



GLENMORE TRAIL **AREA STRUCTURE PLAN**

August 2021

ACKNOWLEDGEMENT

In the spirit of reconciliation, we acknowledge that Rocky View County and its residents live, work, and play on the traditional territories of the Blackfoot Confederacy (Siksika, Kainai, Piikani), the Tsuut'ina, the Îyâxe Nakoda Nations, and on the homeland of the Métis Nation of Alberta Region No. 3.

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PART I:

Introduction

Executive Summary

The Glenmore Trail Area Structure Plan (GT ASP) is a long-term planning document that provides direction for the development of approximately 1000 hectares (2471 acres) of land in central east Rocky View County. The plan area is situated east of the City of Calgary and Janet, adjacent to the southern boundary of the City of Chestermere, and west of the Hamlet of Langdon. This document is a statutory policy to be adopted pursuant to the Alberta Municipal Government Act (MGA), for the purpose of implementing the County vision to accommodate anticipated growth. The proximity of the plan area to significant corridors, such as Range Road 274, Range Road 282, and Glenmore Trail, expands connectivity opportunities within the County for commercial and industrial lands that respond to the overall vision to generate vibrant employment and service areas.

Glenmore Trail will be a vibrant employment and service hub with convenient access to the Calgary region.

Strategic goals have been established to guide this vision, including:

- protection of natural features and environmentally significant areas,
- enhancement of regional corridors,
- development of a vibrant employment hub,
- provision of unique housing options,
- creation of open spaces that augment natural features,
- celebration of rural character, and
- balancing economic viability and flexibility.

These goals have informed the proposed land use pattern for the GT ASP area, which is composed of a range of commercial, industrial, residential, and open space uses. The strategic combination of these elements forms a functional foundation for successful community building in Rocky View County.

Part 1: Introduction

1.0 Introduction

1.1 Plan Organization

The Glenmore Trail Area Structure Plan (the Plan or ASP) is divided into the following parts.

Part I: Introduction

In this part, the ASP outlines the purpose, boundaries, opportunities, constraints, and design ideas of the development, as well as the planning policy context and public engagement process that informed the Plan preparation process. This section also provides an overview of the document structure and key document terms that influence policy application.

Finally, it presents a clear vision and seven broad goals that are intended to guide development in the GT ASP area over the next 30 years.

Part II: Plan Policies

Part II provides the core policy direction of the ASP, which provides more detailed guidance for development and future planning requirements. It lays out a framework for land use, servicing, and infrastructure that bridges higher-level policies and directs the content for future local area plans and applications. Each section contains a description of its purpose and intent, as well as a series of policies addressing the subject matter.

Part III: Implementation

This final part outlines the implementation process and provides information on the processes for future local plan areas and phasing. It also specifies requirements to ensure plan policies and strategies are adhered to and provides direction regarding the process for the review and amendment of the Plan.

Glenmore Trail ASP

For the purposes of brevity, the Glenmore Trail ASP will be referred to as the “GT ASP” or “the Plan” in this document. It is understood that the name ‘Glenmore Trail’ applies to Highway 560 in this area, although this document will refer to it as ‘Glenmore Trail.’

1.2 Plan Purpose

What is an Area Structure Plan?

An Area Structure Plan (ASP) is a planning document that acts as a high-level blueprint for what can take place on a specific area of land. The ASP outlines the vision for the plan area and provides a framework for getting there.

An ASP provides direction for development, which includes proposed land use, transportation, protection of the natural environment, emergency services, general design, and servicing requirements. When making decisions regarding the development of these areas, Council must consider all possible implications and benefits for a wide range of factors, including broader area goals such as economic goals of the County and the ability to adequately service the plan area.

The GT ASP is a statutory document approved by Council and adopted by bylaw; it aligns with the requirements for ASPs as outlined in the Municipal Government Act of Alberta (MGA). The MGA, as per Section 633, states that an ASP must describe:

- the sequence of development proposed for the area;
- the land uses proposed for the area, either generally or with respect to specific parts of the area;
- the density of population proposed for the area either generally or with respect to specific parts of the area;
- the general location of major transportation routes and public utilities;
- may contain other matters the council considers necessary.

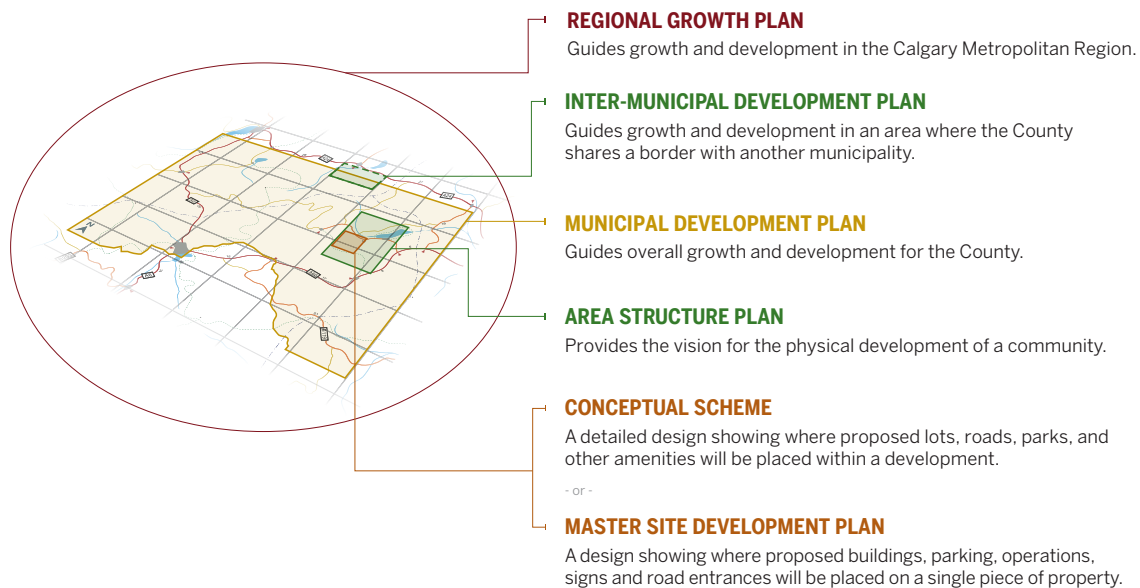


Figure 1. Rocky View County Planning Hierarchy

Local Plans

This document uses the term local plan to refer to a conceptual scheme or master site development. Local plans are developed to demonstrate how development in the plan area will align with the integrity of the overall ASP planning concept and connect to adjacent lands. The County anticipates that most local plans within the GT ASP boundary area will be submitted as conceptual schemes.

The County Plan sets the standard for technical requirements of a local plan. Any additional considerations for development that are specific to the GT ASP are found within the relevant sections of this ASP.

Within the GT ASP, multi-lot subdivisions (subdivisions not recognized as a first parcel out or not exempt from providing municipal reserves under the Municipal Government Act) are expected to submit a local plan as a conceptual scheme. Development that does not propose any subdivision is expected to submit a local plan in the form of a master site development plan.

CONCEPTUAL SCHEME

A conceptual scheme is a non-statutory plan, subordinate to an ASP. It may be adopted either by bylaw or by a resolution of Council. A conceptual scheme is prepared for a smaller area within an ASP boundary and must conform to the policies of the area structure plan. Conceptual schemes provide detailed land use direction, subdivision design, and development guidance to Council, Administration, and the public. If a conceptual scheme area is of sufficient size that further detail is required for specific areas and phases, the conceptual scheme may identify smaller sub-areas and provide detailed guidance at that level. These smaller sub-areas are referred to as development cells.

MASTER SITE DEVELOPMENT PLAN

A master site development plan is a non-statutory plan that is adopted by Council resolution. A master site development plan accompanies a land use redesignation application and provides design guidance for the development of a large area of land with little or no anticipated subdivision. A master site development plan addresses building placement, landscaping, lighting, parking, and architectural treatment. The emphasis of a master site development plan is on-site design with the intent to provide Council and the public with a clear idea of the final appearance of the development.

Interpretation

The following descriptions outline the meaning of key terminology found within the ASP policies:

Shall

A directive term, indicating the actions outlined are mandatory; Administration, the applicant, the Development Authority, and Subdivision Authority must comply without discretion.

Should

A directive term, indicating a strongly preferred course of action by Council, Administration, and/or the applicant; but one that is not mandatory.

May

A discretionary term, meaning the policy in question can be enforced by the County if it chooses to do so, dependent on the circumstances of the site and/or application.

1.3 Plan Area

Location

The GT ASP area comprises approximately 1,000 hectares (2,471 acres) of land in southeast Rocky View County. It is generally located between Range Road 274 and Range Road 282, and on both sides (north and south) of Glenmore Trail (Highway 560). The area is located within the jurisdiction of the Calgary Metropolitan Regional Board (CMRB), which encompasses all Rocky View County. The GT ASP is east of the City of Calgary, west of the Hamlet of Langdon, and south of the City of Chestermere (immediately adjacent to their southern boundary).

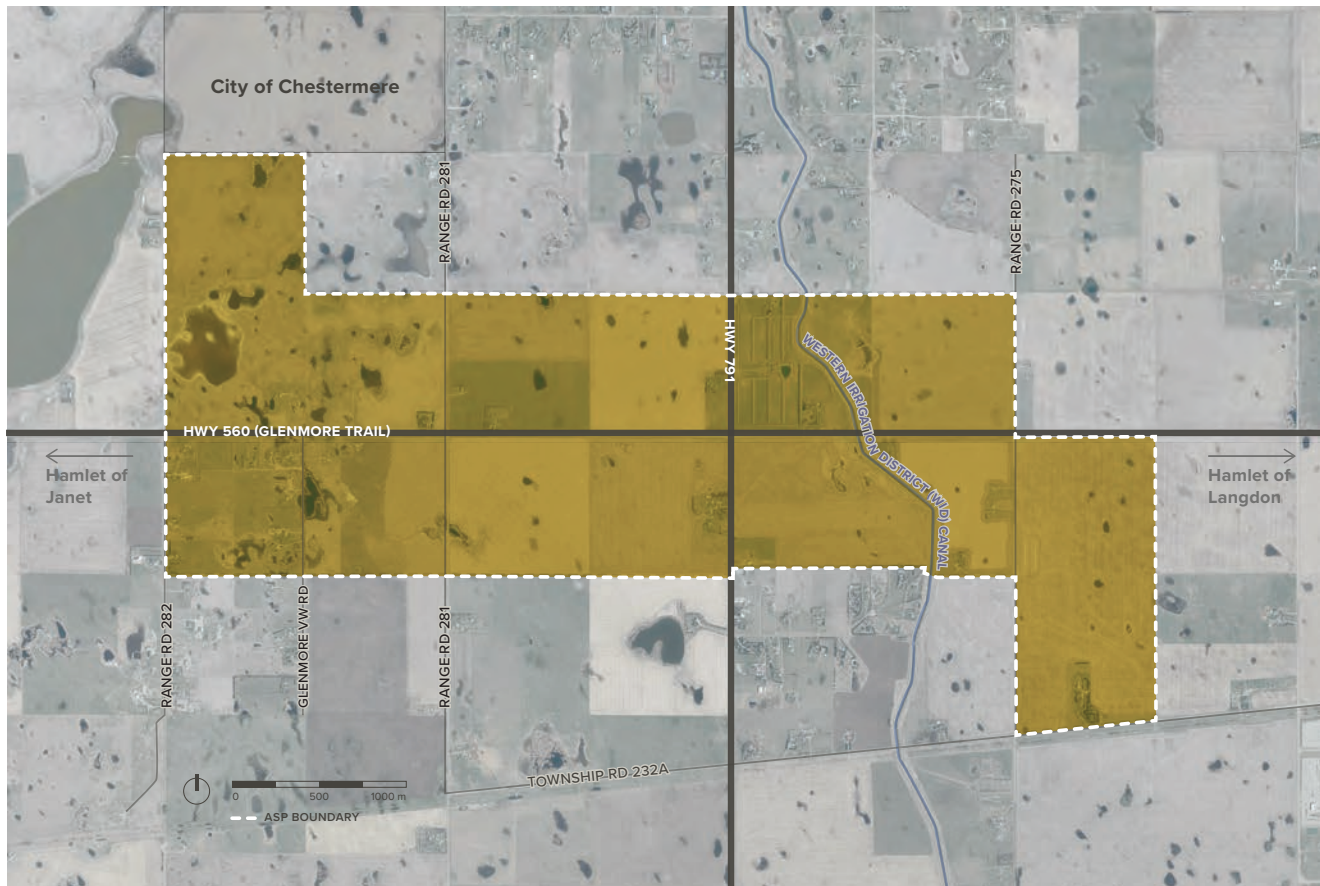


Figure 2. Location Map



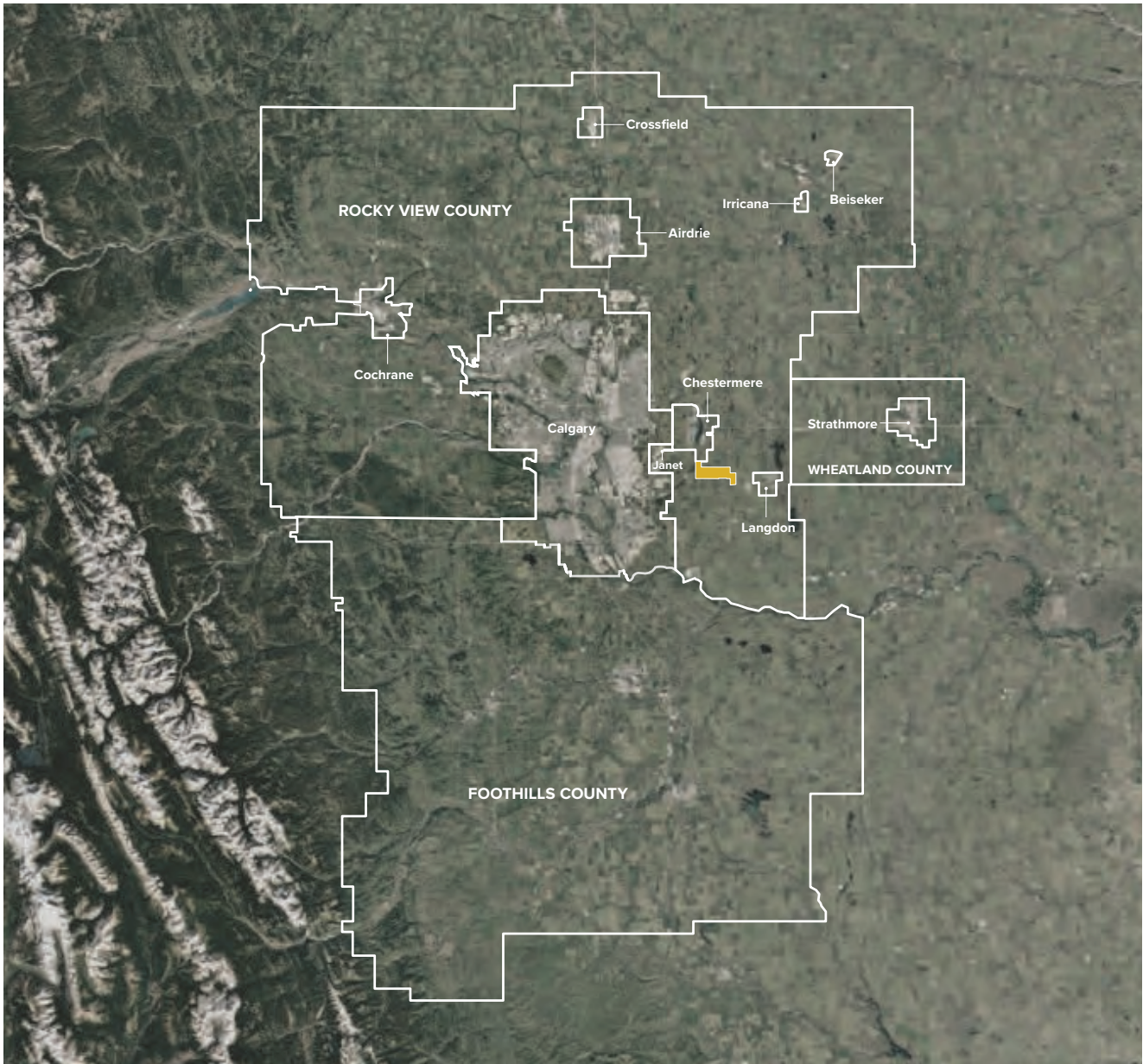


Figure 3. Regional Context Map

 Glenmore Trail ASP Area

Existing Conditions

ENVIRONMENTAL

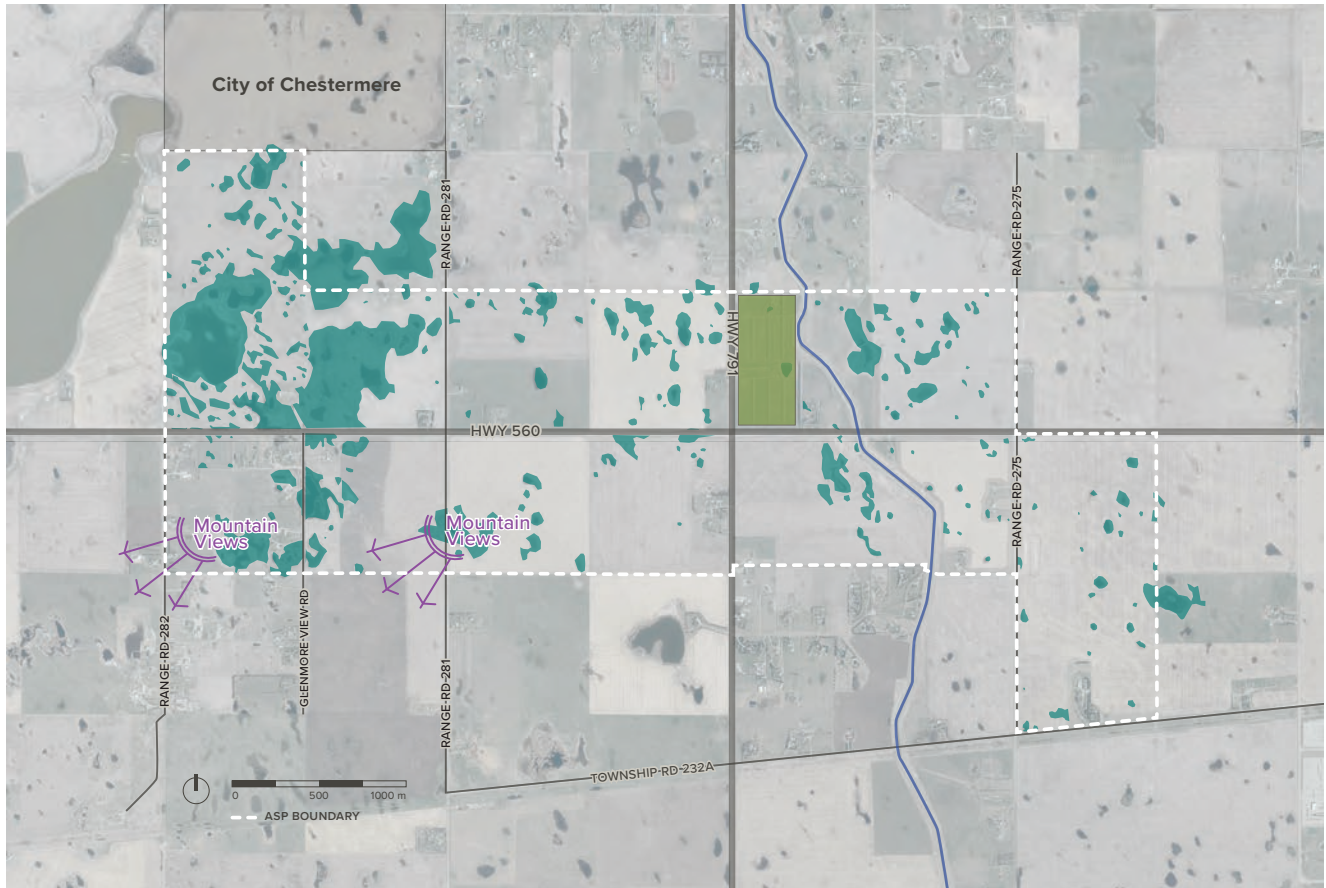


Figure 4. Existing Site Conditions

- Wetlands
- Irrigation Canal
- Vegetation
- Highway
- Road

Most of the ASP area is currently cultivated cropland with wetlands intermittently dispersed throughout. The western portion has a higher density of wetland areas and intact grassland. These existing conditions are demonstrated in *Figure 4. Existing Site Conditions*.

LAND USE



Figure 5. Existing Land Use

- Agriculture, General District
- Agriculture, Small Parcel District
- Agriculture, Small Parcel District (p8.1)
- Business, Agriculture District
- Business, Live-Work District
- Residential, County Residential District
- Residential, Rural District
- Residential, Rural District (p4.0)
- Special, Public Service District

The lands are currently used primarily for agricultural purposes, designated as “Agricultural, General District” with several pockets of country residential, rural businesses, or farmsteads. The current land uses designated in the plan area are demonstrated in *Figure 5. Existing Land Use*.

TRANSPORTATION

The lands are currently accessible from two provincial highways: Glenmore Trail (Highway 560), which runs east-west through the centre of the area, and Highway 791, which runs north-south at the centre of ASP. There is a Western Irrigation District (WID) Canal that runs generally north-south to the east of Highway 791. Various Range Roads run north-south along quarter section lines throughout the ASP area.

Area History

Development history within the plan area is tied to the introduction of the Canadian Pacific Railway (CPR) and the Western Irrigation District (WID) canal (Langdon Chamber of Commerce, n.d.).

Across Canada, in this region, the presence of the CPR enabled economic viability and led to the settlement and formation of rural communities. During this time, the Hamlet of Langdon and the City of Chestermere originated as small cottage associations. The construction of the WID canal encouraged additional settlement within the area through the creation of Chestermere Lake, ultimately leading to the establishment of a permanent municipality (City of Chestermere, n.d.).

1.4 Policy Direction

The GT ASP provides detailed planning direction for lands within the plan area that is consistent with provincial, regional, and County priorities and policies. An overview of the key policies that informed the preparation of the GT ASP are outlined below and identified on *Figure 6. Regional Policy Context Map*.

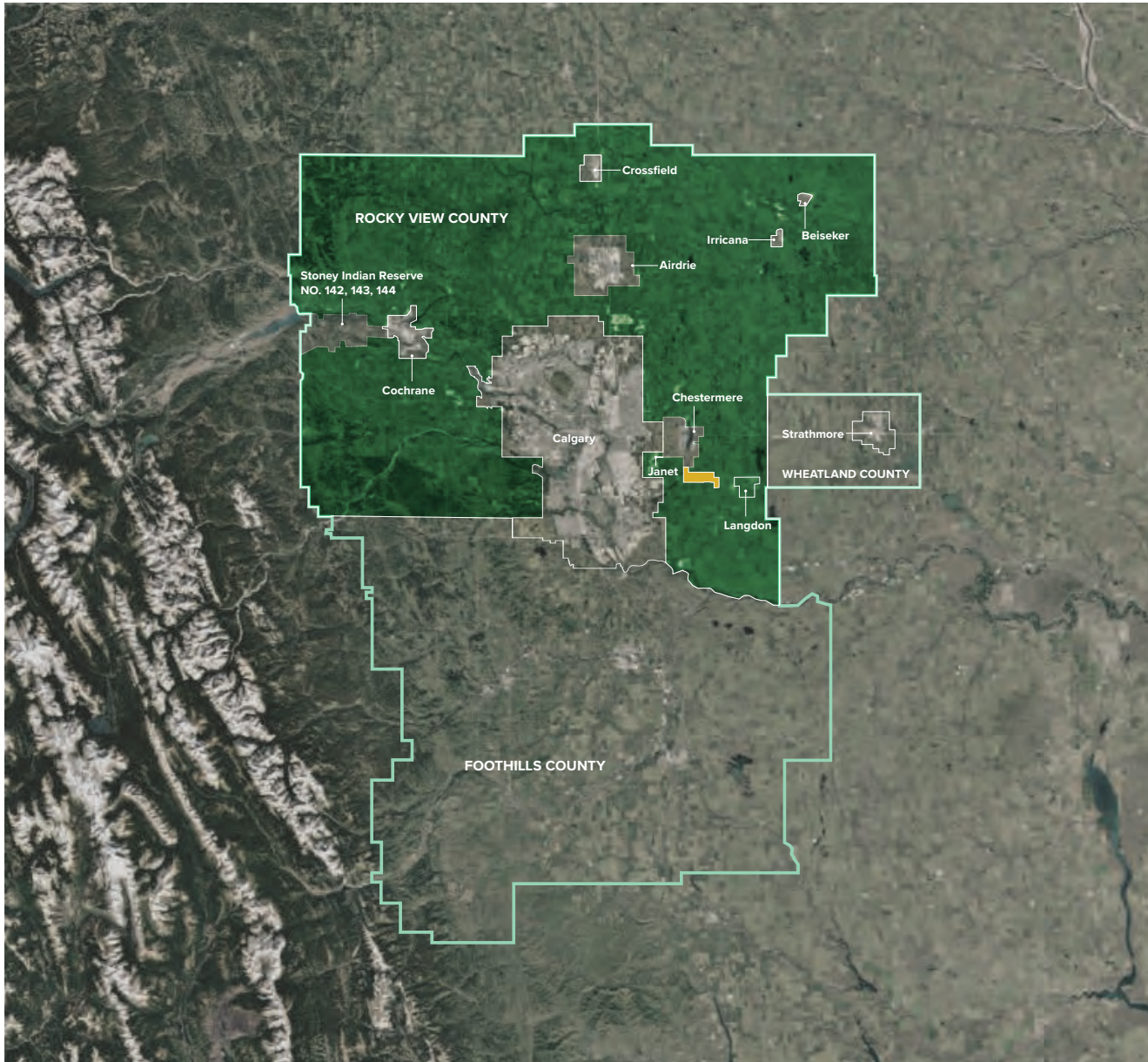





Figure 6. Regional Policy Context Map

-  Glenmore Trail Area Structure Plan
-  Rocky View County
-  Calgary Metropolitan Regional Board Interim Growth Plan

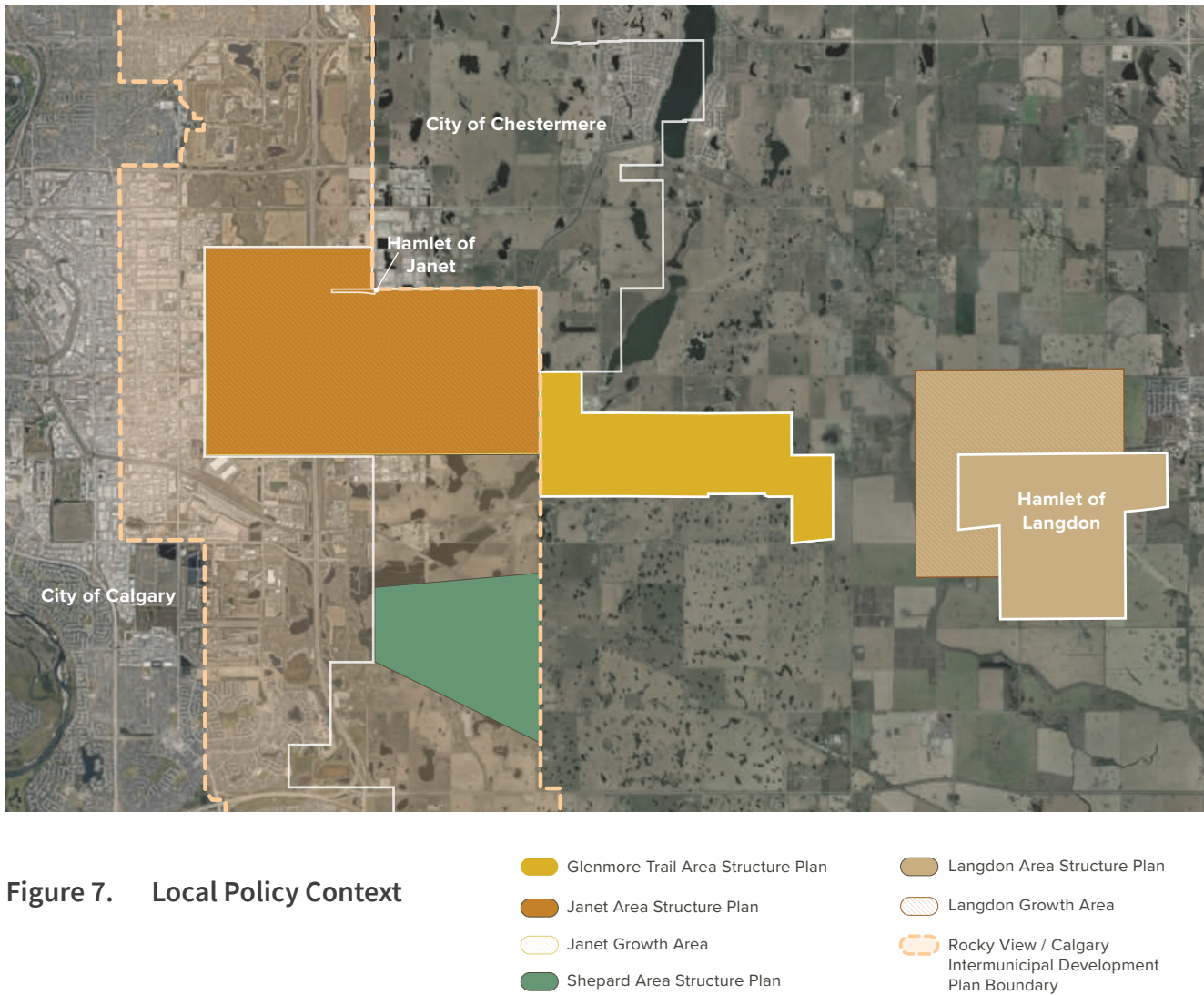


Figure 7. Local Policy Context

Municipal Government Act (MGA)

Section 633 of the Municipal Government Act (MGA) authorizes municipalities to establish and adopt ASPs to provide direction for land subdivision and development. The MGA outlines specific requirements of an ASP, including description of proposed land uses, population densities, general location of transportation and public amenities, and development phasing. An ASP document is not limited to these requirements and must consider other matters deemed necessary by Council, while remaining in alignment with intermunicipal and municipal development plans.

As stated in Section 633 of the MGA, Rocky View’s County Plan outlines other matters that Council considers necessary for ASPs and reflects the County’s terms of reference for the GT ASP requirements and formatting. The GT ASP meets all requirements and formatting standards established in Rocky View’s County Plan.

South Saskatchewan Regional Plan (SSRP), 2014

The South Saskatchewan Regional Plan (SSRP) is a high-level strategic planning document applicable to lands in the South Saskatchewan Region. Rocky View County is located within the South Saskatchewan Region, as such, SSRP policies and objectives are applicable to the GT ASP, including strategies related to land-use patterns, agriculture, water and watersheds, non-renewable resources, historic resources, and transportation.

Through its policy and design considerations, the GT ASP has anticipated the community development needs of Rocky View County and drafted a statutory plan in alignment with the policies and objectives outlined in the SSRP.

Calgary Metropolitan Region Board Interim

Growth Plan (IGP), 2018

In 2018, The Calgary Metropolitan Region Board (CMRB) was formed to manage the strategic planning for future growth in the Calgary Metropolitan Region.

The CMRB is comprised of 10 member municipalities; members worked collaboratively to produce the Interim Growth Plan (IGP) in 2018. The IGP prioritizes 3 Principles: efficient use of regional infrastructure usage, water quality and conservation practices, and efficient growth and sustainable communities.

All new statutory plans must demonstrate to the CMRB alignment with the IGP Principles using the Interim Regional Evaluation Framework (IREF). All aspects of the GT ASP have considered and integrated the 3 Principles of the IGP and their subsequent objectives into the planning, infrastructure, and service delivery.

Development proposed as part of the GT ASP is defined within the IGP under three categories: a new freestanding settlement area (3.4.3); employment area (3.4.5) and Mobility Corridor (3.5.1).

As outlined by the IGP, a new freestanding settlement area is for new residential and/or mixed-use growth and development that is not contiguous to existing settlement areas. The GT ASP is in alignment with the new freestanding settlement area development type policies outlined in the Section 3.4.3.1; and the additional development type policies outlined in Section 3.4.3.2 for new freestanding settlement areas with 500 or greater dwelling units. In addition, the GT ASP is alignment with employment area policies 3.4.5.1 and 3.4.5.2; and mobility corridors policies 3.5.1.1.

Rocky View County and City of Calgary Intermunicipal Development Plan (IDP), 2012

The City of Calgary-Rocky View County Intermunicipal Development Plan (IDP) structures the foundation for a continued cooperative relationship between the bordering areas. The policy has identified the Glenmore Trail as a significant growth corridor, which is in alignment with the GT ASP. Promoting this vision will contribute to the integration of integral community, recreational, and commercial amenities in existing approved development areas and the broader community.

The GT ASP plan area is not specifically located within the IDP policy area. However, the ASP borders the identified City of Calgary industrial growth areas in Janet, as identified in Map 4 of the IDP. The development of this area will contribute

to the GT ASP infrastructure servicing feasibility and will inform potential growth along key corridors.

ROCKY VIEW COUNTY AND CITY OF CHESTERMERE INTERMUNICIPAL BOUNDARY

The City of Chestermere and Rocky View County share boundaries and follow internal processes to ensure appropriate notification, but do not have a formal intermunicipal development plan (IDP) in place to manage growth between the two municipalities. However, the County Plan outlines a guide for coordination between adjacent areas, as required by the Municipal Government Act (MGA). Section 27.10 of the County Plan identifies that planning related matters within 1.6 kilometres of an adjacent municipality shall consult the bordering jurisdiction for comments. This adjacent area, referred to as the notification zone, concerns a variety of items, including area structure plans (ASP's).

Rocky View County - County Plan, 2013 & Municipal Development Plan, 2021

The County Plan provides Rocky View with an overall policy framework for future development and growth. The policy concerns a variety of matters, including the development of residential and commercial areas, to the supply of emergency services and infrastructure. The objective of the County Plan is to direct efficient land use patterns through defined growth areas, and the conservation of agricultural uses in remaining large land parcels. The extension of existing growth areas identified in the County Plan include the GT ASP area, along the Glenmore Trail corridor .

A new Municipal Development Plan (MDP) was being prepared as the GT ASP was being drafted. Consultation with County staff has identified that the Glenmore Trail corridor, and specifically the GT ASP area, has been considered for inclusion in the new Plan as defined growth areas. In particular, the GT ASP is in the proposed employment area and the future planning area, as showcased in Figure 2 and 3 of the draft MDP.

Acknowledging the timing overlap of the approvals processes for the new MDP and the GT ASP, this ASP has been prepared in alignment with the existing County Plan with consideration for the anticipated future growth policies of the new MDP. If the new MDP is not adopted at such time as the GT ASP is before Council, the GT ASP will be assessed per the existing County Plan.

Rocky View 2060 Growth Management Strategy, 2009

The Rocky View 2060 Growth Management Strategy outlines strategic growth recommendations for the County. The strategy focuses on sustainability and smart growth by using a “triple-bottom-line” evaluation approach, to establish a foundation to manage future development. Existing rural communities, including Hamlet of Langdon, are recognized as viable areas for growth where appropriate, especially along the Glenmore Trail corridor. The development of the GT ASP area reflects the future growth strategy, as the Glenmore Trail intersects the plan area, representing a logical growth location along the corridor. Servicing infrastructure will be extended from the adjacent Hamlet of Langdon to support community expansion in this area.

Surrounding Area Structure Plans

JANET AREA STRUCTURE PLAN, 2014

The Janet Area Structure Plan (ASP) is a statutory policy that provides a long-term strategy for land use and servicing. The purpose of the Janet ASP is to establish a relevant framework that reflects the requirements of the MGA and the vision of the County Plan, including the development of a Regional Business Centre and strategic use of key transportation corridors to promote connectivity with bordering areas. Much of the current land area within the Janet ASP consists of commercial business and limited-service industrial. The GT ASP is adjacent to the Janet ASP industrial area and will be considered as a transitional interface. The newly proposed Janet ASP is currently in the draft stages and is intended to replace the existing policy adopted in 2014. The new ASP is consistent with the CMRB Interim Growth Plan and existing County policy.

SHEPARD INDUSTRIAL AREA STRUCTURE PLAN

The Shepard Industrial Area Structure Plan (ASP) is a statutory policy currently being prepared and undergoing consultation. The purpose of the Shepard ASP is to create a guide for future development within the area, inclusive of land use, transportation, natural environment, emergency services, and infrastructure servicing requirements. The plan area is located within Rocky View County, immediately adjacent to the eastern edge of the City of Calgary. This locational relationship has required the consideration of many other factors, including the objectives of the MDP and other local plans. The Shepard Industrial ASP Plan boundary is southwest of the GT ASP Plan area, and due to this proximity, consideration may be made for connecting utility services and transportation corridors such as Range Road 282.

Rocky View County Land Use Bylaw, 2020

Rocky View County's Land Use Bylaw C-8000-2020 (LUB) regulates the land development process within the County. “Land use districts” are used to designate land areas and establish appropriate uses or standards. The undeveloped lands outlined in the GT ASP are primarily designated as “Agricultural, General District” (A-GEN), a land use district that accommodates existing agricultural activities and focuses on continuing rural primary uses. The GT ASP will propose a general, high-level framework of land use in accordance with the Land Use Bylaw C-8000-2020 for future redesignations that will occur after the preparation of local area plans.

1.5 Engagement Strategy

Approach

An Engagement Strategy was drafted to provide a roadmap for engagement throughout the lifecycle of the GT ASP. The Engagement Strategy was guided by the Approved Terms of Reference (TOC), and Rocky View's commitment to inclusive and meaningful public participation.

It is important to note that the GT ASP was developed during the COVID-19 pandemic. To do our part, and respect public health and safety requirements, engagement and communication for the project was conducted using distanced techniques, including posting project information and updates on the County's website, distributing mailouts to landowners within and surrounding the plan area, hosting surveys, and attending meetings with landowners, stakeholders, County staff, and the project team.

Engagement is separated into two phases.

Engagement Phase 1 between April 30, 2021 – May 21, 2021. The purpose of Phase 1 is two-fold: to introduce the GT ASP project; and, to provide opportunities for stakeholders and the public to review the draft vision, goals, land use and transportation concepts.

Engagement Phase 2 between June 2021 – July 2021. The purpose of Phase 2 was three-fold: to outline how public input influenced the refinement of the draft ASP, to provide a copy of the draft ASP for review, and to seek public and stakeholder feedback on the proposed Plan, including policy direction and mapping.

What We Heard

Following each engagement phase, a What We Heard report will be prepared to summarize the feedback received. The reports will be appended to the ASP before it is approved by Council.

1

Phase 1 - Key Findings

[Insert number of survey respondents + key WWHR findings]

Please refer to **What We Heard Report #1** for additional details regarding the engagement process and key findings.

2

Phase 2 - Key Findings

[Insert number of survey respondents + key WWHR findings]

Please refer to **What We Heard Report #2** for additional details regarding the engagement process and key findings.

1.6 Planning for Tomorrow: Vision and Goals

Vision

Taking advantage of key transportation routes within the area, GT ASP will support high-quality highway development and logical expansion of industrial employment areas. GT ASP will be designed with a trail network and open spaces that thoughtfully integrate with residential communities, celebrate the area's rural character, and preserve its rich regional connection to the Western Irrigation District Canal.

The Glenmore Trail ASP area will be a vibrant employment and service hub with convenient access to the Calgary region.

Goals

The goals that guide the development of the GT ASP are based on several factors:

- policy direction of the Calgary Metropolitan Region Board Interim Growth Plan, and the Rocky View County Municipal Development Plan
- the existing physical characteristics of the area; and
- the key issues, constraints, and opportunities identified during the planning process.



DEVELOP A VIBRANT EMPLOYMENT HUB.

The GT ASP will provide commercial and industrial opportunities that vary in configuration and size. This ensures the plan area can adapt to market demands that evolve over time, provide high-quality development, and encourage ongoing public and private investment.



ENHANCE REGIONAL CONNECTIONS AND LOCAL MOBILITY.

With proximity to Stoney Trail, Chestermere, and Langdon, development of the GT ASP will help create connections between key regional destinations, maximizing the potential for industrial growth along Highways 560 and 791. The ASP design will direct traffic efficiently through the plan area, with a thoughtfully-designed local road system and opportunities to support multi-modal connections.



PROVIDE UNIQUE, RURAL-CHARACTER HOUSING OPTIONS.

The plan area will be designed to provide a range of housing options in neighbourhoods with convenient access to the surrounding region. Housing types with larger lot sizes will target a niche market for homes that balance rural character with a strong sense of community.



BUILD ON NATURAL FEATURES TO OFFER A VARIETY OF OPEN SPACES.

This ASP supports a mix of open spaces that will offer natural amenity, convenient passive and active recreation opportunities, and a green buffer where necessary to minimize noise and visual impacts to residential communities and the surrounding prairie landscape.



CELEBRATE RURAL CHARACTER.

The GT ASP will celebrate its scenic prairie landscape by thoughtfully designing the area to foster a distinctive community identity that is tied to the land's rich agricultural history.



PROTECT NATURALLY SIGNIFICANT AREAS.

The GT ASP will protect the major wetlands as local habitat and where appropriate, carefully integrate natural areas into the proposed open space network and drainage infrastructure.



BALANCE ECONOMIC VIABILITY AND FLEXIBILITY.

Development in the ASP area will be designed to ensure that the new community is economically viable and will develop sustainably. Central to this goal is the development of appropriate land uses, phasing, and policies that are flexible enough to allow development to evolve if or when the development context changes, but also specific enough to ensure a sustainable and consistent build-out that remains in alignment with the ASP's vision and goals.

PART II:

Plan Policies

2.0 Land Use Strategy

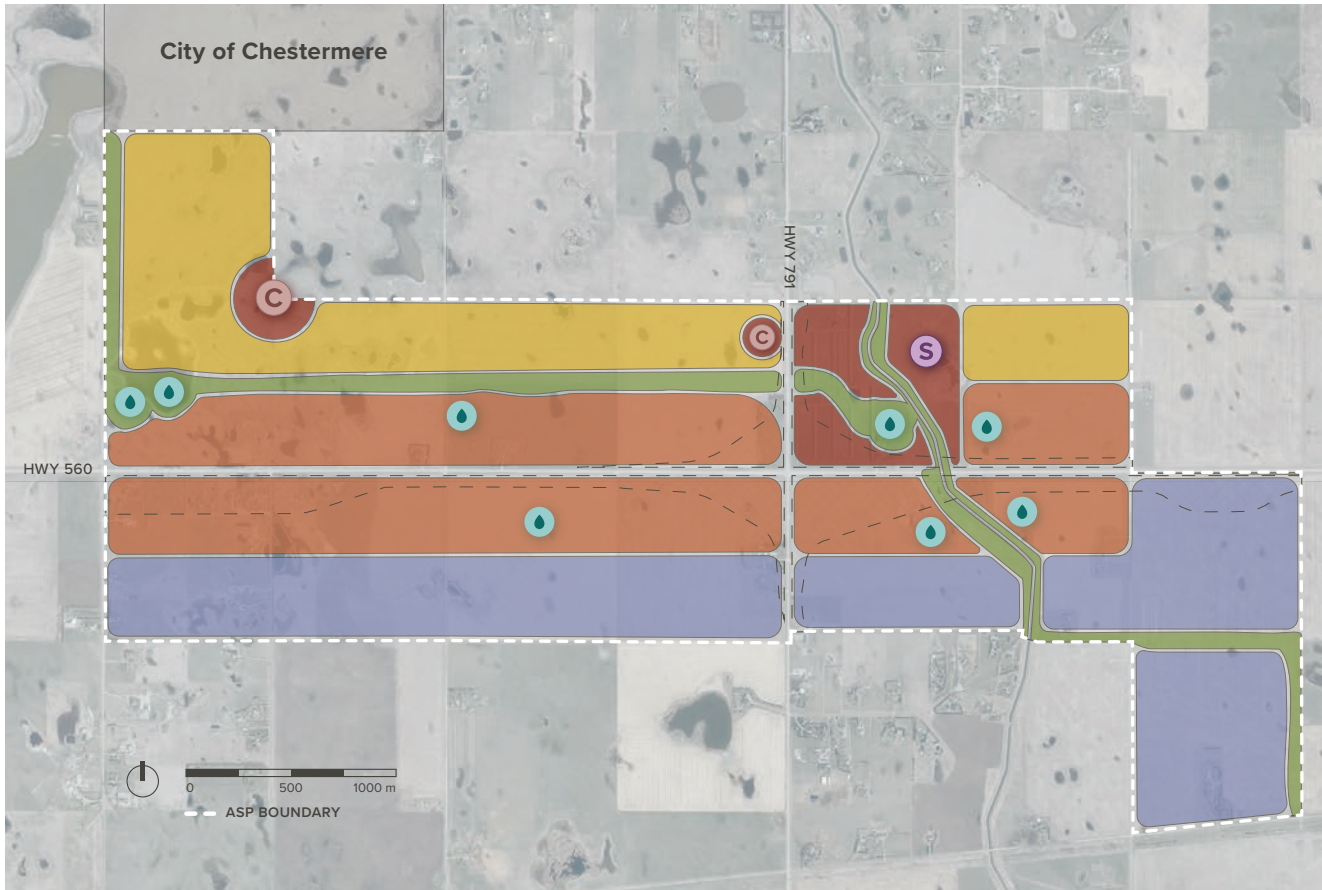


Figure 8. Land Use Strategy

- | | |
|---------------------------|---|
| Residential | Regional Stormwater Facilities & Wetlands |
| Commercial | Local Commercial Hub |
| Business Industrial | Service & Hospitality Hub |
| Light / Medium Industrial | Future Highway Right of Way |
| Parks and Open Space | |

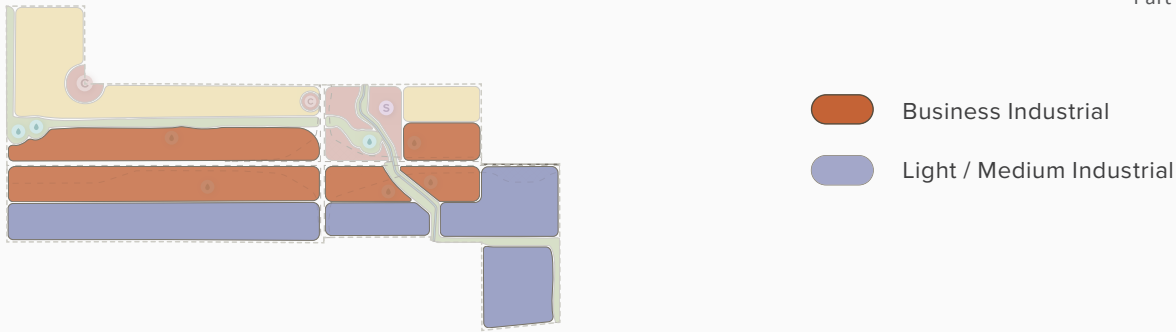
The land use strategy for the GT ASP outlines a framework for the community’s composition and spatial structure. The land uses shown on *Figure 7. Land Use Strategy* will guide the development of this multi-use ASP. The organization of land uses is intended to create an accessible and vibrant development that balances the variety of uses within the plan area; in particular, emphasis is placed on creating sensitive interfaces between the various uses, as well as between the Plan’s industrial uses and external agricultural or residential uses.

Part 2: Plan Policies

2.1 Land Use Statistics

| | <i>HA</i> | <i>AC</i> | <i>%</i> |
|--------------------------------|-------------|-------------|------------|
| Gross Area | 1000 | 2471 | 100 |
| Residential | 145 | 358 | 15 |
| Business | 184 | 455 | 18 |
| Local Commercial | 10 | 25 | 1 |
| Service/Hospitality Commercial | 30 | 74 | 3 |
| Light/Medium Industrial | 210 | 519 | 21 |
| Parks and Open Space | 100 | 247 | 10 |
| Regional Stormwater Facilities | 21 | 52 | 2 |
| Roads | 300 | 741 | 30 |
| | 1000 | 2471 | 100 |

** All numbers in the table are subject to rounding.*



2.2 Employment Lands

GOAL ALIGNMENT



Develop a vibrant employment hub.



Balance economic viability and flexibility.



Enhance regional connections and local mobility.

General

As a part of the County that is designated as an Employment Area and targeted for growth in the near-term, the industrial areas of the Glenmore Trail ASP will establish employment opportunities in proximity to the growing hamlet of Langdon, Chestermere, and Calgary; the area's location along a key east-west transportation route will help a variety of industrial uses emerge and grow to meet demand from these residential and commercial centres.

The ASP plans for two types of industrial: 1) business industrial and 2) light/medium industrial. Each will offer unique benefits for the region and the County as a whole and create diverse employment opportunities.

POLICIES

- 2.2.1 *Figure 7. Land Use Strategy* shall guide the general location of employment lands within the ASP area.
- 2.2.2 All employment lands must be contained within a local plan.
- 2.2.3 Local plans must be a minimum of 80 acres (32.4 hectares) if they contain only employment lands.
- 2.2.4 Local plans shall define the exact boundaries of employment lands.
- 2.2.5 At the local plan stage, measures should be put in place to ensure a high quality of design and that visual impacts to a provincial highway are managed for employment lands.
- 2.2.6 All development within employment lands should follow the County's Commercial, Office and Industrial Design Guidelines.
- 2.2.7 Residential uses shall not be permitted within employment lands unless ancillary to a business or industrial use and for security purposes.
- 2.2.8 Interim uses in employment lands shall be limited to agricultural or employment uses that limit potential impacts on long-term developability. Non-agricultural uses shall be screened from all provincial highways.
- 2.2.9 If and when regional transit services are available, development in employment lands should be oriented to support and benefit from these future transportation networks.

Part 2: Plan Policies

Business Industrial

Business Industrial uses are located along the Glenmore Trail corridor to take advantage of access points and key transportation routes within the area. Development will accommodate a combination of office, industrial, and business park types that will include a sensitive transition between business industrial development and residential uses north of Glenmore Trail, and larger lot industrial activity south of Glenmore Trail. Development may contain uses such as business parks or commercial campus-style development.

Typical attributes of this type of development are:

- greater office space requirements,
- client and employee parking,
- exposure to the highway network,
- smaller outdoor storage needs, and
- comprehensive landscaping and site circulation.

POLICIES

- 2.2.10 Industrial uses that have the potential to create site nuisances (including noise, smell, sound, or unattractive effects) should be discouraged within business industrial areas.
- 2.2.11 Local area plans should consider high-quality design that promotes a rural aesthetic in business industrial areas through the following elements:
- building height and orientation,
 - ease of access through clear wayfinding and signage,
 - comprehensive landscaping design,
 - visibility from the highways, and
 - gateway and entrance features.

Light/ Medium Industrial

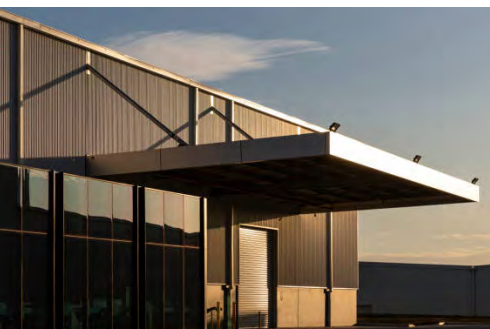
Light/Medium Industrial uses are located in the southern portion of the Plan. Development in this part of the Plan will provide for industrial uses that require larger lots and industrial activity without any significant nuisance such as noise, appearance, or odour, extending beyond the boundaries of the site. Development may contain uses such as logistics facilities, industrial manufacturing and assembly, and resource/construction facilities.

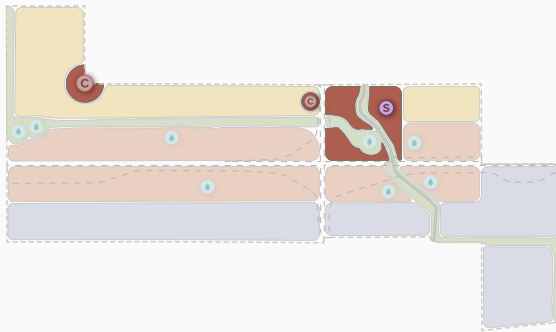
Typical attributes of this type of development are:




- greater need for indoor manufacturing/storage space,
- indoor and outdoor equipment storage,
- larger parcel sizes,
- buffers between non-industrial uses, and
- heavy or large vehicle access.

POLICIES

- 2.2.12 Developments in light/medium industrial areas that may have an effect on the safety, use, amenity, or enjoyment of adjacent or nearby sites due to appearance, noise, odour, emission of contaminants, fire or explosive hazards, or dangerous goods should be discouraged.
- 2.2.13 Access to light/medium industrial developments should, where possible, be as direct and separate from other uses.





-  Commercial
-  Service and Hospitality Hub
-  Local Commercial Hub

2.3 Commercial Areas

GOAL ALIGNMENT



Develop a vibrant employment hub.



Balance economic viability and flexibility.



Celebrate rural character.

Commercial development is located at the intersection of Glenmore Trail and 791, and in the northwestern portion of the plan area, south of the City of Chestermere. These commercial areas will provide a destination for commercial opportunities, offer services to the traveling public, and create local or regional employment options for residents in the Glenmore Trail area and in neighbouring communities.

POLICIES

- 2.3.1 *Figure 7. Land Use Strategy* shall guide the general location of commercial development within the ASP area.
- 2.3.2 All commercial areas must be contained within a local plan.
- 2.3.3 Local plans shall define the boundaries of commercial areas.
- 2.3.4 At the local plan stage, measures should be put in place to ensure a high quality of design and that visual impacts to a provincial highway are managed for commercial areas.
- 2.3.5 All development within commercial areas should follow the County's Commercial, Office and Industrial Design Guidelines.
- 2.3.6 Interim uses in commercial areas shall be limited to agricultural or commercial uses that limit potential impacts on long-term developability. Non-agricultural uses shall be screened from all provincial highways.
- 2.3.7 Access to and egress from commercial areas shall be carefully planned to promote safety for all modes of travel, especially considering connections to provincial highways.
- 2.3.8 If and when regional transit services are available, development in commercial areas should be oriented to support and benefit from these future transportation networks.



Service and Hospitality Hub

GOAL ALIGNMENT



Develop a vibrant employment hub.

A Service and Hospitality Hub will build on the area's natural prairie landscape and its proximity to the Western Irrigation District (WID) Canal to offer a unique location for destination service and hospitality business opportunities (e.g., wedding venue, conference/retreat centre location) that highlights the area's natural amenity value.

Development may contain uses such as hospitality services, special function businesses (wedding venues), and conference/retreat centres.

Typical attributes of this type of development are:

- limited building footprint,
- a focus on natural amenity and outdoor venues,
- extensive landscaping and open space,
- multiple facilities within a single parcel, and
- the potential for accommodation and food services.

POLICIES

- 2.3.9 Commercial uses such as conference centres, wedding venues, or other permanent developments to support concerts, tradeshow, and or galas are deemed appropriate within the Service and Hospitality Hub.
- 2.3.10 Outdoor storage that is not incidental to a primary use and is not screened shall not be permitted within the Service and Hospitality Hub.



Local Commercial Hubs

GOAL ALIGNMENT



Develop a vibrant employment hub.

Two Local Commercial Hubs are identified within the GT ASP area.

- The Local Commercial Hub adjacent to Highway 791 will offer convenient access to local retail development for people who live and work in the area, and for those who live in adjacent communities. Commercial development along the Highway corridor will incorporate high-quality design to create visually appealing development in this location. This will also act as a buffer between the highway and residential uses.
- The Local Commercial Hub located in the northwest portion of the Plan will be tailored to meet the needs of Glenmore Trail residents with mobility connections to the City of Chestermere.

Development may contain uses such as local retail/commercial operations, convenience and daily-need commercial services.

Typical attributes of this type of development are:

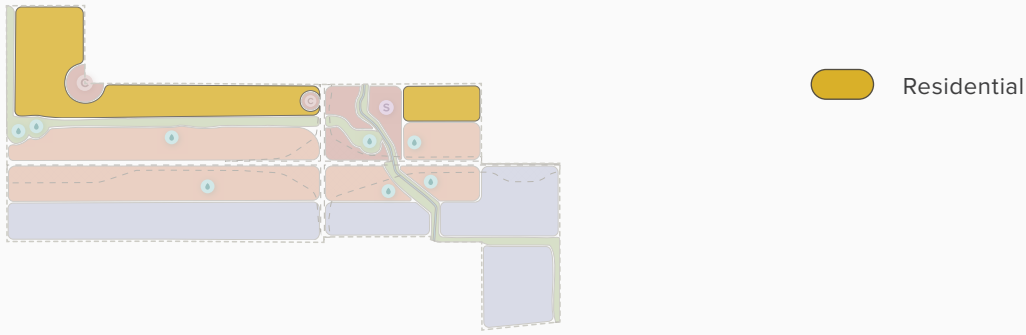
- Local-oriented retail operations,
- drive-through services,
- extensive landscaping,
- internal roadways and parking facilities, and
- easily navigable ingress/egress for clientele and deliveries.

POLICIES

- 2.3.11 Commercial uses such as convenience, local retail, and service commercial are deemed appropriate within a Local Commercial Hub.
- 2.3.12 Local Commercial Hubs should be designed to meet the daily needs of local residents and employees.



Part 2: Plan Policies



2.4 Residential Development

GOAL ALIGNMENT



Provide unique, rural-character housing options.



Celebrate rural character.

Residential development within the GT ASP plan area will balance moderate population growth while maintaining a strong sense of community. Residential development will provide opportunities for a range of housing types, lot sizes, and densities that are carefully planned and are in keeping with the rural character of area.

POLICIES

- 2.4.1 *Figure 7. Land Use Strategy* shall guide the general location of residential development.
- 2.4.2 Local plans shall be required for multi-lot residential development within the ASP.
- 2.4.3 Local plans shall delineate exact boundaries for residential development.
- 2.4.4 The gross residential density for any local area plan within the ASP area shall be a minimum of 4 upa and a maximum of 6 upa.
- 2.4.5 The plan area shall contain a mix of housing types, with higher-density housing concentrated around regional and neighbourhood hubs.
- 2.4.6 All housing types should be designed to maintain the rural character of the area.
- 2.4.7 All local plans containing residential development shall address the County’s “dark sky” requirements.



3.0 Interfaces

3.1 Industrial and Commercial Interfaces

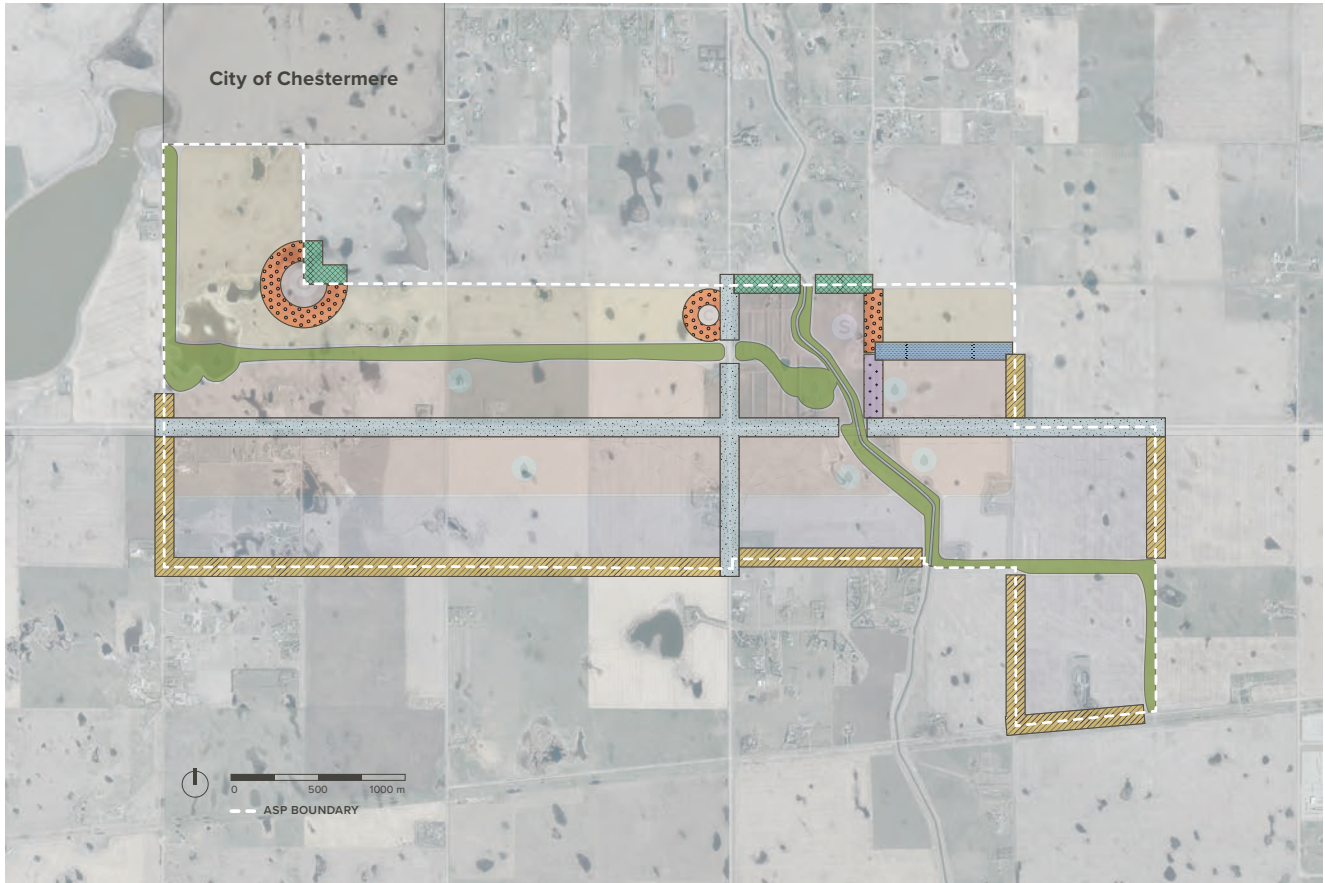









Figure 9. Industrial, Commercial, and Corridor Interfaces

-  Green Space
-  Residential to Commercial
-  Industrial to Agricultural
-  Residential to Industrial
-  Commercial to Agricultural
-  Highway to Development
-  Industrial to Commercial

GOAL ALIGNMENT



Celebrate rural character.



Provide unique, rural-character housing options.



Build on natural features to offer a variety of open spaces.



Balance economic viability and flexibility.



Develop a vibrant employment hub.

Minimizing the direct impact of commercial and industrial development on existing and future residential or agricultural areas is

Part 2: Plan Policies

accomplished by considering land use, spatial separation, roadway design, landscaping and the design and layout of buildings.

Local plans should address and provide guidance for specific contexts of different interfaces, but should use the following interface principles:

- Ensure the transition between employment lands and residential development is managed effectively by supporting complementary land use types and densities in interface areas.
- Provide the framework for local plans to address an appropriate transition between residential areas comprising different housing forms.
- In accordance with the County's Agricultural Boundary Design Guidelines, ensure an appropriate interface between non-agricultural uses and agricultural land or operations, to avoid negative impacts on agricultural operations.

POLICIES

- 3.1.1 Local plans for business and industrial uses shall include an interface strategy that addresses the policies of this section.
- 3.1.2 Acceptable uses along industrial and commercial interfaces are those business or industrial activities primarily carried on within an enclosed building that generate no significant nuisance impact outside of the enclosed building.
- 3.1.3 High quality building appearance should be emphasized where industrial / commercial buildings face residential areas.
- 3.1.4 Garbage storage, loading bays, loading doors, or other activities creating heavy truck movements should not face onto a residential area or ASP boundary.
- 3.1.5 Proposals for non-agricultural development adjacent to agricultural lands located either within or outside of the Plan boundary shall incorporate buffering, siting, and design techniques to minimize negative impacts on agricultural lands.
- 3.1.6 Public access such as trails, pathways, and parks should be discouraged adjacent to agricultural lands unless supported by the County's Parks and Open Space Master Plan or this ASP's proposed pathway network (*Figure 8. Open Space and Pathways*).
- 3.1.7 The County's Agricultural Boundary Design Guidelines should be used to help inform interface conditions between agricultural and non-agricultural lands.

3.2 Corridor Gateways & Interfaces

The Glenmore Trail corridor is a high-visibility transportation route that intersects with another key regional roadway, Highway 791, at the centre of the plan area. The interfaces and connection points along these high-traffic, high-visibility corridors must be carefully designed to ensure that the character of the area is appropriately integrated, and that safety, wayfinding, and transportation accessibility are considered. Local area plans will be responsible for incorporating design details of the gateways and interfaces, important considerations should include:

- buffering and land use,
- screening and landscaping,
- roadway signage,
- development signage, and
- building design and orientation.

POLICIES

- 3.2.1 Local plans for business and industrial uses shall include an interface strategy that addresses corridor interfaces and the policies of this section.
- 3.2.2 Local plans shall consider all requirements for screening and landscaping in the County Land Use Bylaw when determining design guidelines for an interface strategy.
- 3.2.3 Local plans that contain connection points to the Glenmore Trail or Highway 791 corridors shall address access details that manage safety and comfort for all roadway users.
- 3.2.4 Local plans should address wayfinding that considers access from the Glenmore Trail and/or Highway 791 corridors.
- 3.2.5 Signage should promote safe access to the area and clear direction for vehicle traffic and other roadway users.
- 3.2.6 High quality building appearance should be emphasized for buildings facing the Glenmore Trail and/or Highway 791 corridors where screening is not possible or appropriate, at the discretion of the County.
- 3.2.7 Garbage storage, loading bays, loading doors, or other activities creating heavy truck movements shall be properly screened from visibility along Glenmore Trail or Highway 791.
- 3.2.8 All interim uses besides agriculture shall be properly screened from visibility along Glenmore Trail or Highway 791.

4.0 Open Space and Community Uses

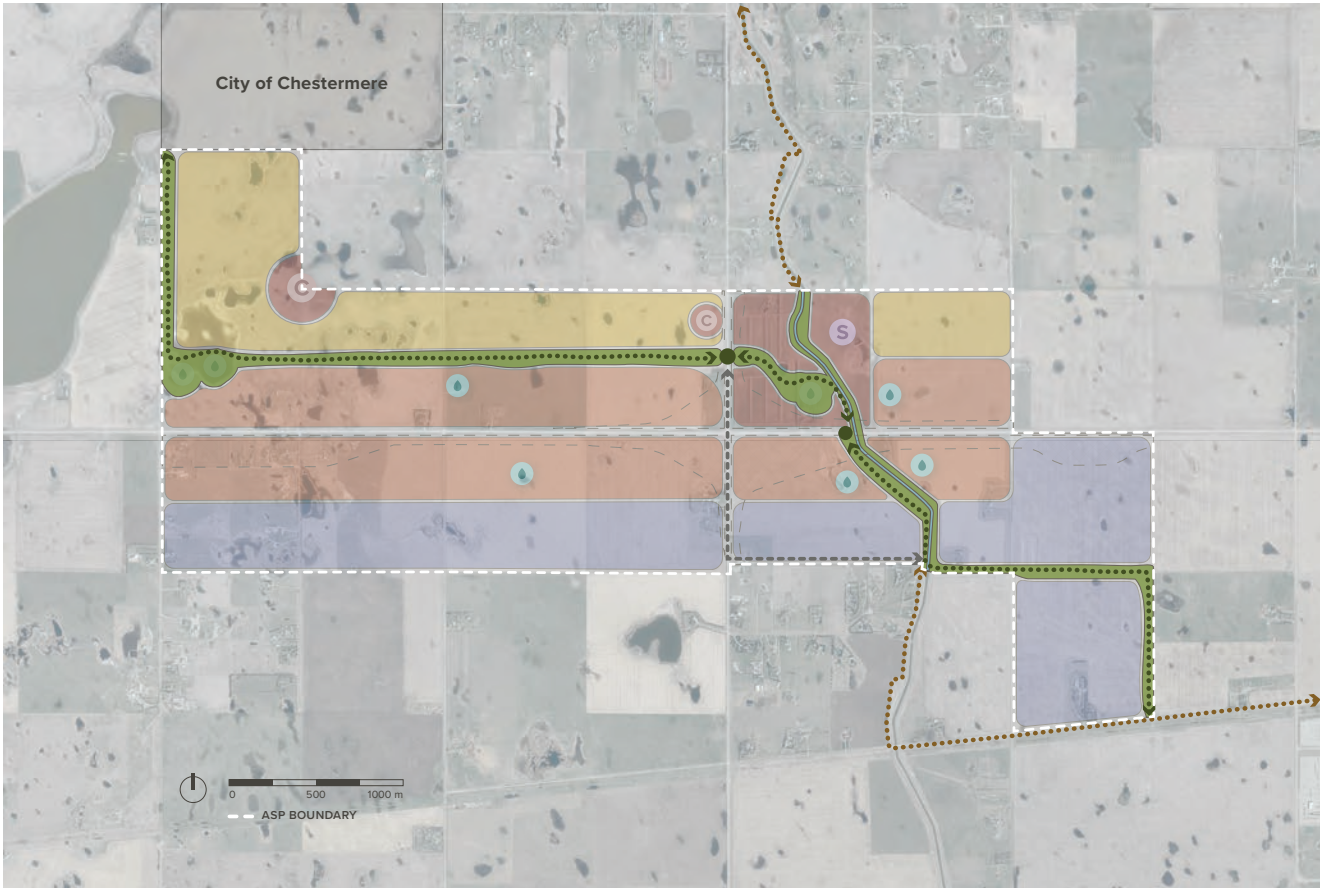


Figure 10. Open Space & Community Uses

- Pathway Connection
- Optional Pathway Connection
- Proposed Regional Pathway
- Raised Pedestrian Crossing
- Parks and Open Space

4.1 Environmental Lands

GOAL ALIGNMENT



Build on natural features to offer a variety of open spaces.



Protect naturally significant areas.

Environmental lands will contain sensitive natural habitat that must be preserved without major disruption. Within the GT ASP, these lands will continue to provide important natural functions, in addition to acting as passive recreation locations and creating sensible buffers between residential and non-residential uses.

The natural environment represents land with the most sensitive and naturally existing ecological conditions. These lands will remain undeveloped in a naturalised state and may accommodate pathways and passive recreation opportunities.

POLICIES

- 4.1.1 Local plans shall confirm the boundaries of natural environmental areas, and the specific requirements for their protection, through the required environmental studies.
- 4.1.2 County and provincial policies shall be used in decision-making regarding wetlands and water bodies. Local plans shall use the Alberta Wetland Classification System when identifying, classifying, and evaluating wetlands.
- 4.1.3 Environmental lands shall not be used for development other than passive recreation and critical infrastructure (as deemed appropriate by the County).

4.2 Community Recreation Amenities and Parks

Community recreation amenities and parks will be important for the growing residential areas both within and surrounding the ASP area but can also support those who work or travel within this Plan. As a mixed-use ASP, numerous groups of people will have the potential to access amenities in the plan area; a thoughtful combination of parks and recreational community facilities can support social connection, community cohesiveness, and regional wellbeing.

POLICIES

- 4.2.1 Local plans shall refine the size, location, and design of parks, but shall coordinate to ensure that variety of function is maintained across the ASP area.
- 4.2.2 Parks in the plan area should be designed with regard to facilitating different types of recreation and activities across the plan area.
- 4.2.3 All parks and community amenities should be connected by a system of pathways or trails.
- 4.2.4 Pathways should provide continuous connections within the ASP area, and, wherever possible, offer logical connections to current or future pathway systems in proximity. Proposed pathways should connect with proposed or existing County pathways as identified in the County's Active Transportation Plan.
- 4.2.5 Pathways should be designed to accommodate both active transportation and recreation.
- 4.2.6 Public and private partnerships may be considered, or encouraged, in the provision of community amenities.
- 4.2.7 At local plan stage, the County and applicant shall collaboratively explore the opportunity for a raised pedestrian crossing over Glenmore Trail.



4.3 Reserves

The allocation of public land for the purpose of municipal reserve or environmental reserve is strategically dedicated by the County during the subdivision process. The purpose for municipal reserve dedication is to produce community enhancements, such as parks, schools, and recreational amenities. Similarly, the environmental reserve process provides protection of the natural environment through development prevention measures in unsuitable areas, such as ravines, floodways, or unstable slope zones.

POLICIES

- 4.3.1 Reserves shall be estimated through a Reserves Analysis as part of the local plan process.
- 4.3.2 Reserves shall be provided to the County, to the satisfaction of the Subdivision Authority, at the time of subdivision.
- 4.3.3 All reserves shall be publicly accessible.
- 4.3.4 The County may require the following in determining environmental reserves:
- a biophysical impact analysis report;
 - a geotechnical analysis; and/or
 - other assessments acceptable to the County.

5.0 Servicing

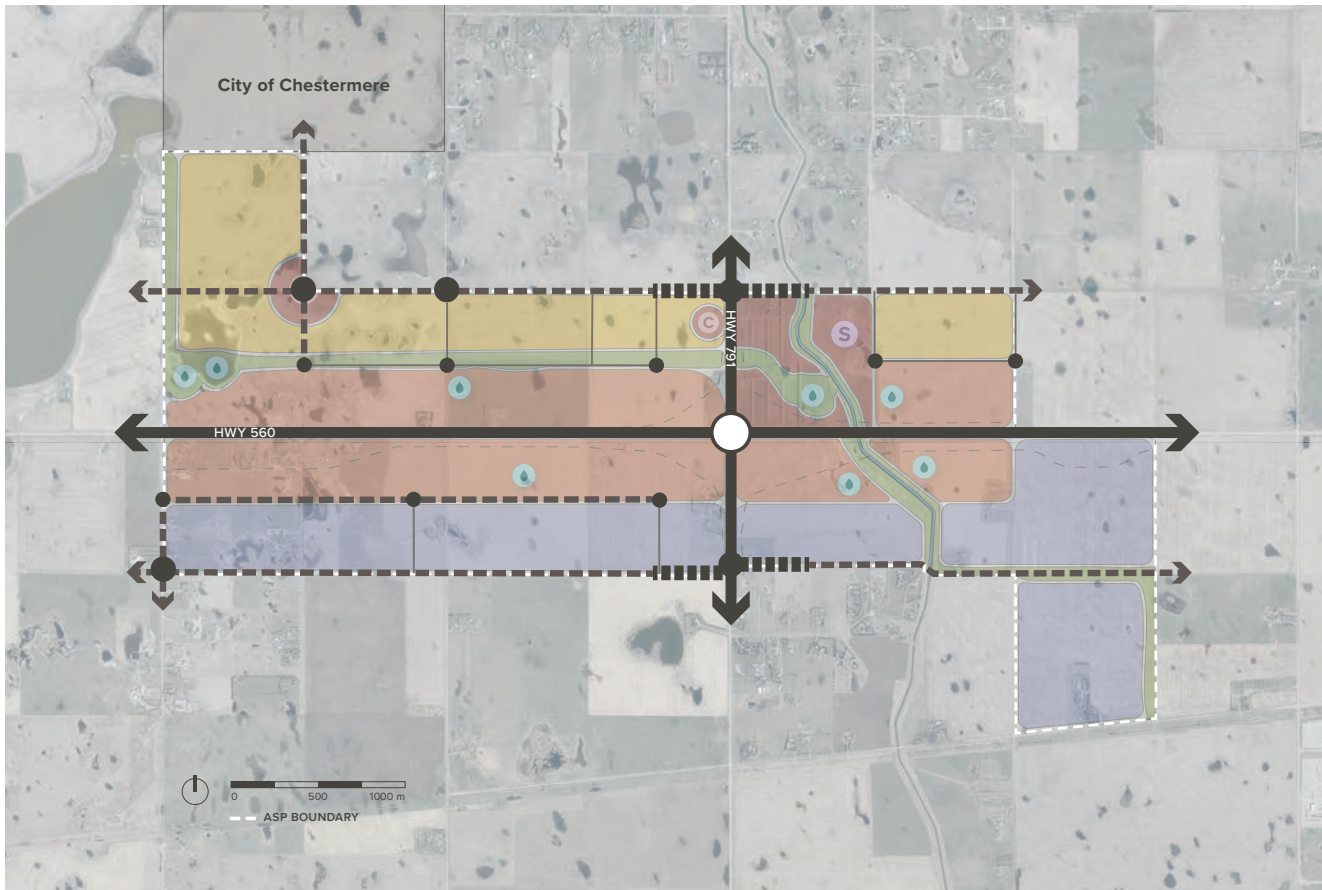


Figure 11. Transportation Network

-  Highway
-  Arterial
-  Industrial / Commercial Collector
-  Internal Intersections
-  Highway Interchange

5.1 Transportation

GOAL ALIGNMENT



Build on natural features to offer a variety of open spaces.



Protect naturally significant areas.

The GT ASP’s road network will be designed to ensure safe, functional and efficient travel for all users of the area. The network will ensure that vehicle travel to and within the community is accommodated with a hierarchy of suitably scaled and functional roadways, separating residential,

commercial and industrial traffic where appropriate. The network design will seek to preserve the natural environment where suitable while providing efficient multimodal connections internally, and regional opportunities for public transportation, active transportation, and recreation.

The GT ASP identifies the major network alignments, as shown on *Figure 11. Transportation Network*. Additional technical reporting that is required to support more detailed design will be provided at the local plan stage for all transportation and servicing infrastructure in the plan area.

A Transportation Impact Assessment (TIA) was completed by ISL Engineering and Land Services Ltd. The TIA identifies the roadway infrastructure (e.g. at-grade intersection or interchange), lane configurations (e.g. number of lanes on Hwy 560, left turn bay, right turn bay) and traffic control (e.g. stop controlled, signal, roundabout) required to accommodate the background traffic on Glenmore Trail and the GT ASP traffic in the short, medium and long-term .

INTERNAL ROAD NETWORK

A network of internal roadways, comprised of arterials and collectors, will connect all areas of the ASP to Glenmore Trail and Highway 791. *Figure 11. Transportation Network* provides a conceptual plan for the ASP transportation network and is expected to be refined and revised with further transportation studies and planning at the local plan stage.

Local plans must consider how to safely incorporate opportunities for active transportation in the internal road network, including the potential for a connection across Glenmore Trail via the existing canal network and or a future interchange.

While there is currently no regional public transit system proposed, potential transit route connections and transit stop connection points should be considered at local plan preparation as community development occurs in the future.

EXTERNAL ROAD NETWORK

Access to Glenmore Trail and Highway 791 are important considerations in this ASP, and the transportation network will be planned to efficiently access and align with the provincial and regional highway network.

Infrastructure needs and access will vary over the course of the ASP's build out:

- In the short and medium term, access to Glenmore Trail would be from the existing Glenmore Trail at-grade intersections at Range Road 275, Range Road 281, Glenmore View Road and Range Road 282. Access to Highway 791 would remain unchanged.
- In the long term with the interchange at Glenmore Trail / Highway 791, all at-grade Glenmore Trail intersections (Range Road 275, Range Road 281, Glenmore View Road, Range Road 282) would be closed. Lands within the ASP could access Glenmore Trail from the future Glenmore Trail / Highway 791 interchange or the Glenmore Trail / Rainbow Road (Range Road 283) interchange (outside of the ASP area). Connectivity to Highway 791 would be from the ASP internal road network while connectivity to Rainbow Road would be from the future service road or internal roads within the neighbouring lands.

The Glenmore Trail Functional Planning Study identified the segment of Glenmore Trail adjacent to the ASP area, which includes the Glenmore Trail / Highway 791 interchange, with a construction timeline of 20+ years. The projected timelines will be impacted by regional overall growth in the area, including the Hamlet of Langdon, the City of Calgary, the Janet ASP, the Shepard Industrial ASP, and GT ASP.

POLICIES**GENERAL TRANSPORTATION NETWORK**

- 5.1.1 The transportation network should be developed in accordance with *Figure 11. Transportation Network*.
- 5.1.2 The classification, street sizing and intersection/access spacing of roads shall be determined at local plan stage based on further transportation analysis and shall be in accordance with the County's Servicing Standards for Roadways and requirements of Alberta Transportation.
- 5.1.3 Local plans shall identify exact roadway connections and alignments in the transportation network. The design of the transportation network shall ensure safe, efficient, well integrated connections between internal and external road networks and to provide appropriate linkages to adjacent existing communities and future development outside of the plan area.
- 5.1.4 A Traffic Impact Assessment shall be prepared for each local plan. The County may defer this requirement to the subdivision application process where deemed appropriate.
- 5.1.5 Accesses to Glenmore Trail and Highway 791 shall be designed to ensure safe and efficient transportation flow.
- 5.1.6 Any costs associated with transportation improvements to the local road network that are identified through a Transportation Impact Assessment shall be the applicant's responsibility. Cost-sharing may be required for improvements to the provincial highways.
- 5.1.7 The design and construction of roadways within the local and regional transportation network shall use appropriate access management principles and shall be in accordance with County Servicing Standards.
- 5.1.8 Local roadways shall be designed in general accordance with the urban or rural cross-section requirements established by the County Servicing Standards. New or modified cross-sections may be proposed at the time of a local plan and or subdivision stage for consideration by the County.

REGIONAL TRANSPORTATION NETWORK

- 5.1.9 The County shall collaborate with adjacent municipalities to ensure opportunities for roadway and pathway connections across municipal borders.
- 5.1.10 The County shall collaborate with the Province and the City of Calgary regarding regional road connections and design of interchanges with respect to Glenmore Trail and Highway 791.
- 5.1.11 Appropriate land dedication shall be provided at the subdivision stage for the development of the future provincial interchange at Glenmore Trail and Highway 791.
- 5.1.12 The County encourages and supports the addition of a pedestrian and bicycle network as part of the provincial highway interchange design and construction subject to Alberta Transportation approval.
- 5.1.13 Design of intersections with Glenmore Trail and Highway 791 shall consider noise attenuation for existing and planned residential areas, and may include berms, landscape buffers or other approved methods to the satisfaction of the County.

PUBLIC TRANSIT

Part 2: Plan Policies

- 5.1.14 Opportunities for local and regional public transit connections should be supported through local plan preparation.
- 5.1.15 Appropriate roadway types and roadway design should be considered at the local plan stage to accommodate future public transit routes.
- 5.1.16 Appropriate locations and design considerations for public transit stops shall be considered at the local plan stage.

INDUSTRIAL/COMMERCIAL ROADS

- 5.1.17 Industrial and commercial road cross sections shall be determined at the time of local plan preparation.
- 5.1.18 All road cross sections should consider safe and efficient pedestrian and bicycle circulation between buildings, sites, and, where applicable, adjacent areas.



5.2 Water Servicing

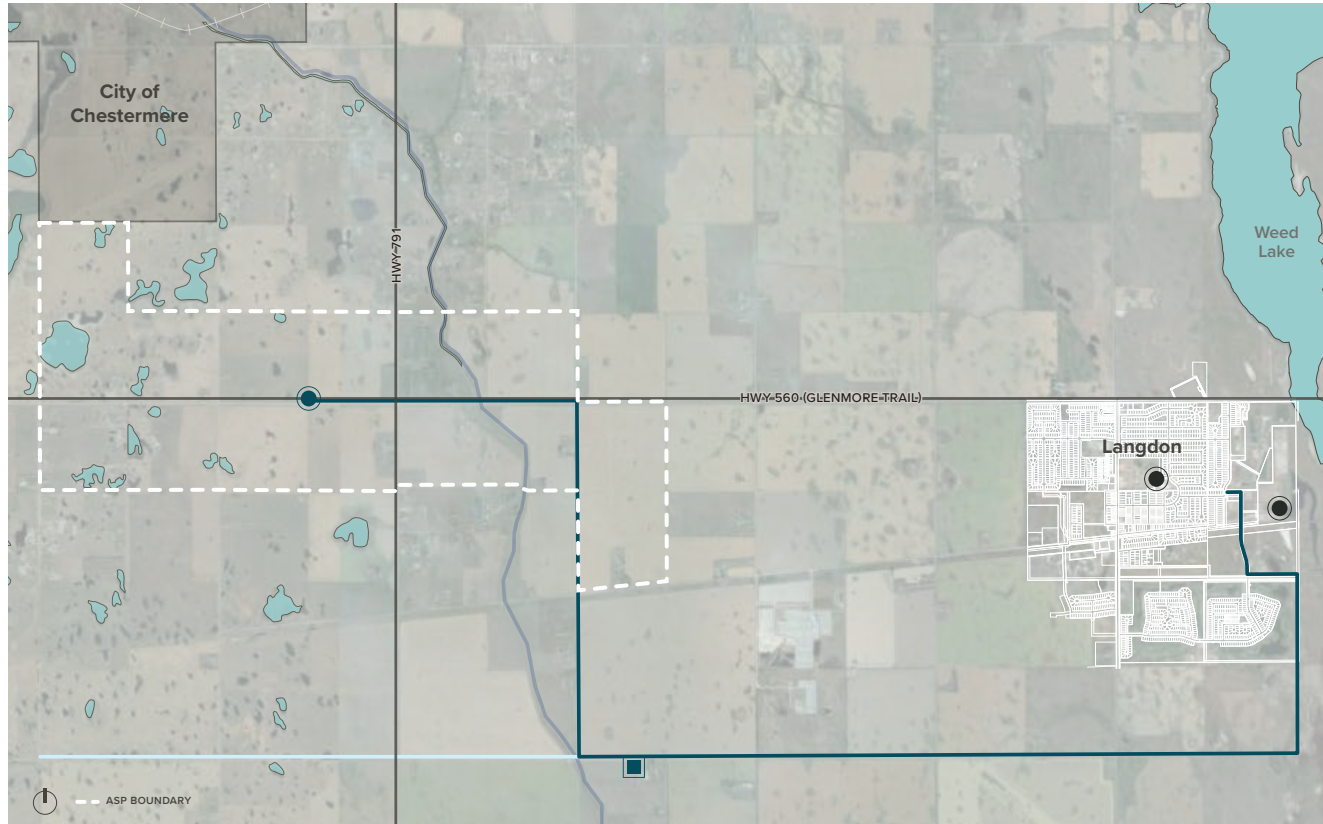
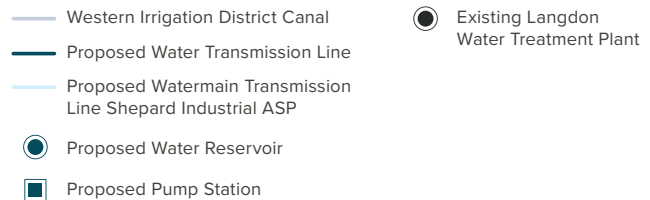


Figure 12. Water Network



The water servicing within the plan area will ensure potable water systems are provided in a safe, cost effective and fiscally sustainable manner. All potable water servicing will occur in a logical, sequenced manner and will be in conformance with the current Rocky View County Servicing Standards.

Water servicing to the GT ASP area will be provided by an extension of the Hamlet of Langdon infrastructure through the Langdon Waterworks utility system. A conceptual plan for the water servicing of the plan area is demonstrated in *Figure 12. Water Network*. The ultimate water servicing concept proposes the use of an onsite elevated water storage reservoir that is provided water from a booster pump station. This strategy is similar to that proposed for the Shepard Industrial ASP and provides the opportunity to share infrastructure with that ASP to lower overall servicing costs for the region.

The exact alignment and extents of the water servicing system will be identified at the subdivision stage, based on additional detail provided under local plans submissions with a Water Network Analysis. In all instances fire suppression and water supply infrastructure will be provided to the satisfaction and standards of Rocky View County.

Development within the Plan area may be serviced temporarily by individual potable water systems, such as water storage tanks, to the satisfaction of the County until regional servicing is available.

Part 2: Plan Policies

POLICIES

- 5.2.1 The alignment, provision and capacity of water distribution system shall be in general accordance with *Figure 12. Water Network*.
- 5.2.2 All new development shall be connected to the County's potable water system once available.
- 5.2.3 Notwithstanding policy 4.2.2, development within the plan area may be serviced temporarily by individual water storage tanks or alternative potable water systems deemed appropriate by the County, and/or in accordance with relevant County policy, until regional servicing is made available. All development proposing interim servicing must meet the following criteria;
- The County's potable water system is not yet available to the site;
 - The interim servicing solution is set up such that it can easily tie into the ultimate system once available;
 - The developer enters into a deferred services agreement and connects to services when available;
 - The developer agrees that no compensation will be provided to the developer for the costs incurred for the construction of the temporary servicing solution.
 - The proposed temporary solution meets provincial regulations; and
 - The development is not a heavy water user.
- 5.2.4 The detailed location and size of utility rights-of-way and easements, and related line assignments, should be determined at the local plan stage to the satisfaction of both the County and the applicant. Alberta Transportation will be consulted where utilities run within the Highway 560 and or Highway 791 right-of-way.
- 5.2.5 A Water Network Analysis shall be completed at local plan preparation in support of the proposed grid layout of water mains, and shall include calculated pressures and flows, and demonstration of redundant water main looping.
- 5.2.6 The potable water distribution system and bulk water storage volumes shall be designed to ensure adequate fire protection throughout the development based on minimum fire flows for the planned population, industrial and commercial areas, at the time of local plan preparation.
- 5.2.7 The County should encourage the reduction and reuse of water in accordance with provincial guidelines.
- 5.2.8 Major water infrastructure, including large diameter water mains for multiple local plan areas, are to be located within rights-of-way, wherever possible.

5.3 Wastewater Servicing

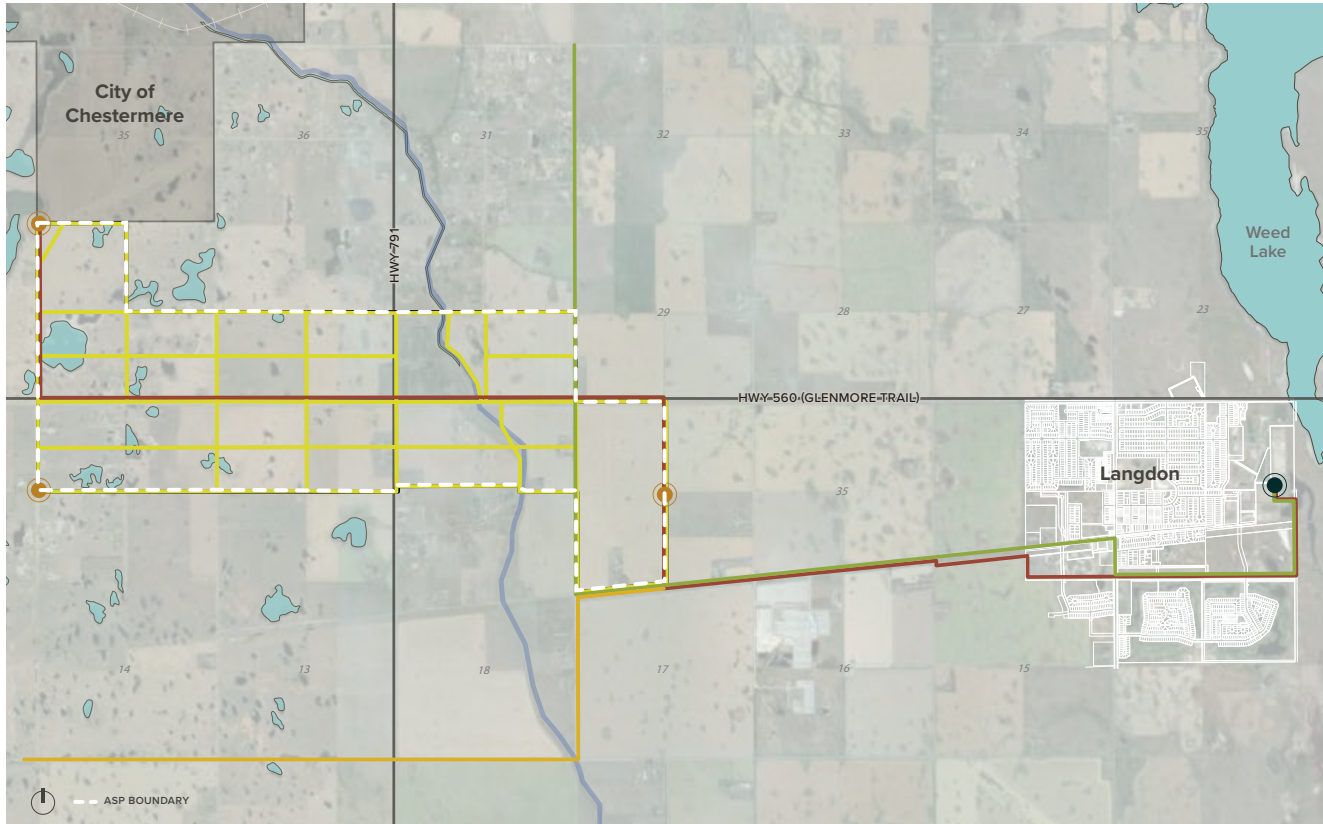


Figure 13. Wastewater Servicing

- Western Irrigation District Canal
- Sanitary Catchment
- Existing East Rocky View Regional Wastewater Transmission Main (ERVRWWTM)
- Proposed Forcemain Shepherd Industrial ASP
- Proposed Forcemain
- Proposed Lift Station
- Existing Langdon Wastewater Treatment Plant

The wastewater servicing within the Plan area will ensure wastewater systems are provided in a safe, cost effective and fiscally sustainable manner. All wastewater servicing will occur in a logical, sequenced manner and will be in conformance with the current Rocky View County Servicing Standards.

Wastewater servicing for the Glenmore Trail ASP area will be provided by extension of a forcemain system to the Hamlet of Langdon Wastewater Treatment Plant. A conceptual plan for the wastewater servicing of the Plan area is demonstrated in *Figure 13. Wastewater*. The ultimate wastewater servicing concept proposes the use of lift stations located at low points with local gravity mains servicing development. This strategy is similar to that proposed for the Shepard Industrial ASP and provides the opportunity to share infrastructure with that ASP to lower overall servicing costs for the region.

The exact alignment and extents of the wastewater servicing system will be determined at subdivision stage, based on further detail provided under local plans with a Sanitary Servicing Study.

Development within the Plan area may be serviced temporarily by individual wastewater systems, such as septic tanks, to the satisfaction of the County until regional servicing is available.

Part 2: Plan Policies

POLICIES

- 5.3.1 All new development shall be connected to the County's wastewater system once available.
- 5.3.2 Notwithstanding policy 4.3.1, development within the plan area may be serviced temporarily by individual wastewater solutions deemed appropriate by the County, and or in accordance with relevant County policy, until regional servicing is made available. All development proposing interim servicing must meet the following criteria;
- The County's wastewater system is not yet available to the site;
 - The interim servicing solution is set up such that it can easily tie into the ultimate system once available;
 - The developer enters into a deferred services agreement and connects to services when available;
 - The developer agrees that no compensation will be provided to the developer for the costs incurred for the construction of the temporary servicing solution;
 - The proposed temporary solution meets provincial regulations; and
 - The development is not a heavy wastewater user.
- 5.3.2.1 Major wastewater infrastructure, including linear sewer trunks for multiple local plans areas, are to be located within rights-of-way, wherever possible.
- 5.3.2.2 A Sanitary Servicing Study shall be completed at the time of each local plan preparation in support of the proposed layout of wastewater sewers. This Study shall include calculated flows and capacities.

5.4 Stormwater Servicing

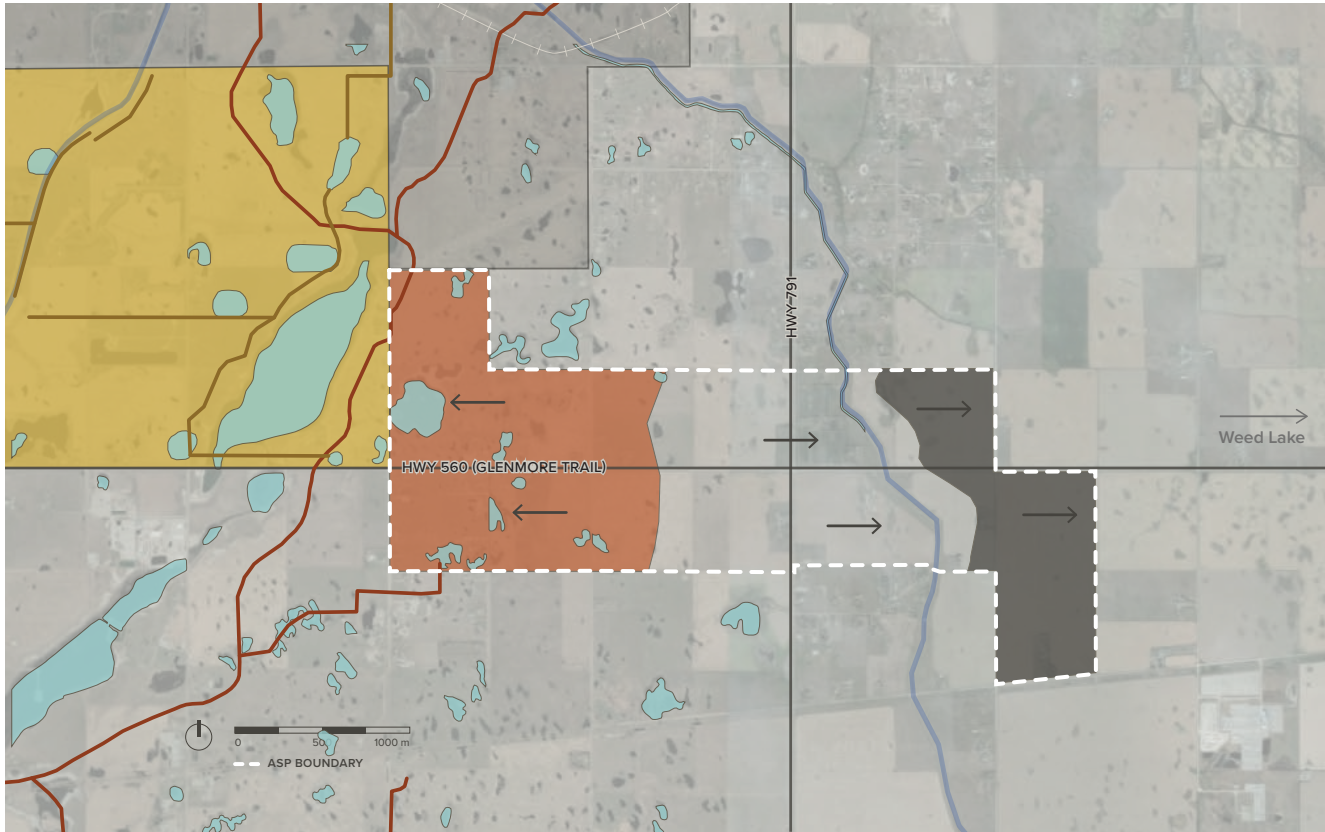


Figure 14. Stormwater Existing Conditions

- Proposed Janet Conveyance
- Proposed Shepard Conveyance
- Western Irrigation District Canal
- Major Road
- Railway
- Predevelopment Flow Direction
- Janet ASP Boundary
- Shepard Regional Drainage Plan
- Existing Water Bodies
- Cooperative Stormwater Management Initiative

GOAL ALIGNMENT



Protect naturally significant areas.

The stormwater management plan for the GT ASP area will seek to maximize the use of natural stormwater drainage conveyance systems, protect these routes and storage areas, and provide stormwater that is safe fiscally sustainable, and preserves high value wetlands within the plan area.

The west side of the GT ASP is located within the Shepard Regional Drainage Plan (SRDP) (MPE Engineering Ltd., March 2011) plan area and the east side of Canal A lies within the Co-operative Stormwater Management Initiative (CSMI) (MPE

Engineering Ltd., April 2014). However, the area to the west of Canal A is currently outside the current CSMI study area and currently unaccounted for. As a result, part of the drainage strategy will be influenced by the recommendations presented in the SRDP and CSMI. The area is characterised by numerous localized depressions which form wetlands with temporary or permanent water features, depending on the upland contributing catchment, overflow from upstream areas, and the potential spill elevations. The GT ASP area contains three main high points in the middle of the site, causing half of the site to drain towards the east where it historically collected by Western Irrigation District (WID) Canal A and Weed Lake. The remaining half drains west towards the existing natural wetlands of the Shepard Slough Complex.

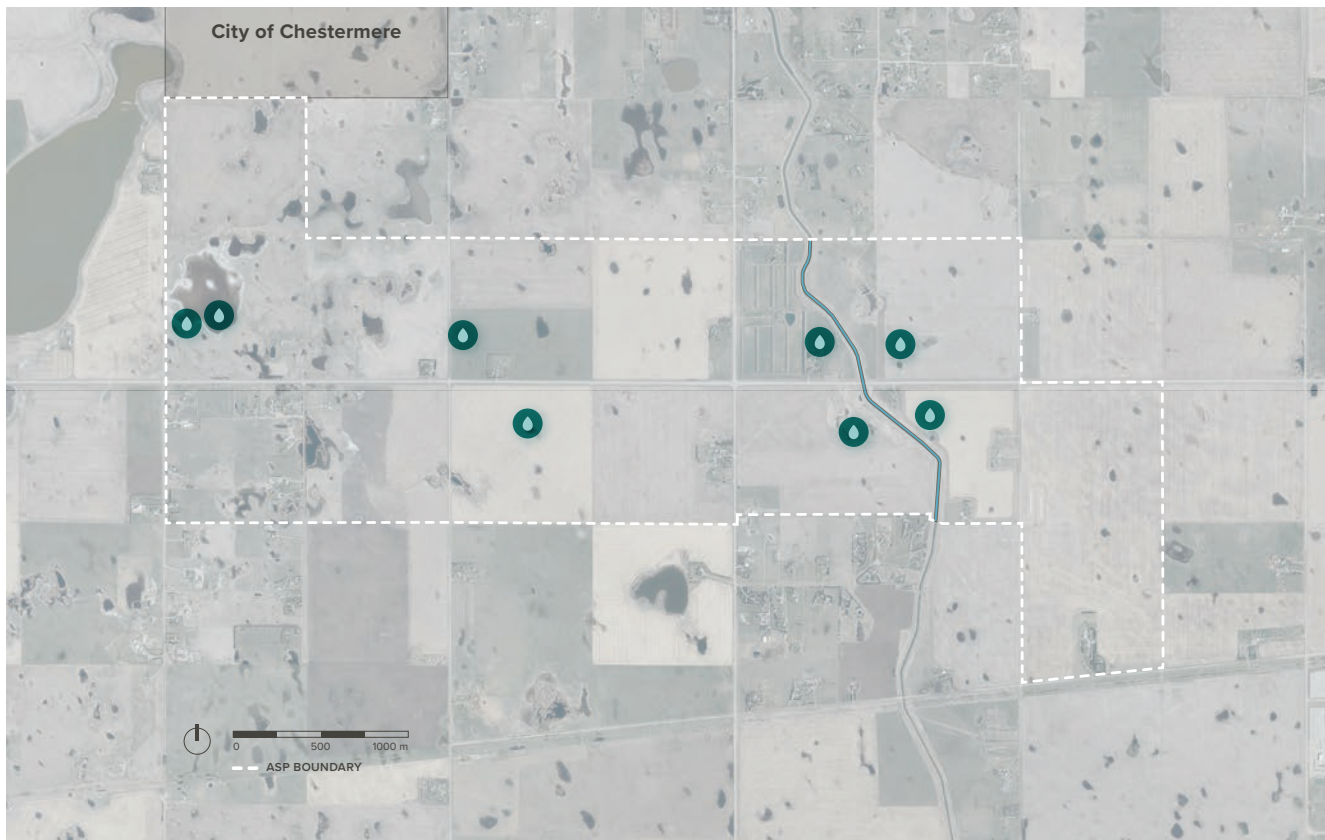



Figure 15. Stormwater

 Conceptual Regional Stormwater Facilities & Wetlands

The SRDP proposed to treat and move water south of the Western Headworks Canal through a series of naturalised and constructed conveyance systems intended to minimize the impacts on the Shepard ditch, wetlands, and other natural drainage areas.

The CSMI proposed the use of an out-of-canal solution whereby all stormwater runoff is diverted away from the WID irrigation system by utilizing existing and proposed conveyance systems that discharge to Weed Lake. This initiative may result in a stormwater management system that complements the SDRP system or, alternatively replaces the SRDP Plan.

The stormwater management existing conveyance alignments and water bodies are demonstrated in *Figure 14. Stormwater Existing Conditions*. A conceptual plan for the locations of stormwater facilities is demonstrated in *Figure 15 Stormwater*. Exact alignment, locations, and extents of the stormwater servicing system will be determined at subdivision, based on further detail provided by local plans with Sub-Catchment Master Drainage Plan.

The stormwater management plan for the GT ASP area will seek to maximize the use of natural stormwater drainage conveyance systems, protect these routes and storage areas, provide stormwater that is provided is safe, cost-effective, and fiscally sustainable, and preserve high value wetlands within the plan area.

POLICIES

- 5.4.1 The applicant shall submit a Stormwater Management Report at local plan stage that is consistent with the County Plan, the general principles of the GT ASP Sub-Catchment Master Drainage Plan, and the policies of this Plan, including the general locations of facilities as shown on *Figure 15. Stormwater*.
- 5.4.2 The Stormwater Management Report prepared at local plan stage shall comply with any new stormwater plans, management policies, and interim servicing policies that may be introduced after the adoption of this Plan.
- 5.4.3 The stormwater management system should be designed to operate on a gravity basis where possible and accommodate stormwater flows from the adjacent road network.
- 5.4.4 As part of the preparation of a local plan and any supporting Sub-Catchment or Master Drainage Plans, best management practices and alternative solutions for the improvement of stormwater quality and reduction of quantity shall be required. Solutions may include:
- design of stormwater facilities that incorporate source controls in order to reduce the amount of water moving downstream and the need for end of pipe treatment facilities;
 - use of low impact development methods, such as bio-swales, rain gardens, constructed wetlands, green roofs and permeable pavements;
 - reduction of impervious surfaces;
 - the re-use of stormwater; and
 - consideration of stormwater ponds and constructed wetlands at the sub-regional level to support the reuse of stormwater.
- 5.4.5 Stormwater management systems shall be provided in a safe, cost-effective, and fiscally sustainable manner, including efficiently using land by combining raw water storage for the purpose of re-use and stormwater facilities.
- 5.4.6 Stormwater management systems, including re-use or irrigation, should be designed at a scale that services the local plan area. The County discourages stormwater ponds designed for individual lots.
- 5.4.7 Major stormwater infrastructure, where servicing an applicant's land, shall be located on lands owned by that applicant, wherever possible.
- 5.4.8 Stormwater conveyance systems should develop in an orderly, logical, and sequential pattern of development.
- 5.4.9 Stormwater shall be conveyed in a manner that protects downstream properties and preserves the water quality of receiving water courses.
- 5.4.10 Stormwater conveyance systems must provide a right-of-way of sufficient width to accommodate upstream stormwater flow.
- 5.4.11 The volume and rate of stormwater discharge into the Shepard Ditch shall be in accordance with the SRDP, or other plans that amend, replace, or add to that plan, where appropriate.
- 5.4.12 The volume and rate of stormwater discharge into the CSMI system and ultimately Weed Lake shall be in accordance with the CSMI plan, or other plans that amend, replace, or add to that plan, where appropriate.

Part 2: Plan Policies

- 5.4.13 Natural wetlands and/or natural drainage courses that are retained should receive treated storm water through direct or indirect flow in order to maintain the integrity of the wetland and the drainage course.
- 5.4.14 Open space amenities and functional playing fields should be included within dry ponds, where possible.
- 5.4.15 Stormwater ponds and constructed wetlands shall be designed to be fully integrated within the open space network, with particular focus on human enjoyment and ecological functionality, as well as their infrastructure requirement.
- 5.4.16 Stormwater ponds and constructed wetlands should be subdivided as public utility lots at the subdivision stage.
- 5.4.17 The applicant shall obtain all relevant approvals for stormwater discharge from Alberta Environment and Parks.

5.5 Utilities

The GT ASP seeks to ensure shallow private utility systems are provided efficiently to new development in the plan area. Shallow utilities, including gas, power and communication servicing, will be provided to the subject lands through the extension of existing networks in the surrounding areas, including existing Hamlet of Langdon. Shallow utility feeds will be extended into the development on a phased basis and will be established in detail at the subdivision stage, based on further information through the preparation of local plans. Consultation with FortisAlberta Inc may determine the need for regional upgrades for power.

POLICIES

- 5.5.1 Utility service development should support an orderly, logical, and sequential pattern of development.
- 5.5.2 Costs associated with servicing all new developments with shallow utilities shall be at the expense of the applicant.
- 5.5.3 The location and size of utility rights-of-ways and easements should be determined at the local plan stage to the satisfaction of the County, applicant, and the utility provider.
- 5.5.4 Wherever possible, shallow utilities should be constructed in common locations to maximize the developability of the lands and to reduce any off-site impacts.
- 5.5.5 Utility rights-of-way and easements for shallow utilities shall be provided at the subdivision or development permit stage, as deemed necessary by the County and the utility provider. The purpose of rights-of-way and easements is to ensure that the identification and maintenance of multiple utilities can be done without service interruptions.
- 5.5.6 Commercial Communications Facilities should be constructed in land identified for industrial or commercial development and shall not be located within parks and open space areas, natural environment areas, or within a community node or entranceway. The design of these facilities shall align with County policy.



5.6 Solid Waster and Recycling

It is important to plan for the management of solid waste through all stages of development, from construction and demolition, to how waste management will function at full build out of the community. This ASP emphasizes the reduction and diversion of waste through the recycling and reuse of materials, prioritizes alignment with the County's Solid Waste Master Plan, and endeavours to promote proper disposal and recycling of solid waste material from its construction sites.

POLICIES

- 5.6.1 Costs associated with the management and disposal of solid waste for generate through all stages of construction and development shall be at the expense of the applicant.
- 5.6.2 Through the preparation of local plans, the applicant will identify and arrange for any post-construction waste management that will be required from the County.
- 5.6.3 Solid waste disposal shall be the responsibility of property owners, business owners, and/or lot owner associations within the GT ASP area until such time as a County-sponsored waste management program is available.
- 5.6.4 At the local plan stage, a solid waste management plan should that aligns with the County's Solid Waste Master Plan and/or Solid Waste Servicing Strategy shall be prepared.
- 5.6.5 Waste minimization and waste diversion practices should be encouraged for the GT ASP area, and incorporated in all decisions regarding solid waste management, including but not limited to, centralized community collection facilities and the 3Rs hierarchy of reducing, reusing, and recycling.





5.7 Emergency Services

The ASP area will be served by the County, for emergency fire and policing services. Existing fire and emergency services within the regional area include Langdon Fire Station 111 . The build out of the Glenmore Trail area will incorporate design to optimize the delivery of fire and protective services, in accordance with County policy.

POLICIES

- 5.7.1 The design and programming of the community shall not interfere with the delivery of any emergency services. Applicants must consult with emergency service providers and follow all County regulations at the local plan stage.
- 5.7.2 Policing will be provided by the RCMP as per the Provincial Police Service Agreement, to meet current and future needs until such time as another policing solution is required or sought out.
- 5.7.3 Emergency Fire services will be provided by existing County emergency fire service facilities where feasible, or by contract from adjacent municipalities.
- 5.7.4 Emergency service providers shall work with applicants to identify land for location of required facilities at the local plan stage.
- 5.7.5 Crime Prevention Through Environmental Design (CPTED) elements should be considered and incorporated into the design and construction of all new development, wherever possible.
- 5.7.6 All industrial and commercial buildings should provide fire suppression systems and they shall be in compliance with the County's Fire Suppression Bylaw.
- 5.7.7 Local plans shall address fire suppression requirements and ensure water and necessary infrastructure is available to all development. The fire suppression plan should consider opportunities and locations that allow for shared infrastructure between local plan areas.

PART III:

Implementation

6.0 Implementation

This Area Structure Plan outlines the vision for the future development of a mixed-use community along Glenmore Trail, providing guidance about infrastructure requirements, land use, subdivision, and development.

This section describes the implementation process to ensure that the development of the GT ASP area aligns with the goals in this document and realizes the vision of becoming a vibrant regional employment and service hub along Glenmore Trail.

6.1 Local Plans, Redesignation, Subdivision and Development Applications

Local plans within the GT ASP are to be developed using the framework provided in this document. The following policies outline the general requirements that must be addressed in local plans due to the location and specific conditions of the proposed development area. The standard technical requirements of a conceptual scheme or master site development plan are identified in the County Plan.

POLICIES

- 6.1.1 Fiscal impact to the County and market demand shall/should be determined through appropriate supporting studies at the local plan stage to support the proposed land use strategy.
- 6.1.2 Applications for redesignation, subdivision, and/or development require the concurrent or prior adoption of a local plan, unless otherwise directed by the policies of this plan or determined by the County not to be required.
- 6.1.3 Notwithstanding the policy above (REF), applications for a development permit in an area where land use has been approved prior to the adoption of this Plan do not require a local plan.
- 6.1.4 Local plans shall address and adhere to the requirements of the GT ASP. In support of local plans and redesignation applications, the applicant will be required to submit a rationale showing how their proposal is consistent with the vision and policies of the GT ASP.
- 6.1.5 Where a local plan does not exist or is silent on a subject, the policies of the GT ASP shall apply.
- 6.1.6 All local plans shall be developed in consultation with the County.
- 6.1.7 The preferred minimum planning area for local plans is one quarter section (160 acres) in size, but the County may consider applications for smaller local area plans.
- 6.1.8 Land use, subdivision, and development applications shall address and adhere to the requirements of the local plan and the policies of the GT ASP.
- 6.1.9 All planning or development applications, and any associated infrastructure construction, should meet the technical requirements of:
 - a) the County Plan,
 - b) the County Land Use Bylaw,
 - c) the GT ASP and associated technical studies,
 - d) County Servicing Standards,
 - e) Other County policies, and
 - f) provincial and federal requirements.
- 6.1.10 All local plans adopted by Council shall be appended, by bylaw, to this Area Structure Plan.

6.2 Intermunicipal Coordination and Cooperation

The plan area contains one intermunicipal interface with the City of Chestermere at the northwest corner. The Plan proposes residential land uses along this interface, which is intended to match the proposed land uses in the Chestermere Municipal Development Plan (MDP) for the adjacent areas within the City.

While it does not border any other municipal boundaries, the GT ASP takes into account the influence of its proximity to the City of Calgary. The County will coordinate appropriate development considerations for the area through higher level policy, collaboration through the Calgary Metropolitan Region Board, and ongoing discussions between the County and the City of Calgary.

POLICIES

- 6.2.1 Where planning processes within the GT ASP impact land that borders the City of Chestermere, the County shall consult the City.
- 6.2.2 Proponents of local plans that contain areas adjacent to the City of Chestermere shall work collaboratively with the County to address all relevant intermunicipal concerns .
- 6.2.3 Prior to approval of local plan applications adjacent to the City of Chestermere, the County shall ensure through policy that material cross boundary impacts are reviewed at subdivision stage, and that appropriate mechanisms are implemented through any subdivision approvals to address these impacts identified by the County.
- 6.2.4 The County should collaborate with the City of Chestermere and relevant provincial agencies to support the establishment of baseline conditions for infrastructure needs and environmental assets which assist in the planning and assessment of future growth and development within the plan area.

6.3 Phasing



Figure 16. Phasing

The Municipal Government Act states that an ASP must describe the sequence of development proposed for the area. This Plan recognizes that development within the GT ASP area should be phased in a logical and efficient manner, recognizing pace of development within the region, future land requirements, and logical extensions of transportation and servicing infrastructure.

Several factors contribute to the sequencing of development in Glenmore Trail, including varied land ownership, market conditions, servicing capacity and timing, and the timing of provincial highway projects, among others. Working with these factors, *Figure 16. Phasing* outlines a high-level proposed phasing of development within the plan area. Logical variations to the sequencing will be permitted by the County without amendment to this Plan.

Dashed area denotes "*limited servicing interim development*" - parcels that can be developed with interim servicing, but may have to connect once piped services are available for ultimate development

POLICIES

- 6.3.1 Phasing of development, including the preparation and sequence of local plans, should be in general accordance with *Figure 16. Phasing*.
- 6.3.2 Notwithstanding Policy (REF) above, variations to phasing may be permitted by the County based on additional servicing analysis, market studies, and other rationales provided within a local plan and may not require an amendment to this Plan.

6.4 Infrastructure Cost and Levies

The build out of the GT ASP area and implementation of the policies within this plan will require significant new infrastructure, as well as infrastructure improvements within and external to the plan area.

Anticipated “hard” infrastructure improvements may include upgrades or construction of intersections and interchanges, roads, water, wastewater, and stormwater. Anticipated “soft” infrastructure improvements may include police facilities, fire stations, and recreational facilities.

Development costs will be covered through a variety of revenue sources, including applicant-funded and -implemented improvements, development levies, County improvements, provincial contributions, special tax assessments and user fees.

The need, cost, and timing of infrastructure construction will vary with the type of infrastructure improvements and the development project. All levies are subject to periodic review and include development costs associated with internal and external improvements to service the plan area. Non-levy costs and improvements will be determined through periodic review of the master servicing documents, and at the local plan preparation stage.

POLICIES

- 6.4.1 As part of the local plan approval process, the identification, timing, and funding of any required off-site improvements relating to hard and soft infrastructure shall be required.
- 6.4.2 Off-site improvements that are:
- a) internal to the plan area will be determined to the satisfaction of the County;
 - b) external to the plan area, including provincial or adjacent community infrastructure, will be determined to the satisfaction of the County, in consultation with the relevant community an/or provincial department.
- 6.4.3 Costs associated with transportation and/or utility service infrastructure to support new development within the plan area should be the responsibility of the applicant. However, appropriate cost-sharing agreements between stakeholders, municipalities, and/or the Province may be explored as part of separate agreements for the specific infrastructure improvement.
- 6.4.4 Applicants relying on transportation and/or utility infrastructure improvements (water, wastewater, and/or stormwater) provided by other developments shall be required to pay cost recovery as per the requirements of the applicable cost contribution agreement, and in accordance with the County’s Cost Recovery Agreement policies.
- 6.4.5 Applicants relying on transportation and/or utility infrastructure improvements provided by other developments shall be required to pay proportionally allocated cost recovery as per the requirements of the applicable cost recovery agreement(s).
- 6.4.6 Development applicants shall be required to pay Rocky View County’s applicable:
- a) Water and Wastewater Off-Site Levy;
 - b) Stormwater Off-Site Levy;
 - c) Transportation Off-Site Levy; and
 - d) “Soft” Infrastructure Off-Site Levies.

6.5 Monitoring & Amendments

The progress in implementing the GT ASP will be monitored based on a number of performance measures including development activity and infrastructure expansion. Where necessary, Administration may make recommendations about how to manage growth in the plan area or how the Plan may be updated to meet changing circumstances. The vision, goals, and policies of the GT ASP are intended to address a multi-decade build-out of the area. While the Plan aims to provide flexibility to account for change over this time, periodic review, and occasional amendments to align with changes in the local or regional context may be required.

POLICIES

- 6.5.1 County Administration will report to Council on implementation of the GT ASP as part of Administration's yearly reporting on the overall implementation of the County Plan.
- 6.5.2 The GT ASP shall be subject to an assessment and review in accordance with the County Plan.

APPENDIX A: REGIONAL TRANSMISSION CORRIDORS SCHEDULES

Schedule 4: Mobility Corridors – Transit and Active Transportation

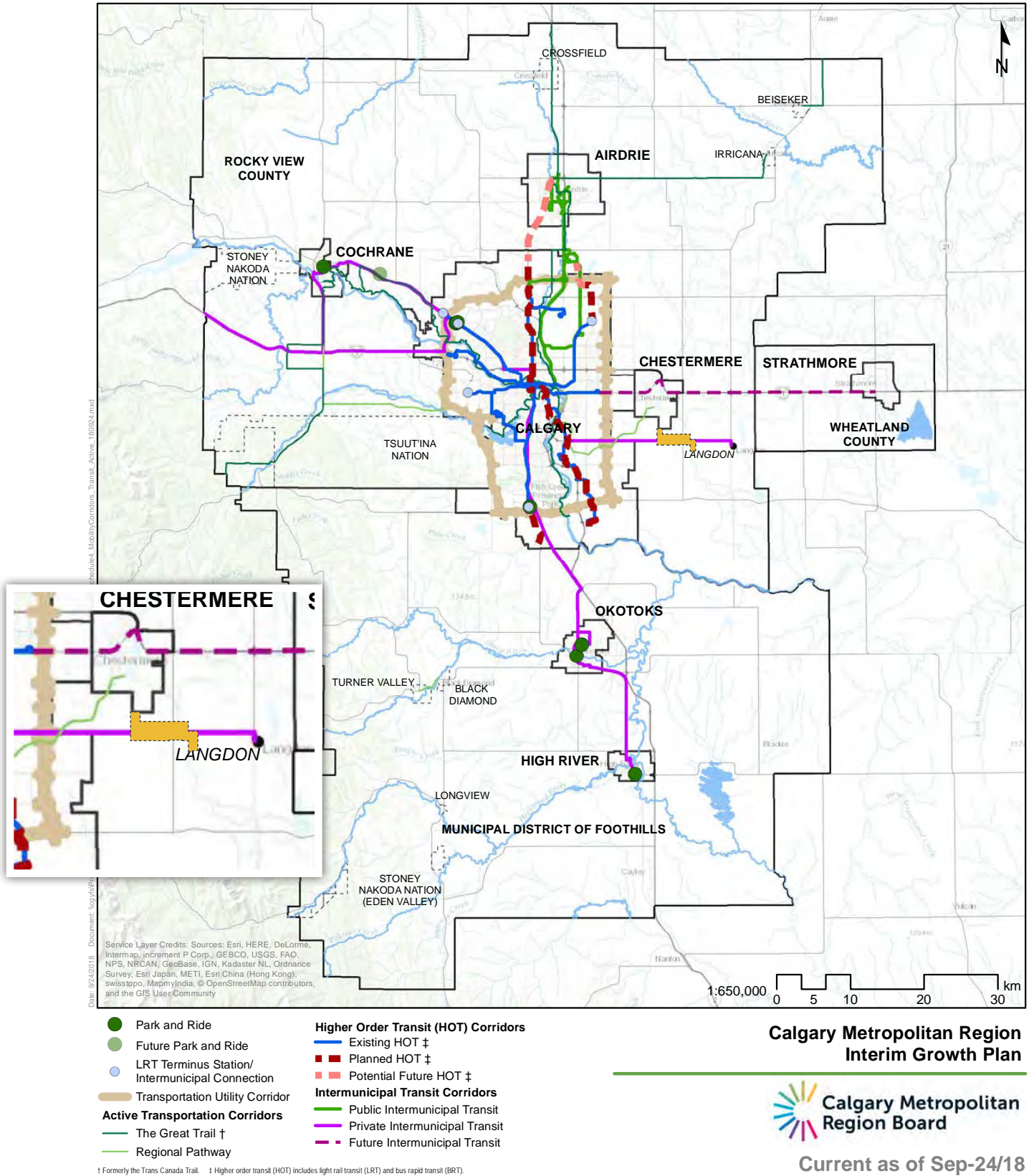
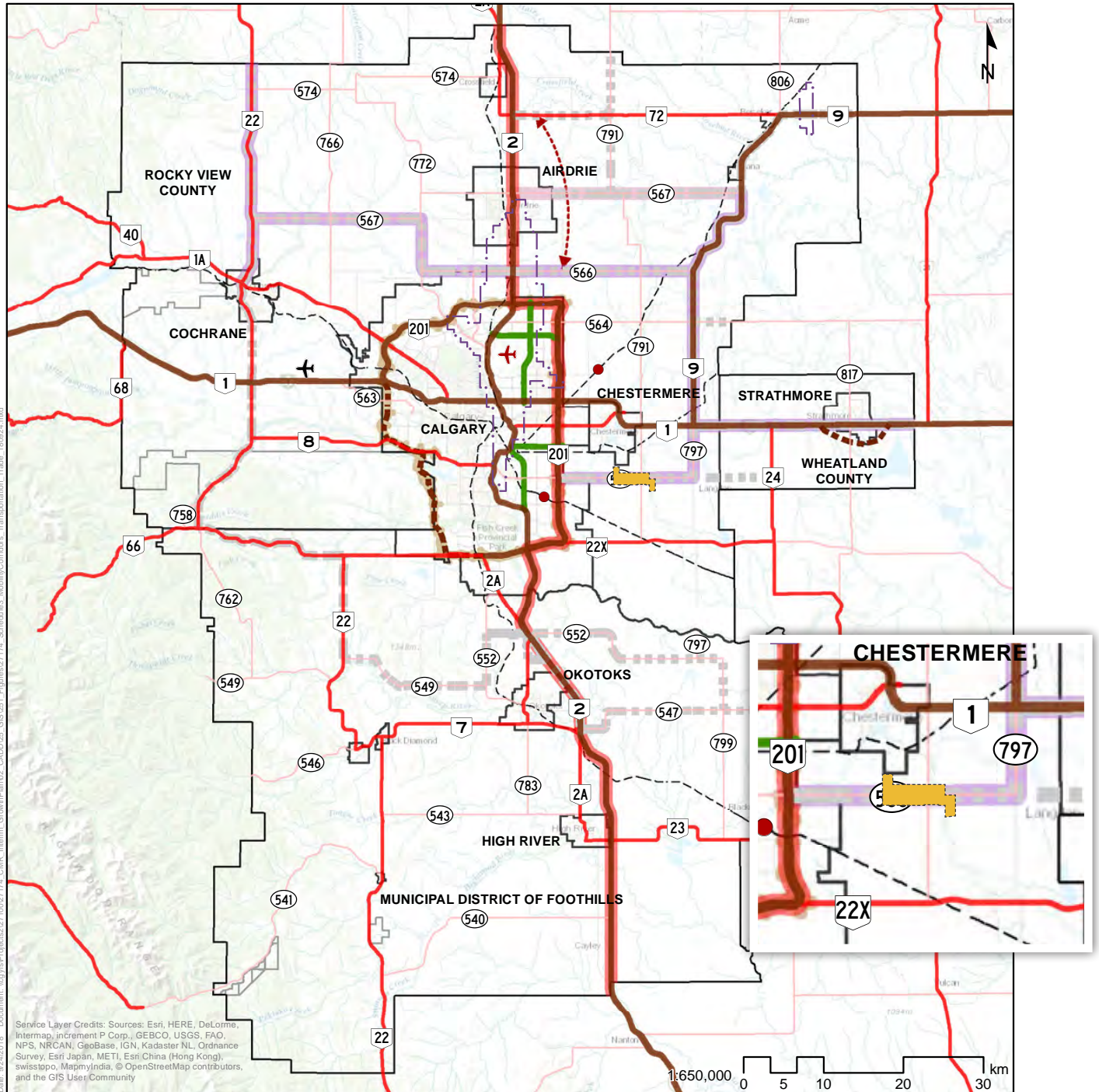


Figure A-1. Regional Transmission Corridors – Water (Schedule 5)

Schedule 3: Mobility Corridors – Transportation and Trade



Date: 9/24/2018
 Document: \gdrive\Projects\271002774_CMR_Initial_GrowthPlan02_CADD05_GBR\201_Figures\27174_Schedule3_MobilityCorridors_Transportation_Trade_180924.mxd
 Service Layer Credits: Sources: Esri, HERE, DeLorme, Intermap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, GeoBase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China (Hong Kong), swisstopo, MapmyIndia, © OpenStreetMap contributors, and the GIS User Community

- | | | |
|------------------------------------|--------------------------|-----------------------------------|
| ● Intermodal Facility | Transportation | Trade Corridors |
| ✈ International Airport | --- Rail | ● CANAMEX |
| ✈ Springbank Airport | — Level 1 Highway | — Existing High Load † |
| ⊕ Airport Vicinity Protection Area | — Level 2 Highway | — Proposed High Load † |
| — Transportation Utility Corridor | — Level 3 Highway | ■ Proposed Oversize/Overweight †* |
| | — Future Level 1 Highway | — Goods Movement |
| | ↔ Conceptual Bypass | |

Calgary Metropolitan Region Interim Growth Plan

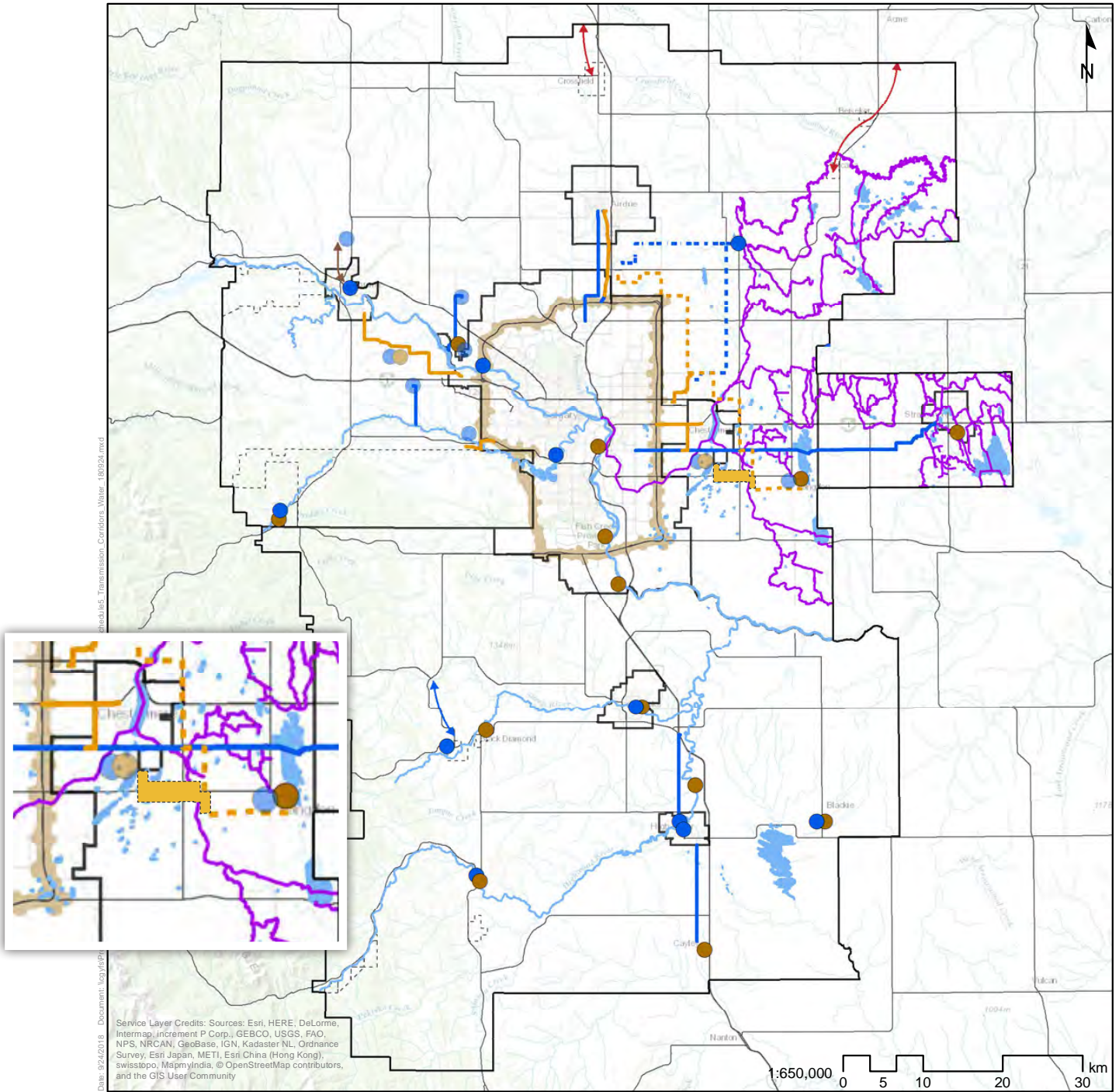


Current as of Sep-24/18

† Per AT's 2015 High Load Corridor on Provincial Highways map. † Per AT's 2018 High Load Corridor and Proposed Oversize / Overweight Corridor on Provincial Highways map.
 * Foothills County indicates it has not yet accepted or agreed to AT's proposal to use its 274 and 306 Avenues, and Highway 552 east of Highway 2 as Oversize/Overweight Corridors.

Figure A-2. Regional Transmission Corridors – Energy (Schedule 6)

Schedule 5: Transmission Corridors – Water



Date: 9/24/2018 Document: cgyfs/ps/ps Service Layer Credits: Sources: Esri, HERE, DeLorme, Intermap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, GeoBase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China (Hong Kong), Swisstopo, MapmyIndia, © OpenStreetMap contributors, and the GIS User Community

- | | | |
|---|---|--|
| <ul style="list-style-type: none"> Transportation Utility Corridor Treatment Plants Public Water Public Wastewater Private Water Private Wastewater | <ul style="list-style-type: none"> Water Corridors Intermunicipal Water Potential Intermunicipal Water Millarville Water Water Commission Line † Western Irrigation District System | <ul style="list-style-type: none"> Wastewater Corridors Intermunicipal Wastewater ‡ Potential Intermunicipal Wastewater Cochrane Lake Wastewater ‡ |
|---|---|--|

† Water commission lines are conceptual until accurate alignments received from the commissions. ‡ Alignment of wastewater line servicing Cochrane Lake is conceptual.

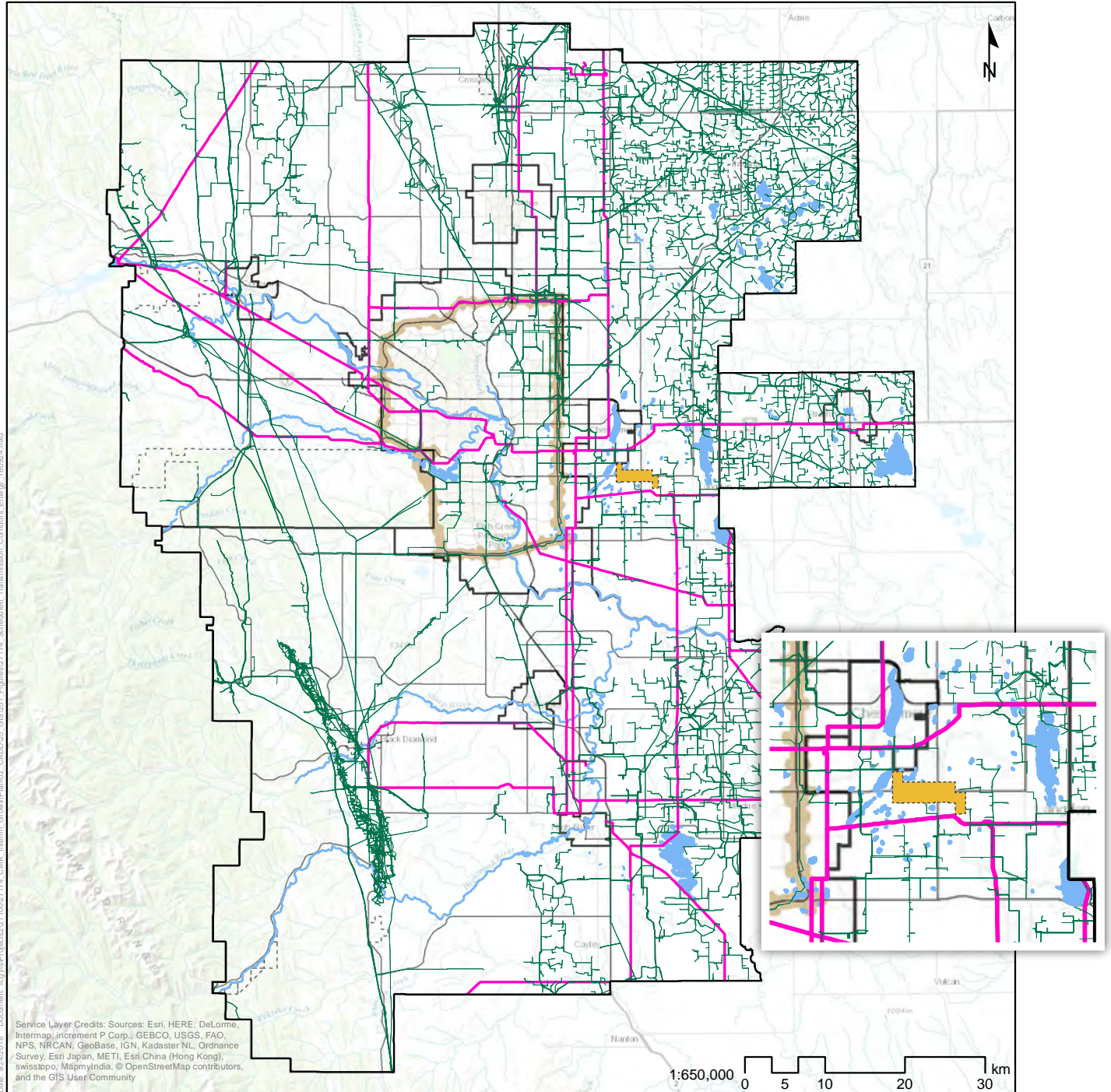
Calgary Metropolitan Region Interim Growth Plan

Calgary Metropolitan Region Board

Current as of Sep-24/18

Figure A-3. Regional Transmission Corridors – Water (Schedule 5)

Schedule 6: Transmission Corridors – Energy



- Transportation Utility Corridor
- Energy Corridors**
- Power Transmission Line
- Pipeline

**Calgary Metropolitan Region
Interim Growth Plan**



Current as of Sep-24/18

† Water commission lines are conceptual until accurate alignments received from the commissions. ‡ Alignment of wastewater line servicing Cochrane Lake not yet confirmed.

Figure A-4. Regional Transmission Corridors – Energy (Schedule 6)