FIGURE A.4. Business Commercial Wastewater Servicing

****NEW CONTENT**

A.9 Development Phasing

Due to the location of ultimate servicing as well as the contiguous development to the south, development will likely progress from south to north; however, due to the possibility of interim servicing, this may influence the phasing sequencing of the subject lands. The ultimate phasing of development will be informed by subsequent subdivision and development permit applications following land use. The subject lands may develop independently of the remainder of the lands in the CS area.

A.10 Implementation

In order to proceed with development of the lands, a successful redesignation must occur to designate the lands with the appropriate land use district in accordance with the Rocky View County Land Use Bylaw.

Following the land use process, subdivision and/or development permit applications may follow which should be consistent with the Balzac West ASP and the Rocky Creek CS. All relevant policies under Sections 15.1.1 Land Use Designation Policies and 15.2.1 Subdivision & Development Permits Policies of the CS shall apply to the subject lands.