



# BYLAW C-8174-2021

A Bylaw of Rocky View County, in the Province of Alberta, known as the **East** Highway 1 ~~East~~ Area Structure Plan, pursuant to Section 633 of the *Municipal Government Act*.

The Council of Rocky View County enacts as follows:

## Title

1 This Bylaw may be cited as “**East** Highway 1 ~~East~~ Area Structure Plan”.

## Definitions

2 Words in this Bylaw have the same meaning as those set out in the *Municipal Government Act* except for the definitions provided below:

- (1) “**Council**” means the duly elected Council of Rocky View County;
- (2) “**Municipal Government Act**” means the *Municipal Government Act*, RSA 2000, c M-26, as amended or replaced from time to time; and
- (3) “**Rocky View County**” means Rocky View County as a municipal corporation and the geographical area within its jurisdictional boundaries, as the context requires.

## Effect

3 THAT Schedule ‘A’ to Bylaw C-8174-2021 is adopted as the “**East** Highway 1 ~~East~~ Area Structure Plan” to provide a policy framework for land use, subdivision, and development in a portion of central Rocky View County.

## Transitional

4 Bylaw C-8174-2021 passed and comes into full force and effect when it receives third reading and is signed in accordance with the *Municipal Government Act*.

READ A FIRST TIME IN COUNCIL this 27 day of April , 2021

*PUBLIC HEARING WAS HELD IN COUNCIL this day of , 2021*

READ A SECOND TIME IN COUNCIL this day of , 2021

READ A THIRD TIME IN COUNCIL this day of , 2021

---

Reeve

---

CAO or Designate

---

Date Bylaw Signed

**SCHEDULE 'A'**

**FORMING PART OF BYLAW C-8174-2021**

An Area Structure Plan to guide land use, subdivision and development in the eastern portion of Rocky View County, within an area along the Highway 1 corridor, herein referred to as the **East** Highway 1 ~~East~~ Area Structure Plan.



# EAST HIGHWAY 1

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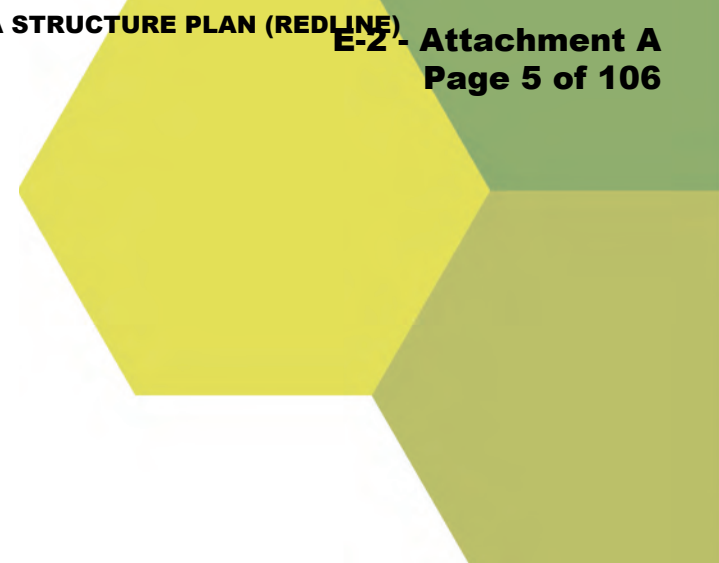
## AREA STRUCTURE PLAN

Draft 8: July 9, 2021

REDLINE VERSION



ROCKY VIEW  
COUNTY



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# ACKNOWLEDGMENTS

## Rocky View County Council

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- Division 2: Kim McKylor (Deputy Reeve)
- Division 3: Kevin Hanson
- Division 4: Al Schule
- Division 5: Jerry Gautreau
- Division 6: Greg Boehlke
- Division 7: Daniel Henn (Reeve)
- Division 8: Samantha Wright
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- Terradigm Development Consultants
- MVH Urban Planning & Design Inc.
- IDEA Group Inc.
- JCB Engineering Ltd.

## Lead Ownership and Development Team:

- Meadow Lake Estates Chestermere Ltd.
- GSL Sunbelt Development Ltd.
- Aman Brar

---

## Indigenous Acknowledgement

In the spirit of reconciliation, we acknowledge that we live, work and play on the traditional territories of the Blackfoot Confederacy (Siksika, Kainai, Piikani), the Tsuut'ina, the Îyâxe Nakoda Nations, and on the homeland of the Métis Nation of Alberta Region No. 3.

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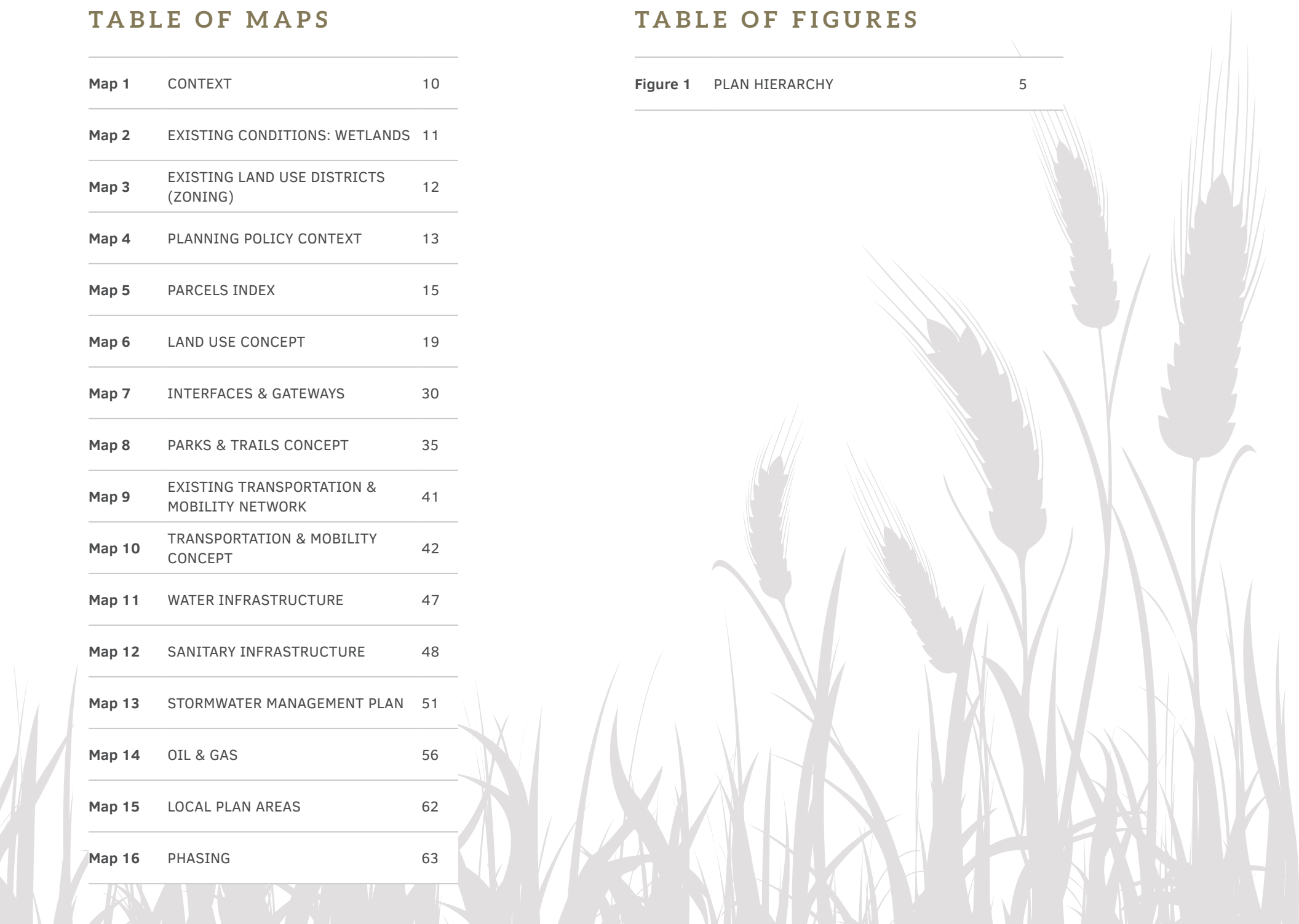
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## SUPPORTING REPORTS

(under separate cover)

Report	Author
Biophysical Inventory	Trace Associates Inc.
Transportation Impact Assessment	JCB Engineering Ltd.
Stormwater Management Study	IDEA Group Inc.
Servicing Study	IDEA Group Inc.



# EXECUTIVE SUMMARY

## VISION

*These lands are envisioned to become a part of the 'Sunbelt', an intermunicipal agricultural-industrial area, regional landmark, and a unique employment and business **hub corridor** in Rocky View County and Wheatland County. The significance of this area as a vital part of food security, research, and production is unmatched in the Calgary Region.*

## Purpose

The East Highway 1 Area Structure Plan outlines the future vision for development of the Plan area including land use, transportation, natural environment, emergency services and servicing requirements. This ASP provides a guide for Council and Administration when reviewing planning and development applications including local plans, land use amendments and subdivision. When development is considered in the East Highway 1 area, Council must consider the proposed plan within the context of this ASP as well as many other factors including the Municipal Development Plan, Local Plans, and the ability to provide servicing.

## Size, Location and Existing Land Use

The East Highway 1 Area Structure Plan area includes approximately **1,560 hectares (3,855 acres)** ~~1,482.1 hectares (3,662.2 acres)~~ of land in the central east portion of Rocky View County. It is located immediately south of Highway 1, between Highway 791 and the border with Wheatland County at Boundary Road, north of Township Road 240.

## Planning Context

The East Highway 1 ASP is consistent with the policies of the Interim Growth Plan (IGP) of the Calgary Metropolitan Region Board, but **will may** require further consideration as a Joint Planning Area (JPA) to align with the policies of the draft Regional Growth Plan currently under consideration. A portion of the ASP area falls within the Rocky View-Wheatland County Intermunicipal Development Plan (IDP) study area, which is currently undergoing the draft process. ~~The East Highway 1 Industrial area is designated as an 'Employment Area' within the Rocky View County draft Municipal Development Plan (MDP) currently undergoing final approval. Pending MDP approval the ASP looks to Rocky View County's County Plan and aligns with the policies of the Plan.~~

## Development Framework

The overall development intent for the East Highway 1 ASP is to create an agri-business **hub corridor** for the Calgary Region that provides a unique mix of agri-business related land uses, supportive commercial and industrial uses, agriculture food production, agricultural land conservation, rural residential uses, and conservation of major wetland systems.

# INTRODUCTION



# 1.0

## PLAN ORGANIZATION

The East Highway 1 Area Structure Plan (the Plan or ASP) is organized in three parts followed by the appendices.

### PART I: INTRODUCTION

This Part outlines the Plan's purpose, boundaries, policy terminology, relationship to other plans, the public engagement process, key issues, opportunities, and design ideas that informed the Plan's preparation process. It also contains a description of the development of the East Highway 1 area from its early beginnings to today. Finally, it presents a vision of what East Highway 1 will be like in the future.

### PART II: PLAN POLICIES

This Part is the core of the Plan, containing the policy direction to guide development in the East Highway 1 Plan Area; it sets out the land use, servicing, and infrastructure strategy for the area. Each section contains a description of its purpose and intent, a summary of relevant technical studies, a list of objectives, and a series of policies addressing the subject matter.

### PART III: IMPLEMENTATION

This Part presents the Plan implementation process, covering the following items:

- Local plan areas and requirements;
- Plan monitoring and review;
- Actions for further work that will supplement the Plan policies and assist in achieving the Plan vision, goals and objectives; and
- Intergovernmental affairs and regional planning considerations.

### APPENDICES

Appendix A: Definitions

Appendix B: Open House Summary

Appendix C: Policy Direction

Appendix D: Regional Context Maps

## 2.0

## PLAN PURPOSE

Section 633 of the Municipal Government Act states:

For the purpose of providing a framework for subsequent subdivision and development of an area of land, a council may by bylaw adopt an area structure plan.

An **area structure plan** (ASP),

- a. Must describe:
    - i. The sequence of development proposed for the area.
    - ii. The land uses proposed for the area, either generally or with respect to specific parts of the area.
    - iii. The density of population proposed for the area either generally or with respect to specific parts of the area.
    - iv. The general location of major transportation routes and public utilities.
- and
- b. May contain any other matters, including matters relating to reserves, as the council considers necessary.

When making decisions regarding development within an area structure plan, Council must consider the plan and a wide range of other factors such as the goals of the County, regional growth, and the ability to provide servicing.

### LOCAL PLANS

For brevity, this document uses the term **local plan** to refer to a **conceptual scheme** or **master site development plan**. The County anticipates the majority of **local plans** within the East Highway 1 Area Structure Plan boundary will be submitted as **conceptual schemes**.

FIGURE 1 | PLAN HIERARCHY





## CONCEPTUAL SCHEME

A conceptual scheme is a non-statutory plan, subordinate to an area structure plan. It may be adopted either by bylaw or by a resolution of Council. A conceptual scheme is prepared for a smaller area within an area structure plan boundary and must conform to the policies of the area structure plan. Conceptual schemes provide detailed land use direction, subdivision design, and development guidance to Council, Administration, and the public.

If a conceptual scheme area is of sufficient size that further detail is required for specific areas and phases, the conceptual scheme may identify smaller sub-areas and provide detailed guidance at that level. These smaller sub-areas are referred to as development cells.

## MASTER SITE DEVELOPMENT PLAN

A master site development plan is a non-statutory plan that is adopted by Council resolution. A master site development plan accompanies a land use redesignation application and provides design guidance for the development of a large area of land with little or no anticipated subdivision. A master site development plan addresses building placement, landscaping, lighting, parking, and architectural treatment. The plan emphasis is on-site design with the intent to provide Council and the public with a clear idea of the final appearance of the development.

## 2.1 PLAN INTERPRETATION

The following describes the meaning of some of the key words that are contained in a policy:

*Shall*: a directive term that indicates the actions outlined are mandatory and therefore must be complied with, without discretion, by Administration, the developer, the development authority, and subdivision authority.

*Should*: a directive term that indicates a strongly preferred course of action by Council, Administration, and/or the developer, but one that is not mandatory.

*May*: a discretionary term, meaning the policy in question can be enforced by the County if it chooses to do so, dependent on the particular circumstances of the site and/or application.

## 3.0

## DESIGN PRINCIPLES &amp; VISION

## 3.1 DESIGN PRINCIPLES

This plan embraces development principles that incorporate sustainability, resilience and climate change directions that should guide responsible development of individual parcels as well as contribute to the overall look and feel of the East Highway 1 project area. These design principles could be achieved by using techniques in the Design Guidelines found in **Section 24: Agricultural Industrial Design Guidelines**.



**Optimize roads and access:** The road systems shall parallel Highway 1 to prioritize use outside and inside the site, minimize external impacts and direct traffic efficiently.



**Maximize environmental sensitivity:** The major wetlands shall be conserved as part of a natural local habitat and stormwater connected system.



**Create a connected trail and pathway network:** The wetland and drainage infrastructure networks should be integrated with the proposed trails and pathways system and extended to all parts of the East Highway 1 community.



**Maximize green and the quality of streetscape:** The streetscapes should include tree/shrub planting and prairie earth forms (earth berms) to help screen storage areas while ensuring that the planting does not obscure the visibility and accesses to the adjoining businesses.



**Provide development flexibility:** The lots and associated uses should be flexible in configuration and size so that they can both respond to the changing market demands and growth of companies within the site.



**Provide safe and social public places:** The development of public places (e.g., the development of the local commercial areas) should be situated so there is a specific level of natural amenity, convenience, and sense of local meeting places within the East Highway 1 area.



**Minimize impacts of development:** The East Highway 1 Plan area should be sensitive to adjoining uses and introduce vegetative and other buffers where necessary to minimize noise and visual intrusions into the prairie landscape and the existing rural residential uses.



**Retain surrounding rural agriculture character:** The development strategy shall provide for plentiful open space that reflects the rural agricultural roots of the area and County.



**Create enduring value:** Public and private investment in public infrastructure will be well-planned to gain maximum value especially for public safety and amenity in the area.

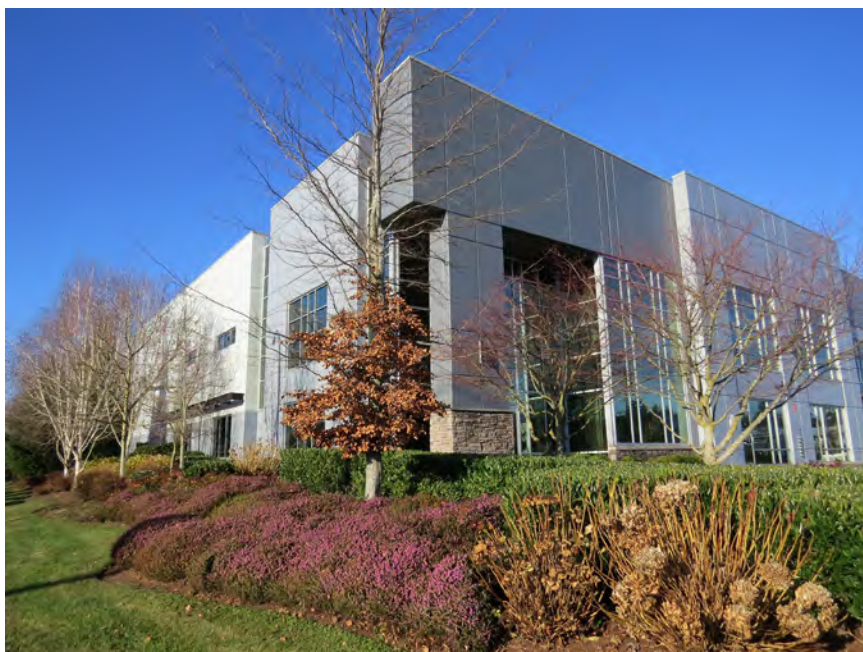


**Economic Diversity and Fiscal Sustainability:** Aligning with the County Plan, the development strategy shall provide for economic ventures that diversify the County economy and facilitate private and public investment in the County.

## 3.2 VISION - THE SUNBELT

### Two Questions for a More Resilient and Responsible Food Secure Future

- What if, instead of transporting our goods from across continents to our kitchen tables, we were able to produce more of those goods ourselves, closer to home?
- What if, in the growing households of Calgary, we “ordered out” less from across the world, and met more of our needs from our own regional “kitchen”?



*Industrial design precedent - Campbell Heights Industrial, Surrey BC.*

### The Agricultural Business **Hub Corridor** in the Calgary Region

This agri-business **hub corridor** would be the place where supplier meets consumer, tractor meets drone, ancient farm practice meets a new “cubic acre”, and land use policies meet land use possibilities. Here an emphasis is placed on achieving better workflow for moving goods and people. Consideration is given to some of the ideas currently being espoused in urban planning to increase density and reduce sprawl but applied in different ways to increase the agricultural output of a smaller footprint of land.

This Area Structure Plan is located within an area and developed under a vision we call “The Sunbelt”. Conceived as a regional approach toward achieving a more circular economy, The Sunbelt stretches across four municipalities, from Chestermere to Strathmore, in one of Canada’s sunniest locations, bordering rail and the Trans-Canada Highway, just minutes from Calgary’s Stoney Trail.

This location appears to be the perfect intersection: of transport, convenience, and need. It will harbour the next generation of agriculture that is more compact, productive, and local in a smaller ecological footprint. This is the doorstep from which we can lead, plan, and build to meet the needs of the next generation of Rocky View and Region residents. We are thinking globally but acting locally.

Welcome to the

# SUNBELT

*A place of forward-thinking Agriculture where:*



The business of agriculture grows and flourishes.



Waste is reused and recycled.



Innovative technology is nurtured.



A new industry is cultivated for the benefit of our community, and the world.

*The Gateway to the Calgary Region's Food Secure Future.*



# 4.0

## EAST HIGHWAY 1 CONTEXT

The site is located between the City of Chestermere to the west (800m away) and adjacent to Wheatland County's border on the east (**See Map 1: Context**). Major highways play a key role in this context with Highway 1 being the main east-west transportation corridor on the northern border of the site and Highway 9/797 being a major north-south link.

The Plan area and surrounding context is mainly rural in nature with agriculture and country residential uses dominant. Limited industrial development has occurred near the Wheatland County border and along Highway 1 within Wheatland County.

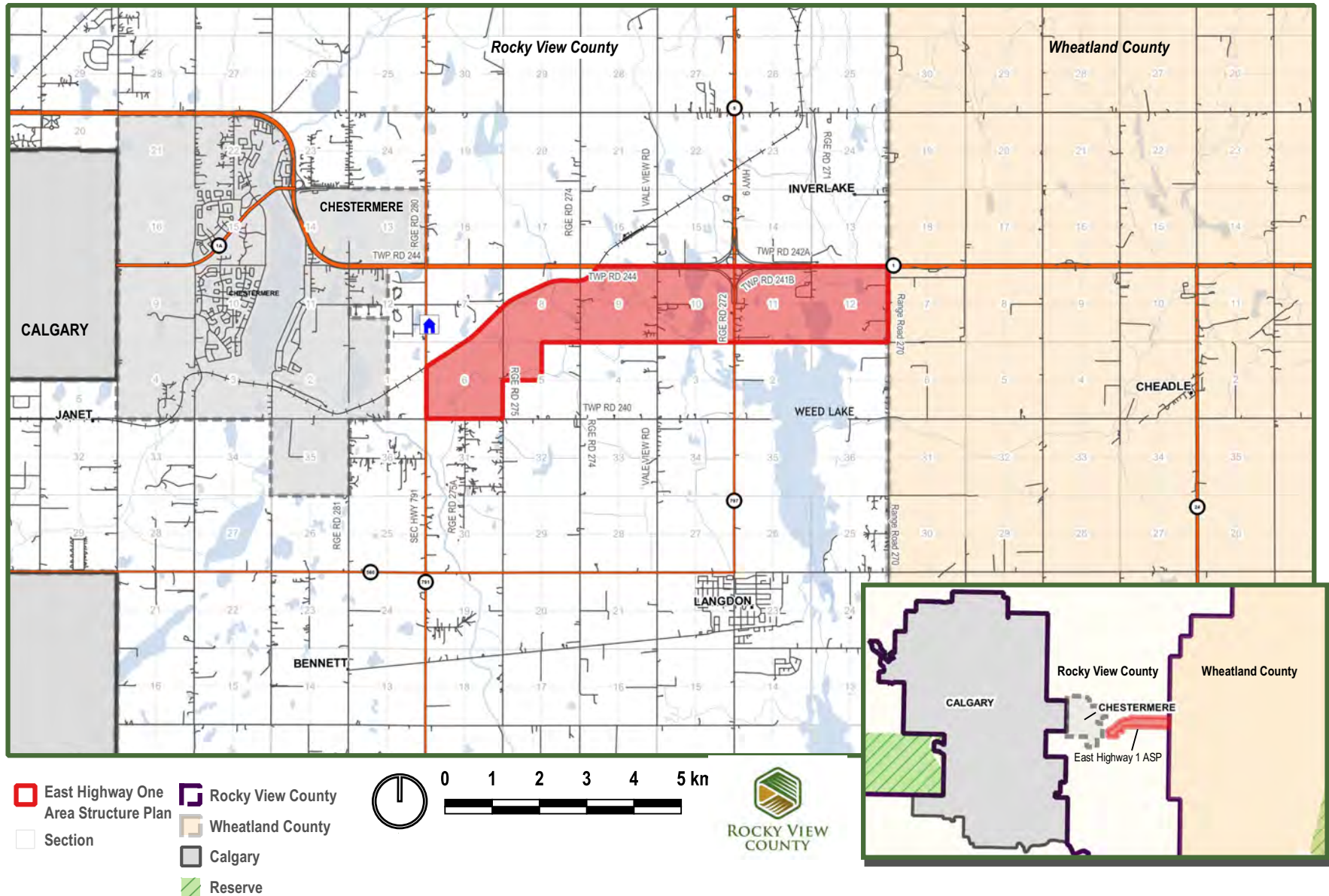
Chestermere High School is located to the west along Range Road 791 outside the Plan area. Highway 791 is a busy road with the school and commuter traffic to Calgary that wishes to avoid Highway 1.

Township Road 240 runs east-west along the south edge with Range Road 274 and Range Road 275 entering the ASP area from the south. Township 240 connects to the Janet Industrial area, a major industrial park in Rocky View County near the City of Calgary border.

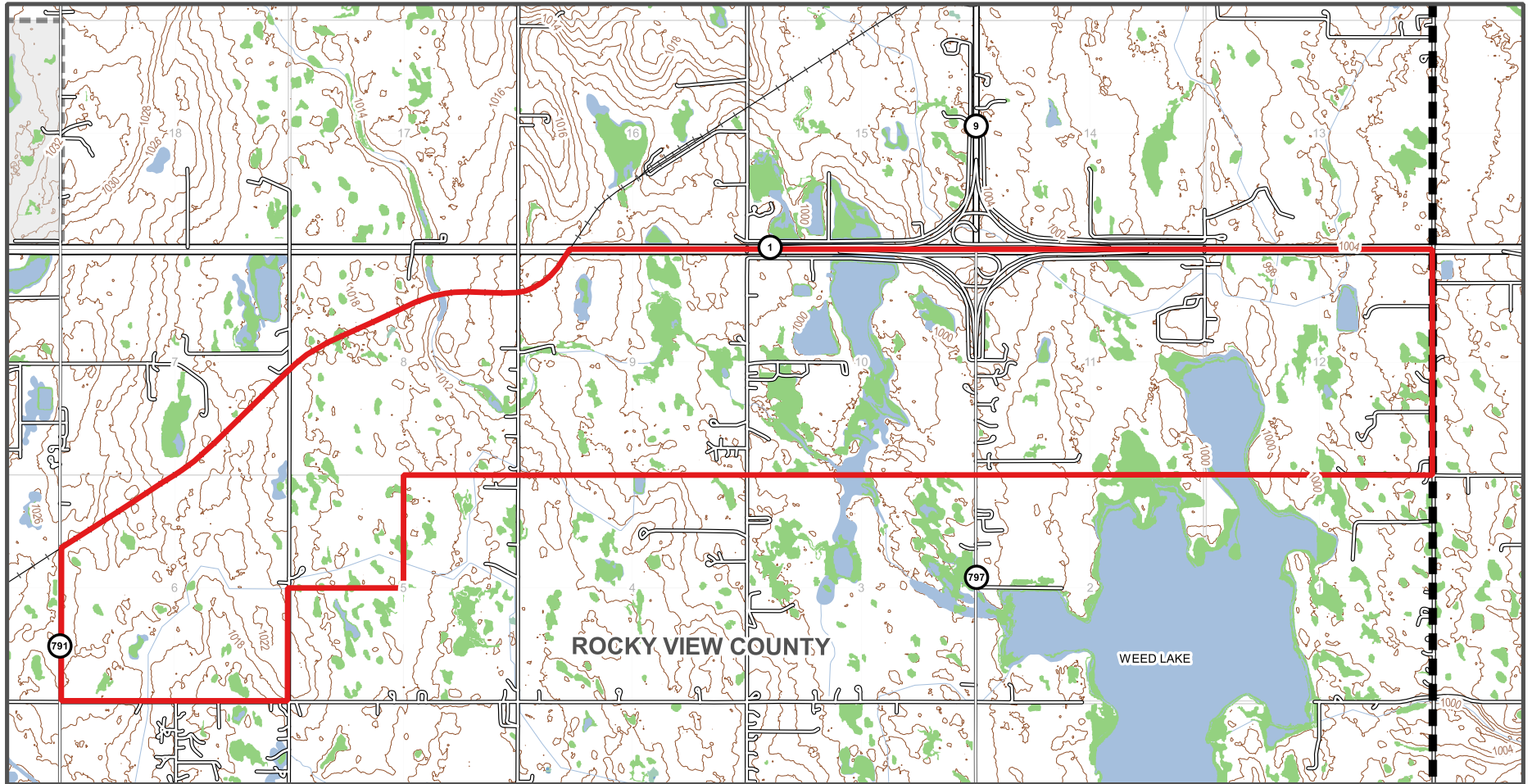
**Two major wetland areas affect the plan area: one borders Highway 797 to the west and Weed Lake forms part of the eastern sector of the site (see Map 2: Existing Conditions -Wetlands).**



MAP 1 | CONTEXT



MAP 2 | EXISTING CONDITIONS: WETLANDS



**Existing Conditions: Wetlands**

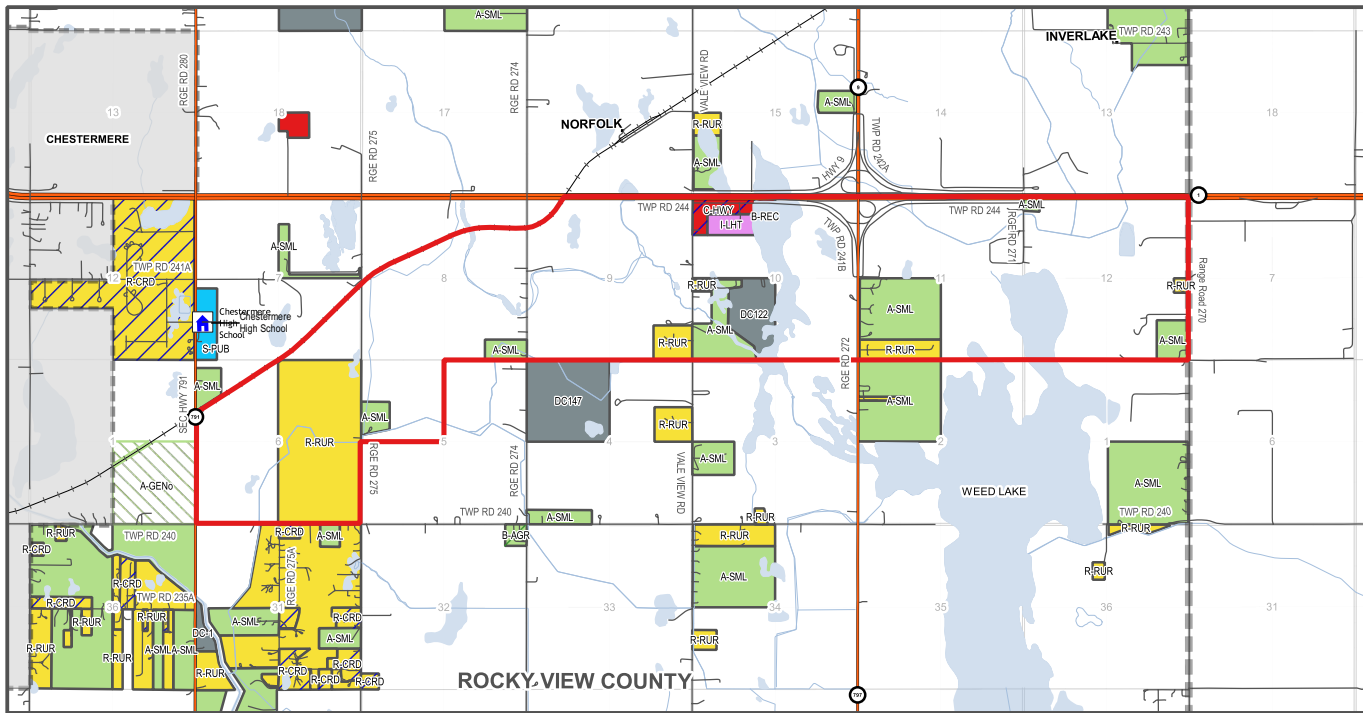
- |                     |               |            |
|---------------------|---------------|------------|
| Area Structure Plan | Local road    | Fen        |
| Railway             | County Border | Marsh      |
| Primary Highway     | City Boundary | Open water |
| Secondary Highway   | Contours      | Swamp      |



### 4.3 EXISTING LAND USE

On September 8, 2020 the County adopted a new *Land Use Bylaw* (LUB), which changed many of the land use districts within the LUB. Existing land use districts (zoning) within the ASP and surrounding areas include Residential Rural (R-RUR), Agricultural (A-MSL), Light Industrial (I-LHT), Commercial Highway (C-HWY), Business (B-REC), and a Direct Control district (DC-122) (see **Map 3: Existing Land Use Districts (Zoning)**). **Wheatland County's West Highway 1 ASP borders the plan area to the east (see Map 4: Planning Policy Context).**

**MAP 3 | EXISTING LAND USE DISTRICTS (ZONING)**

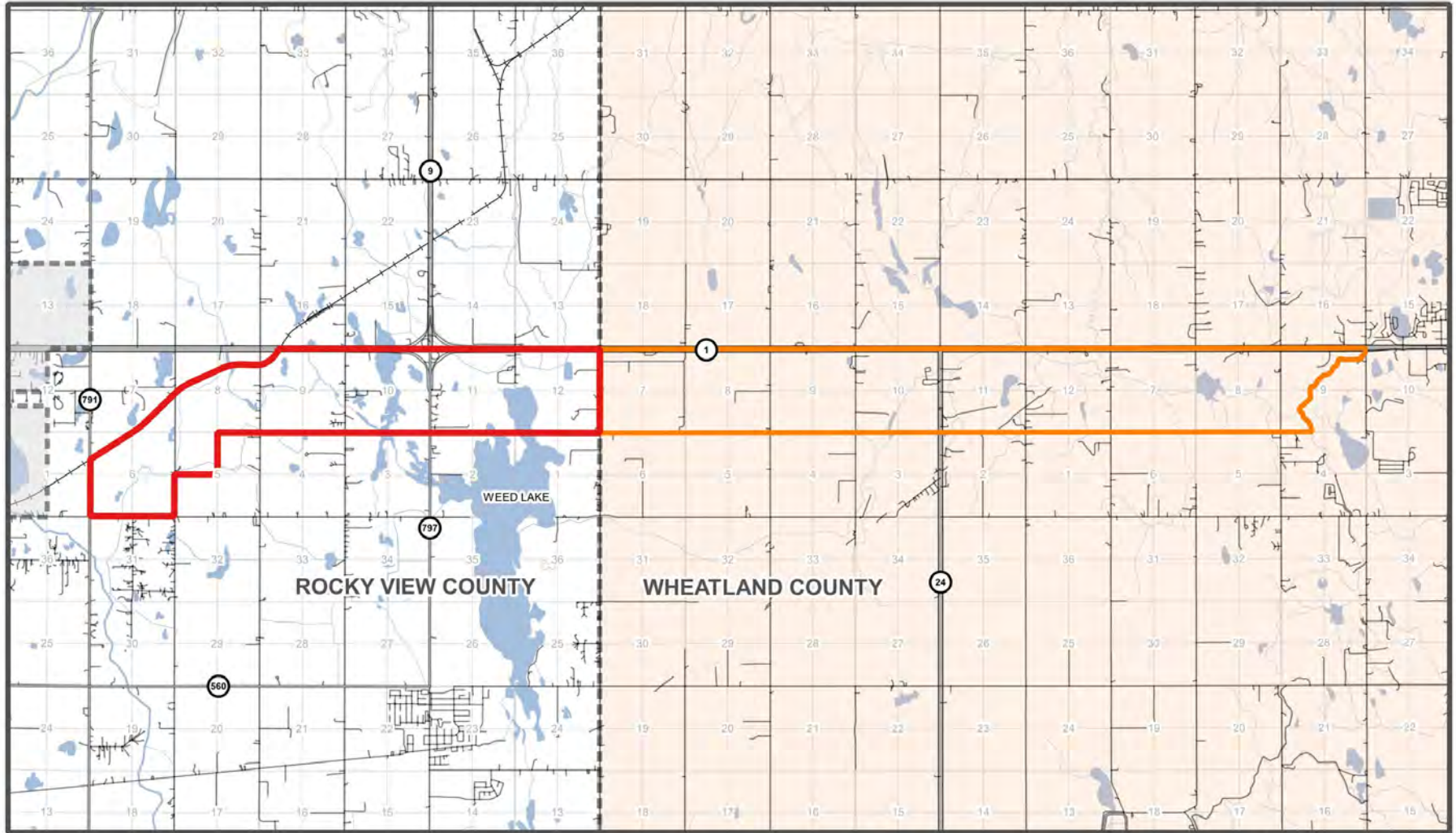


**Existing Land Use**

- |   |  |  |
|---|--|--|
| Area Structure Plan                       | C-HWY, Commercial, Highway District      | I-LHT, Industrial, Light District                |
| Agriculture, general district             | C-LRDs, Commercial, Local Rural District | R-CRD, Residential, Country Residential District |
| A-GENo, Agriculture, General District     | DC-1, Direct Control Bylaw               | R-RUR, Residential, Rural District               |
| A-SML, Agriculture, Small Parcel District | DC-122, Direct Control Bylaw             | S-PUB: Special, Public Service District          |
| B-AGR, Business, Agriculture District     | DC-142, Direct Control Bylaw             |  |
| B-REC, Business, Recreation District      | DC-147, Direct Control Bylaw             |  |

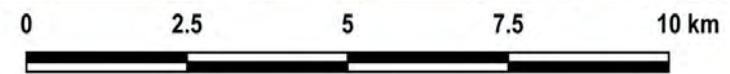


MAP 4 | PLANNING POLICY CONTEXT



**Planning Policy Context**

- Rocky View East Highway 1 ASP
- Wheatland West Highway 1 ASP
- Rocky View County
- Wheatland County
- City
- Access Railway
- Highway
- Local Road



# 5.0

## PLAN AREA

The ASP area consists of primarily un-subdivided quarter sections, larger farming parcels, and a few smaller parcels with agricultural or country residential land uses (see **Map 3: Existing Land Use Districts**). The exception are a few light industrial and commercial subdivided parcels just west of Highway 9/797 bordering on Highway 1.

Highway 9 / 797, a significant north-south secondary highway, intersects Highway 1 within the eastern portion of the Plan area. The western boundary of the Plan area is Highway 791, which comes within 800m (0.5 mile) of the border of the **Town City** of Chestermere. In the western portion of the Plan area the CN Railway tracks follows a portion of the northwest boundary until it intersects with Highway 1. The Plan area also features Weed Lake, a prominent water body / wetland in the east portion of the ASP area, and another significant wetland along the west side of Highway 9/797 up to Highway 1.

### PLAN AREA MAPS

The boundaries and locations of areas shown on the maps within the Plan boundary are not intended to define exact areas except where they coincide with clearly recognizable features or fixed boundaries such as municipal boundaries, property lines, or road or utility rights-of-way. Furthermore, the locations of symbols depicting specific features on the maps are approximate only, not absolute, and should be interpreted as such. The precise location of these boundaries and areas will be determined by the County at the time of consideration and approval. **Approximately 27% or 385.1 hectares are controlled by three landowners actively participating in this ASP (see Map 5: Parcels Index).**



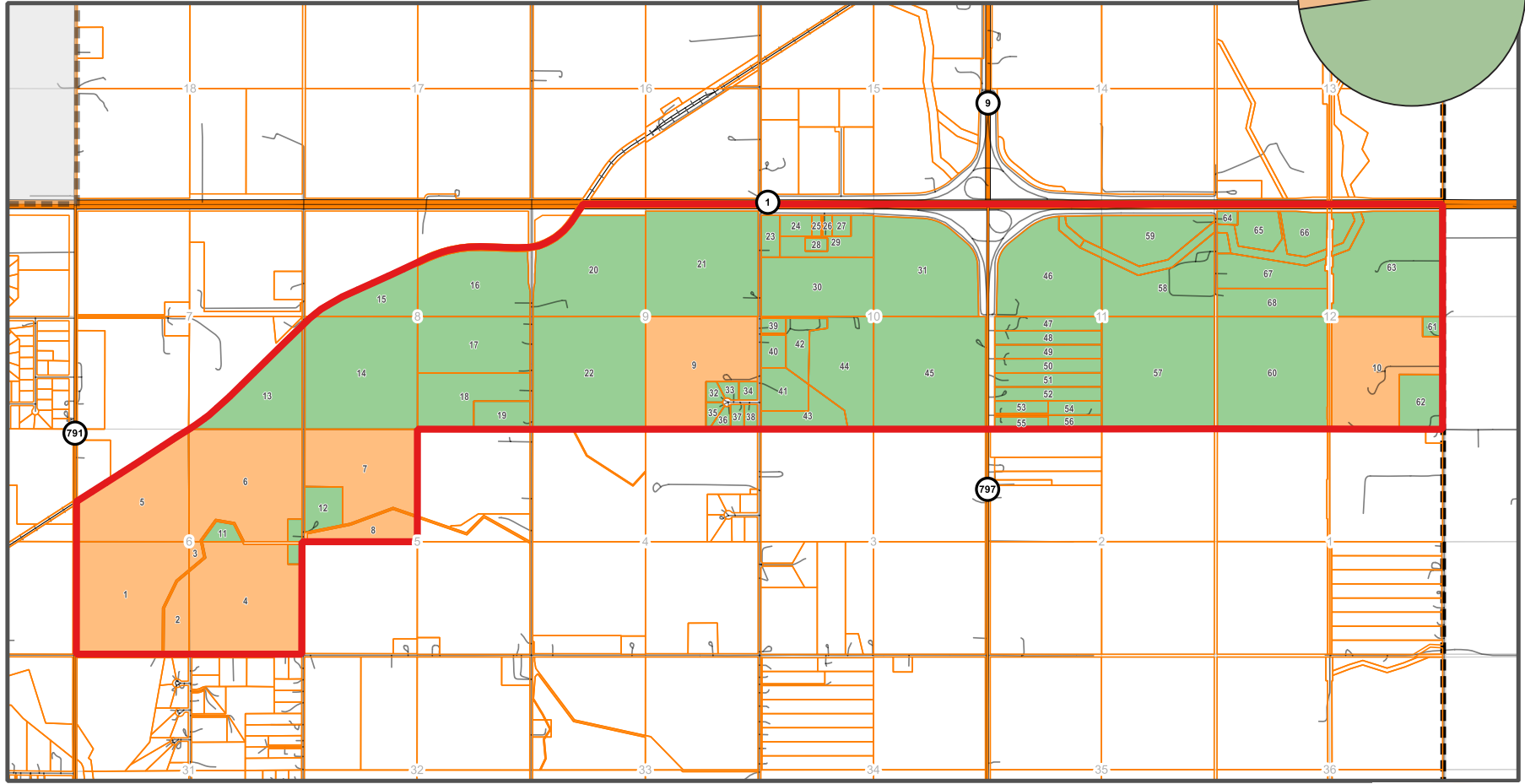
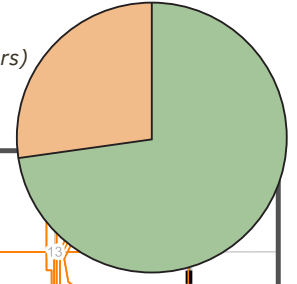
Image Courtesy of Google Earth - from Highway 797 looking west.



Image Courtesy of Google Earth - from Range Road 270 looking west.

MAP 5 | PARCELS INDEX

27% (385.1 ha Participating Landowners)



**Ownership Index**

- Area Structure Plan
- Landowner actively participating in ASP
- Other landowners (non active) in ASP



# PLAN POLICIES: LAND USE





## 6.0

# LAND USE STRATEGY

## Development Intent:

The overall development intent for the East Highway 1 ASP is to create an agri-business **hub corridor** for the Calgary Region that provides a unique mix of agri-business related land uses, supportive commercial and industrial uses, agriculture food production, temporary agricultural land conservation, rural residential uses, and conservation of major wetland systems.

## Land Use Concept:

The Land Use Concept (see **Map 6: Land Use Concept**) has been based on the following principles and ideas:

- **Retain and enhance rural and agricultural character and landscape** by allocating **40.5% 51% (677 ha)** of lands for agricultural and environmental uses (See **Table 2: Land Use Distribution**), and encouraging design elements found in **Section 24: Agricultural Industrial Design Guidelines**.
- **Create a central utility and transport corridor to service the area.** A high-capacity east-west transportation corridor is proposed to service the site (see **Map 10: Transportation and Mobility Concept**).
- **Provide a potential rapid-bus route and associated park and ride hubs.** As development occurs regional transportation connections should be considered.
- **Retain rural residential uses as part of the overall vision and growth strategy.** 221 hectares of Rural Residential uses are retained.
- **Provide the potential for micro-farming uses.** Agricultural uses are strategically located near light industrial and commercial uses to allow space for agricultural use to evolve with the community and ag-related technology.

- Provide the potential for research component involving the province, universities/colleges and possibly the federal government.
- Encourage the development of an Agri-Plex and other local amenities such as two conservation areas and associated trails and interpretive areas. See **Map 8: Parks and Trails Concept** for potential trail and activity node locations.
- Encourage development options that have a foundation in waste reduction, reuse, recycling and a circular economy.
- Capitalize on potential railway sidings and associated industrial uses. The lands adjacent to the railway corridor are designated as medium industrial in **Map 6: Land Use Concept** to allow for potential intermodal collection and distribution.
- Provide compatible commercial and local support services such as truck services at the west, central, and east end of the site.

## DEVELOPMENT FRAMEWORK

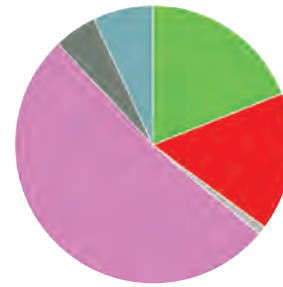
As shown on **Map 6: Land Use Concept** the site has been divided into three districts as follows:

1. **West District:** From Highway 791 to Range Road 274.
2. **Central District:** From Range Road 274 to Highway 797.
3. **East District:** From Highway 797 to Range Road 270.

The districts have been created based on location, size and future distinct and unifying uses. By balancing the distribution of uses each district has the potential to be a more complete rural community.

District by district potential land use distribution:

- **West District:** Includes light (M1) and medium (M2) industrial uses along the rail tracks and around the periphery. Commercial (C) uses anchor the district in the southwest corner and agricultural (Ag1) uses are retained in the centre. There is the potential for agriculture related recreation (Ag R M1) uses such as an Agri-Plex as well as agri-business uses within the industrial and commercial sectors.
- **Central District:** Retains a significant amount of agricultural uses and rural residential (RR) in the short to medium term while creating opportunity for intensification of agri-business uses such as micro-farming. Commercial use along Highway 797 and pockets of light industrial keep the district vibrant and active. Retained wetlands are surrounded by environmental reserve and will act as an amenity creating opportunities for recreation with public pathways.
- **East District:** The northern half of this district houses agricultural lands with the potential for intensification and agri-business uses. Existing rural residential is retained with opportunities for agri-business related uses, and light industrial anchors the district in the southeast corner. The northern portion of Weed Lake would be retained as environmental lands for utility uses.



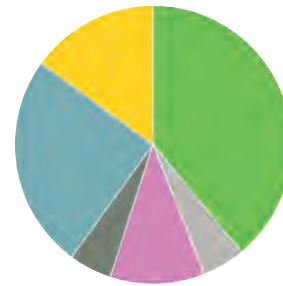
**West District Top 3 Uses:**

- 52% Light and Medium Industrial
- 19% Agriculture
- 16% Commercial



**Central District Top 3 Uses:**

- 33% Agriculture
- 24% Environmental/Stormwater
- 23% Rural Residential

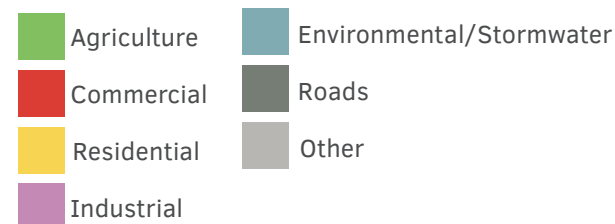


**East District Top 3 Uses:**

- 39% Agriculture
- 25% Environmental/Stormwater
- 15% Rural Residential

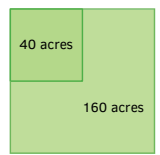
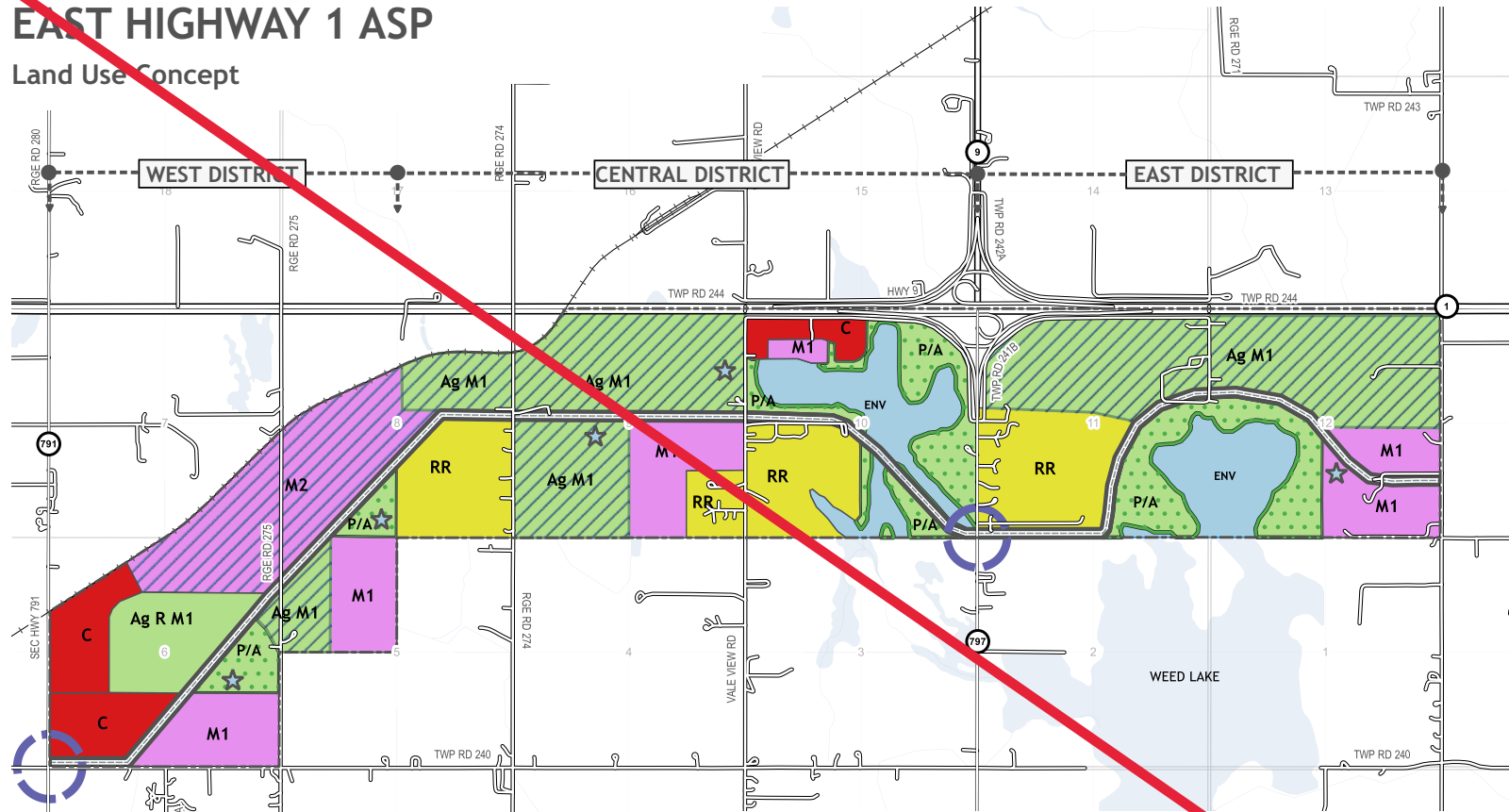
**Table 2: Land Use Distribution**

USE	HECTARES	ACRES	%
Ag1: Agriculture/Ag-Industrial	414.4	1,024.1	26.6%
Ag R: Agriculture, Rec & Agridome	55.1	136.2	3.5%
C: Commercial	85.2	210.6	5.5%
M1 & M2: Industrial (Light & Med)	330.5	816.7	21.2%
30m buffer: Rural Residential	9.5	23.6	0.6%
RR: Rural Residential	248.2	613.3	15.9%
Future Roads (i.e. mobility spine)	69.0	170.6	4.4%
PUL: Public Utility (Stormwater)	137.2	339.0	8.8%
P/A: Parks & Amenities (wetlands)	162.9	402.4	10.4%
Existing Roads	48.0	118.6	3.1%
<b>TOTAL:</b>	<b>1,560.0 ha</b>	<b>3855.1 ac</b>	<b>100.0%</b>

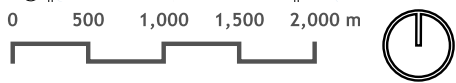


MAP 6 | LAND USE CONCEPT

EAST HIGHWAY 1 ASP  
Land Use Concept



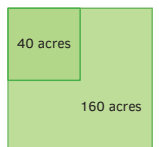
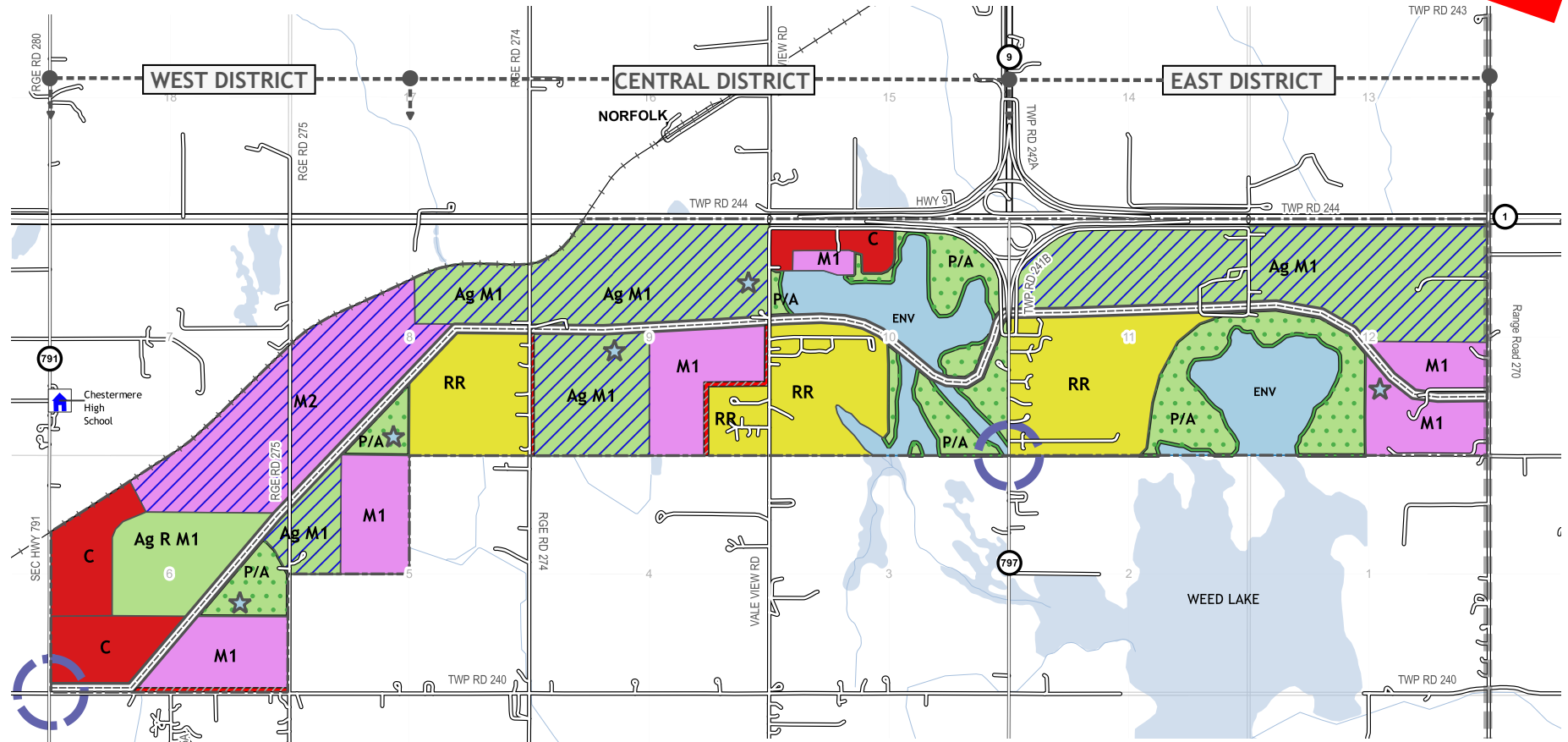
Quarter Section Reference



- Area Structure Plan
- Railway
- Existing Roads
- Quarter Section
- Existing Waterbody
- Transit Hub (Park & Ride)
- Ag M1 Agriculture/Agriculture Industrial
- Ag R Ag / Recreation / Agridome
- C Commercial
- RR Rural Residential
- M1 Industrial (Light)
- M2 Industrial (Medium)
- P/A Park/Amenity
- ENV Environmental/Stormwater
- Public Utility To Be Determined
- Mobility Spine
- Transition / Longterm Overlay
- Ag-business/intensive horticulture overlay

MAP 6 | LAND USE CONCEPT

Map changes



Quarter Section Reference

- |                           |  |  |
|---------------------------|--|--|
| Area Structure Plan       | Ag M1 Agriculture/Agriculture Industrial | P/A Park/Amenity                           |
| Railway                   | Ag R Ag / Recreation / Agridome          | ENV Environmental/Stormwater               |
| Existing Roads            | C Commercial                             | Public Utility: To Be Determined           |
| Quarter Section           | RR Rural Residential                     | Mobility Spine                             |
| Existing Waterbody        | M1 Industrial (Light)                    | <b>Transition / Longterm Overlays:</b>     |
| School                    | M2 Industrial (Medium)                   | Ag-business/intensive horticulture overlay |
| Transit Hub (Park & Ride) | 30 metre Buffer                          |  |

New

# 7.0

## ~~AGRICULTURAL~~ INDUSTRIAL

### OVERVIEW

**Map 6: Land Use Concept** identifies the light-medium industrial uses within the ASP.

The industrial policies support the development of an Employment Area that provides local and regional employment opportunities, increases the County's non-residential assessment base, and contributes to the long-term financial sustainability of the County.

The intent of this ASP is to create a range of industrial development types connected by suitable road network, integrated with pathways and ponds, and easily accessible from major highways. Attractive building proportions and materials will provide an inviting and valued place of business in the region.

Existing landscape features such as shelterbelts (vegetation screens and wind breaks) as well as unique rolling landforms will be retained where possible or used as design inspiration to draw on the legacy of the agrarian landscape.

### OBJECTIVES

- Support the development of well-designed industrial areas in compliance with **Section 24: Agricultural Industrial Design Guidelines**.
- Provide for the growth of local and regional employment opportunities.
- Promote financial sustainability by increasing the County's business assessment base.



*Industrial design precedent - Campbell Heights Industrial, Surrey BC.*

## 7.1 POLICIES

- 7.1.1 The following policies apply to those areas identified on **Map 6: Land Use Concept** as “Industrial”.
- 7.1.2 Development of industrial uses should proceed in an orderly and logical manner supported by full servicing and infrastructure.
- 7.1.3 Industrial uses such as distribution logistics, warehousing, transportation, industrial services, construction, manufacturing, services (business, petroleum, professional, scientific, and technical), and industrial storage are appropriate within all industrial areas identified on **Map 6: Land Use Concept**.
- 7.1.4 Limited small scale commercial uses to serve the development may be permitted at the discretion of the County within identified industrial areas.
- 7.1.5 All private lighting, including security and parking area lighting, shall be designed according to the County's ‘dark sky’ Land Use Bylaw requirements, conserve energy, reduce glare, and minimize light trespass onto surrounding properties.

## 7.2 DESIGN REQUIREMENTS

- 7.2.1 All **local plans** and development applications should demonstrate how the plan or application meets the design guidelines in **Section 24: Agricultural Industrial Design Guidelines** of this Plan and the County's *Commercial, Office, and Industrial Design Guidelines*.
- 7.2.2 A minimum 10.0 m buffer zone should be provided to adjoining residential areas. No parking or storage are permitted in this area.



*Industrial design precedent - Campbell Heights Industrial, Surrey BC.*

## 8.0

## COMMERCIAL

## OVERVIEW

Commercial areas in the East Highway 1 plan area will support traffic via access from the new Sunbelt Boulevard, future industrial development, and existing or future agricultural business and rural residential. Two commercial nodes will anchor the West District, and Central District. Both nodes are located on the periphery of the plan area and are accessible directly by major transportation corridors. The West District commercial node is accessible by Highway 791 and the Central District commercial node is accessible by Highway 1. Both nodes connect directly to industrial uses to support their development. The design intent is to maintain the rural aesthetic and to provide commercial opportunities that support a symbiotic relationship with surrounding agriculture-based uses.

## OBJECTIVES

- Support the development of high quality and thriving commercial nodes that provide a land use transition from industrial land uses to rural agricultural land uses.
- Provide for the growth of local and regional employment opportunities.
- Design commercial areas that can adapt to rapidly changing economic conditions and markets through flexible lot size and design regulations.

## 8.1 POLICIES

- 8.1.1 The following policies apply to those areas identified on **Map 6: Land Use Concept** as “Commercial”.
- 8.1.2 Major commercial developments shall be located in the areas identified on **Map 6: Land Use Concept** as “Commercial”. Minor retail or commercial enterprises may be permitted within the industrial areas at the discretion of the approving authority on condition the use does not detract from the viability of the identified commercial areas.
- 8.1.3 Business or office land uses carried on within an enclosed building may be permitted within the identified commercial area if it does not detract from the provision of commercial retail services.
- 8.1.4 Outdoor storage as a primary use shall not be permitted. Outside storage incidental to the primary use of the site shall be screened and located to the side or rear of the primary building.
- 8.1.5 Outside display areas are permitted provided they are limited to examples of equipment, products, or items related to the site’s use.
- 8.1.6 Commercial areas shall be situated in a location that ensures safe and efficient access and egress from adjacent roadways.
- 8.1.7 Commercial uses located adjacent to agriculture areas shall address the Agriculture Interface and Non-residential / Residential area policies of this Plan (Section 11 and **Map 7: Interfaces and Gateways**).

- 8.1.8 All private lighting, including security and parking area lighting, shall be designed to respect the County's 'dark sky' Land Use Bylaw requirements, conserve energy, reduce glare, and minimize light trespass onto surrounding properties.
- 8.1.9 All local plans and development applications should demonstrate how the plan or application meets the design guidelines in **Section 24 Agricultural Industrial Design Guidelines** of this Plan and the County's Commercial, Office, and Industrial Design Guidelines.

## 8.2 DESIGN GUIDELINES

- 8.2.1 Parking should be generally located in parking areas or 'courts' that are landscaped in front or to the side of the buildings.
- 8.2.2 Developments shall create pedestrian connections in parking lots to make it safer for pedestrians.
- 8.2.3 Developments should provide flexibility in design and universal accessibility of units so that occupancy can change over time.
- 8.2.4 A minimum 8.0 m buffer zone should be provided to adjoining residential areas. No parking or storage are permitted in this area.



*Local-Scaled Rural Commercial Development to support the Industrial uses*



# 9.0

## RURAL RESIDENTIAL

### OVERVIEW

The East Highway 1 Plan area encompasses existing Rural Residential uses. The existing homes are protected and will remain, however the ASP hopes to provide opportunities for diversification of use by permitting agriculture related business ventures.

### OBJECTIVES

- Support the retention of existing rural residential areas while providing opportunities to transition towards uses that connect with the East Highway 1 Agri-business **hub corridor**.
- Ensure the impact of commercial and industrial development on rural residential development is minimized through the implementation of appropriate interface policies and design guidelines.

### 9.1 POLICIES

- 9.1.1 Industrial development adjacent to the existing residential areas shall be subject to the interface policies and design guidelines of this plan (Section 11).
- 9.1.2 The rural residential uses on are encouraged to continue until such time as a transition to industrial or Agri-business use is deemed desirable and a local plan has been prepared, in accordance with the policies of this plan.
- 9.1.3 Subdivision of a residential area into higher density residential shall not be supported.



# 10.0

## DEVELOPMENT IN PROXIMITY TO RAILWAYS

### OVERVIEW

Development in proximity to railways must take the necessary precautions and design considerations to maximize safety in this intermodal space.

### OBJECTIVES

- Ensure optimal safety for operations and development in proximity to railways.
- Collaborate with developers and railway operators to ensure appropriate design of the site

### 10.1 POLICIES

- 10.1.1 Land uses which may be adversely affected by the safety and nuisance impacts of passing trains should not locate immediately adjacent to the railway.
- 10.1.2 Developers should consult with railway operators prior to development within proximity of the railway line.
- 10.1.3 Appropriate safety measures and methods to provide noise and vibration attenuation for development adjacent to the railway should include such elements as; setbacks, berming, and landscaped screening.

10.1.4 Where a development site is located adjacent to the railway, it should be in accordance with the Development in Proximity to Railway Guidelines developed by Federation of Canadian Municipalities and Railway Association of Canada found at [www.proximityissues.ca](http://www.proximityissues.ca).

10.1.5 Where roads or pedestrian networks cross the railway, the County shall collaborate with CN to ensure that crossings are constructed according to appropriate safety standards and any necessary upgrades are undertaken to ensure a safe crossing.



Photo courtesy of Google Earth

# 11.0

## INTERFACE AREAS & TRANSITIONS

### 11.1 AGRICULTURE INTERFACE

#### OVERVIEW

Agriculture is a significant land use within the East Highway 1 Plan area and will continue until envisioned development occurs. It is important that agricultural uses are allowed to continue unimpeded while allowing opportunities for intensification or diversification of use.

The County's Agricultural Boundary Design Guidelines provide recommendations for a variety of buffering, siting, and design techniques to minimize impacts of non-agricultural development on agricultural operations and to reduce potential land use conflicts.

**Map 7: Interfaces and Gateways** outlines the uses bordering the ASP area as well as potential entry points. The bordering uses are mostly agricultural with pockets of rural residential along the southeast edges.

#### OBJECTIVES

- Ensure an appropriate interface between non-agricultural uses and agricultural land and operations, in order to avoid negative impacts on agriculture operations.
- To provide opportunities for innovative agriculture-based business and intensification to occur.

#### POLICIES

- 11.1.1 All lands identified as Interfaces on **Map 7: Interfaces and Gateways**, shall be subject to the Interface Areas & Transitions policies of this section of the Plan.
- 11.1.2 All developments shall comply with the Agricultural Boundary Design Guidelines where possible.
- 11.1.3 Proposals for non-agricultural development adjacent to agricultural lands located either within or outside of the Plan boundary shall incorporate buffering, siting, and design techniques to minimize negative impacts on agricultural lands.
- 11.1.4 Agricultural buffering techniques may include a combination of the following:
- a. barrier fencing to prevent access;
  - b. vegetated berms;
  - c. community agriculture plots;
  - d. stormwater management facilities;
  - e. ecological / vegetative buffers;
  - f. use of topographic barriers such as slopes, roads, watercourses or wetlands; and
  - g. increased setbacks for housing and other buildings.
- 11.1.5 Public access such as trails, pathways, and parks should be discouraged adjacent to agricultural lands unless supported by the open space and pathway plan (**Map 10: Transportation & Mobility Concept**).

## 11.2 INTERMUNICIPAL INTERFACE

### OVERVIEW

The East Highway 1 Plan area's east boundary is Wheatland County (See **Map 1: Context**) and appropriate interface design is vital to ensuring a good neighbour relationship with the landowners and citizens within the County.

### OBJECTIVES

- Ensure high-quality development along the Rocky View County – Wheatland County border to minimize any negative impacts on adjacent land uses from the East Highway 1 development.
- Support good neighbourliness between the two Counties through quality design and planning in the East Highway 1 Plan area.
- Maintain open communication and cooperation between Rocky View County, and Wheatland County.

### POLICIES

- 11.2.1 Any local plan that includes the East Highway 1 east boundary with Wheatland County shall demonstrate how the interface will be appropriately planned and designed to minimize any land use conflict. The local plan should provide illustrations (e.g. cross-section) and graphics to show proposed the interface design.
- 11.2.2 Acceptable uses within the interface area shall include uses for which activities are primarily carried on within an enclosed building and which generate no significant nuisance factors outside of the enclosed building.
- 11.2.3 Spatial separation between industrial and non-industrial uses shall be achieved by providing appropriate setbacks for industrial developments.

- 11.2.4 High-quality landscaping should be emphasized in all interface areas. A landscape plan shall be prepared for any development in the Interface area as part of a local plan, and shall address the design guidelines in **Section 24** of this Plan; the County's Land Use Bylaw; and the County's Commercial, Office and Industrial Design Guidelines.



**NEW**

## 11.3 RURAL RESIDENTIAL INTERFACE

### OVERVIEW

The development of the East Highway 1 area requires careful and sensitive integration of future uses that are adjacent to existing rural residential areas. The goals and policies of this section are intended to incorporate transitional buffers and mitigate the impact of non-residential uses.

### OBJECTIVES

- To minimize the impact of non-residential\* development on residential development.
- To provide edge conditions in non-residential\*/residential buffer areas that are complementary to adjacent residential areas.

### POLICIES - GENERAL

- 11.3.1 Local Plans for industrial and agricultural industrial uses adjacent to areas identified as Rural Residential on Map 6 - Land Use Concept shall include a buffer strategy that addresses the policies of this section.
- 11.3.2 To minimize the impact of the Mobility Spine on residential properties, the Rural Residential areas shown on Map 6 - Land Use Concept should be buffered from the Mobility Spine using landscaping and/or sound and visual attenuation feature. A minimum buffer of 10-metres is required as described in these policies.

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\* The term non-residential refers to commercial, industrial, or other types of business development.

### POLICIES - INDUSTRIAL, AGRICULTURAL INDUSTRIAL USES, AND COMMERCIAL USES

- 11.3.3 All non-residential developments located adjacent to any Rural Residential area (identified on Map 6 - Land Use Concept as "Non-residential / Residential Buffer Areas") shall comply with the following:
- a) Acceptable land uses include business activities primarily carried on within an enclosed building that generate no significant nuisance factor (noise, dust, smells, vibration) outside of the enclosed building. Business uses that interfere with the use and enjoyment of adjacent residential development due to nuisances (noise, dust, smell, vibration) should not be permitted, even where the business activities may be fully enclosed within a building.
  - b) Notwithstanding the above, policy 11.3.3 (a) above does not apply to lands designated as M2 (Medium Industrial) adjoining the railway tracks in the northwest corner of the Plan Area.
  - c) Outside storage is not an acceptable use.

### POLICIES - SETBACK AREA

- 11.3.4 Spatial separation between non-residential and residential uses identified on Map 6 - Land Use Concept as "Non-residential / Residential Buffer Area" should be achieved by providing setbacks for the non-residential buildings.
- a) Where non-residential buildings are on lands directly adjacent to a residential area (even with a road separating the two properties), the non-residential building shall be set back a minimum of 30 metres from the non-residential property line.
  - b) Where a trail or pathway is located within, or adjacent to, a non-residential/residential interface area, the pathway and associated open space, including municipal reserve, may be counted as part of the 30-metre building setback.

- c) A parking area or portion thereof may be located up to 10-metre of the 30-metre landscaped buffer. No storage or other uses besides landscaping can be used in the 30-metre landscaped buffer, excepting a trail or pathway.

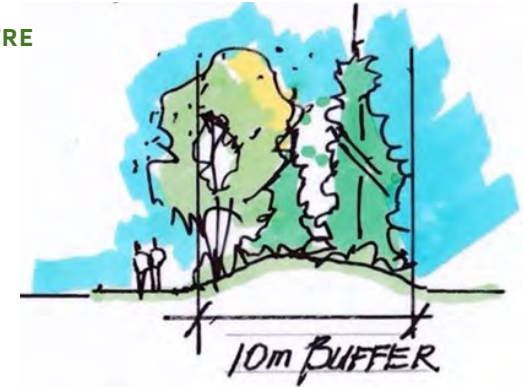
11.3.5 Uses within the 30-metre minimum building setback of Non-residential/residential Buffer Area may include:

- a) Landscaping, berms, landscaped stormwater ponds, natural wetlands, trails, and linear parks; and
- b) Partial surface parking (up to 10 metres) where the parking is hidden from view by berms and landscaping.

11.3.6 High-quality landscaping should be emphasized in the setback (buffer) area. A landscape plan shall be prepared for the setback as part of a *local plan* that addresses the County's Land Use Bylaw and any applicable design guidelines.

\* The term non-residential refers to commercial, industrial, or other types of business development. The **Non-Residential/Residential Buffer Areas** (shown on **Map 6 – Land Use Concept**) are meant to provide a compatible interface between business and rural residential development. The non-residential/residential buffer areas contain the land designated for industrial, commercial, or other business use adjacent to a residential area. A compatible buffer is achieved by providing for the appropriate land use, building setbacks, lot and building design, and landscaping within this area.

**FIGURE 3 | 10 METRE NON-RESIDENTIAL/RESIDENTIAL BUFFER AREA**

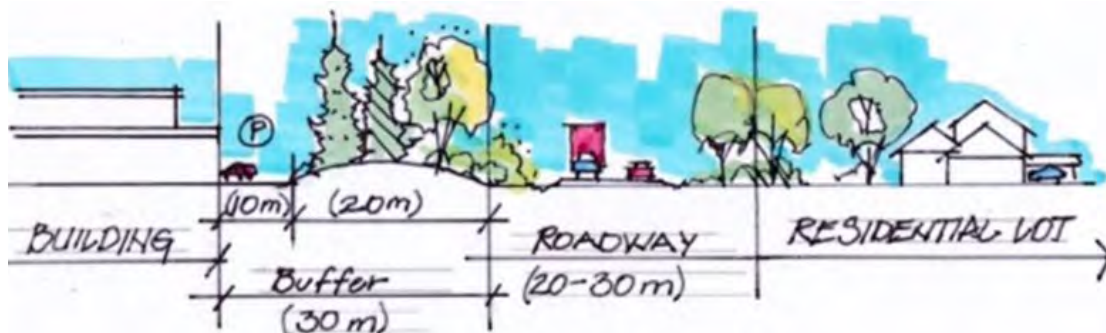


\*A 10-metre landscaped buffer is required on all other property interfaces to ensure the proper transitions between commercial, industrial, and agricultural industrial uses. The landscape requirement policies in these transition areas will follow the policies as specified above, excepting no parking or trails or pathways are permitted in the 10-metre buffer area.

11.3.7 Within the Non-residential/Residential Buffer Area mass plantings and/or berms shall be required to minimize the visual impact of the commercial/industrial buildings. The plantings and earth berms should incorporate natural contours and variations in height to achieve a natural landscaped appearance.

11.3.8 Security fencing (dark green or black chain link) may be constructed in this area to a maximum of 2.5 metres in height subject to significant coniferous tree planting on the rural residential side of the fence, so it is significantly screened from view.

- a) The security fencing may be in either the Non-Residential/Residential Buffer Area or the municipal reserve, if provided.



**FIGURE 2 | 30 METRE NON-RESIDENTIAL/RESIDENTIAL BUFFER AREA**

# 12.0

## GATEWAYS

### OVERVIEW

Gateways are important entrances, along major roads, entering and exiting a municipality and a community. They represent a 'community's welcome' and it is important that they are visually attractive and well maintained. Highway 1 provides connection to the ~~Town~~ City of Chestermere and the City of Calgary in the west, and to Wheatland County in the east.

Primary Gateways provide the entry points to the development area. These gateways should include the intersections of Highway 1 and Highway 791, Highway 797, and Range Road 264, as well as the intersection of Highway 791 and Township Road 240.

### OBJECTIVES

- Create attractive, orderly, and well-maintained gateways through high quality development and landscaping.
- Ensure gateway development is coordinated with adjacent municipalities.

### 12.1 POLICIES

- 12.1.1 All lands identified as Gateways on **Map 7: Interfaces and Gateways**, shall be subject to the gateway policies of this section of the Plan.
- 12.1.2 Consideration shall be given to a high quality visual appearance (in accordance with **Section 24: Agricultural Industrial Design Guidelines**) when determining appropriate land use, siting, building design, and landscaping.
- 12.1.3 Local plan design guidelines for gateways should consider such factors as; sight lines, noise attenuation, setbacks, natural land features, innovative building design, and high quality landscaping and signage.
- 12.1.4 Gateways should be developed in accordance with the County's Commercial, Office, and Industrial Design Guidelines.
- 12.1.5 Rocky View County will collaborate with Alberta Transportation in creating an attractive gateway along Highway 1.

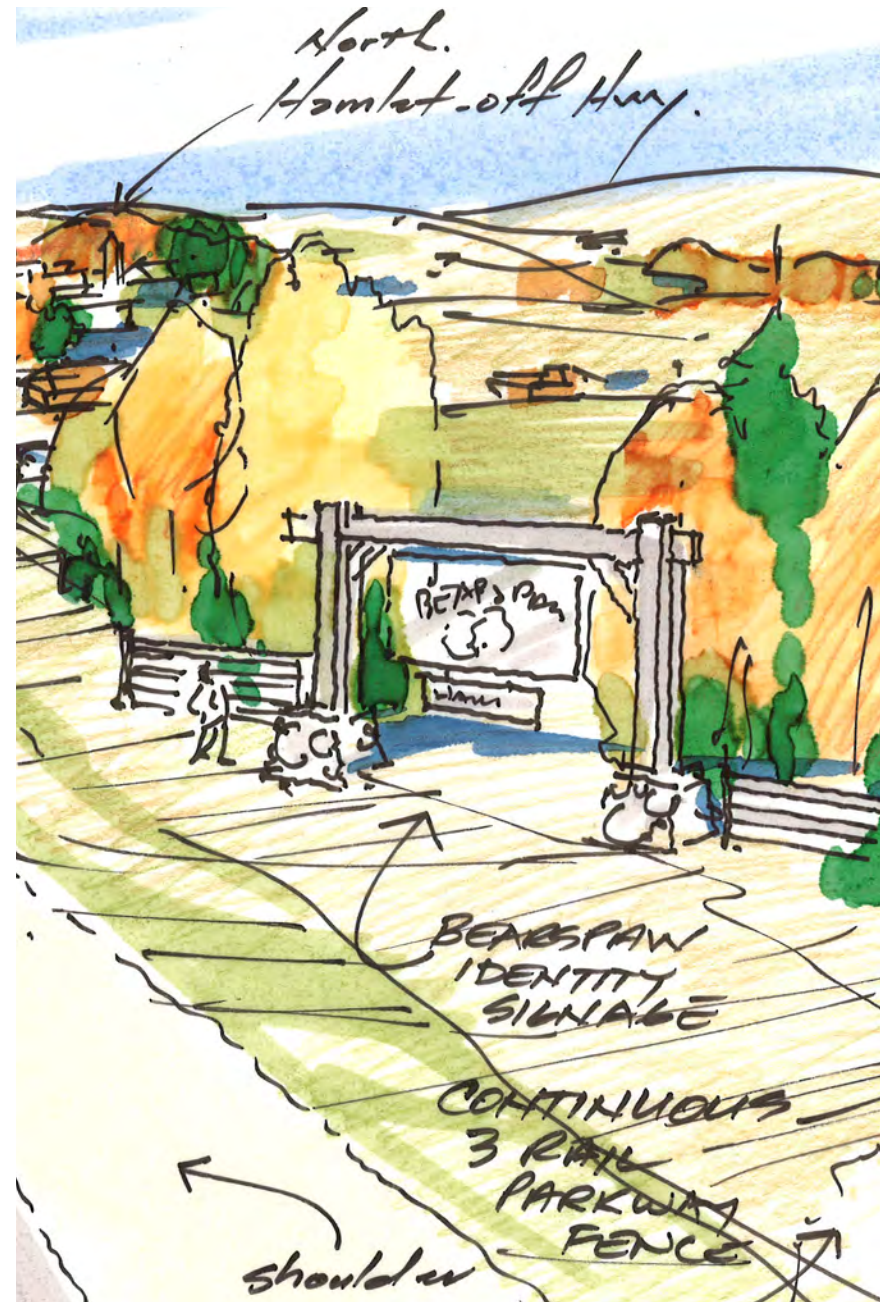
## 12.2 DESIGN GUIDELINES

- 12.2.1 Monument signs, a maximum 1.5 meters high, should be used in accordance with the Land Use Bylaw and in combination with earth berms and planting to appropriately announce the primary accesses.
- 12.2.2 Direct lighting could be considered to complement the entrance without creating glare or other unnecessary impacts on adjoining uses.

### SECONDARY GATEWAYS

Secondary gateways are those entrances distinct to the character areas. Monument signage should be provided in accordance with local regulations, secondary to the primary gateway signs but still visible to drivers.

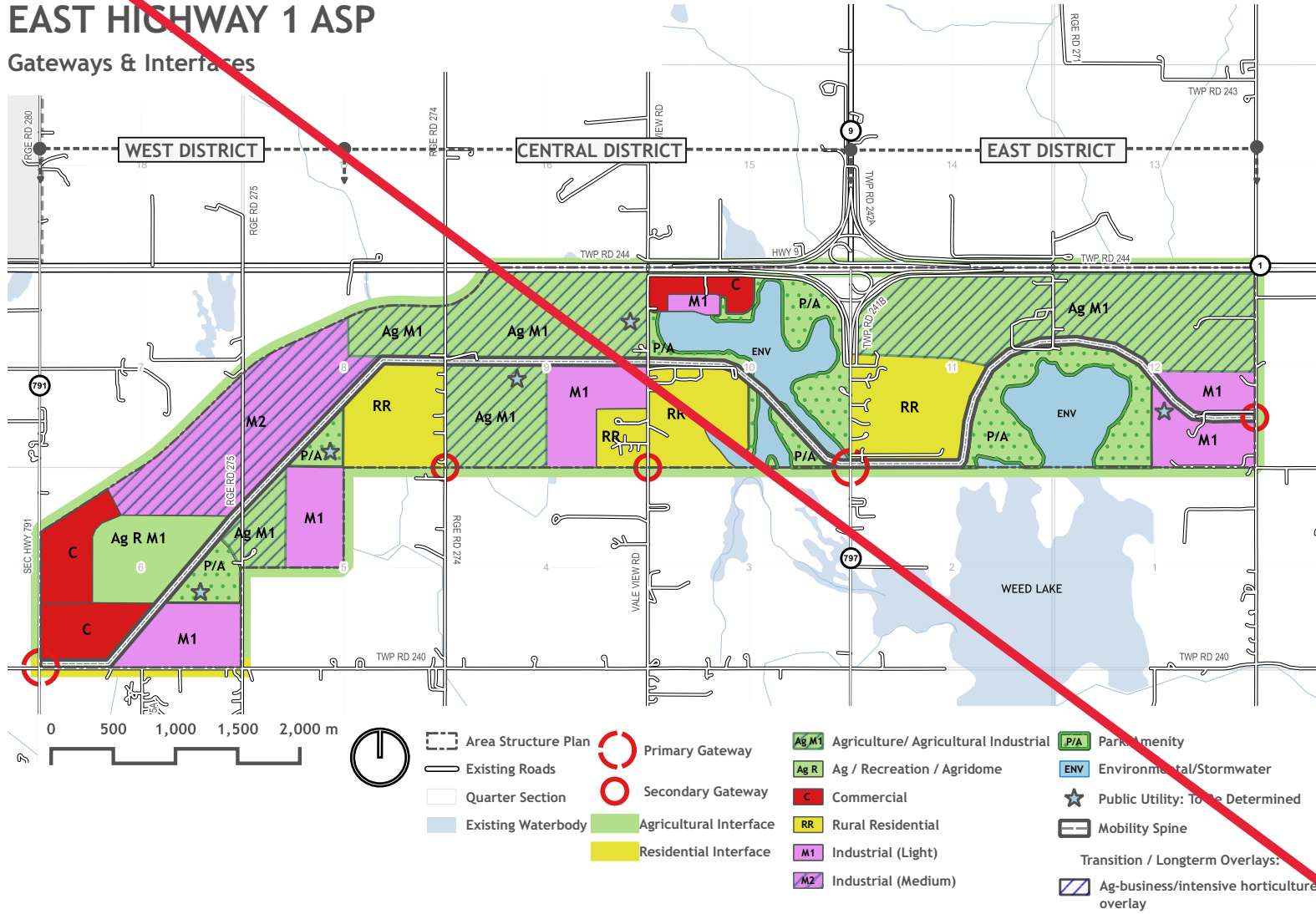
- 12.2.3 Any signage should be integrated with appropriate trees and accent landscaping that flows with adjoining landscaping.
- 12.2.4 Unobtrusive lighting should be provided that features the sign and creates no external glare.





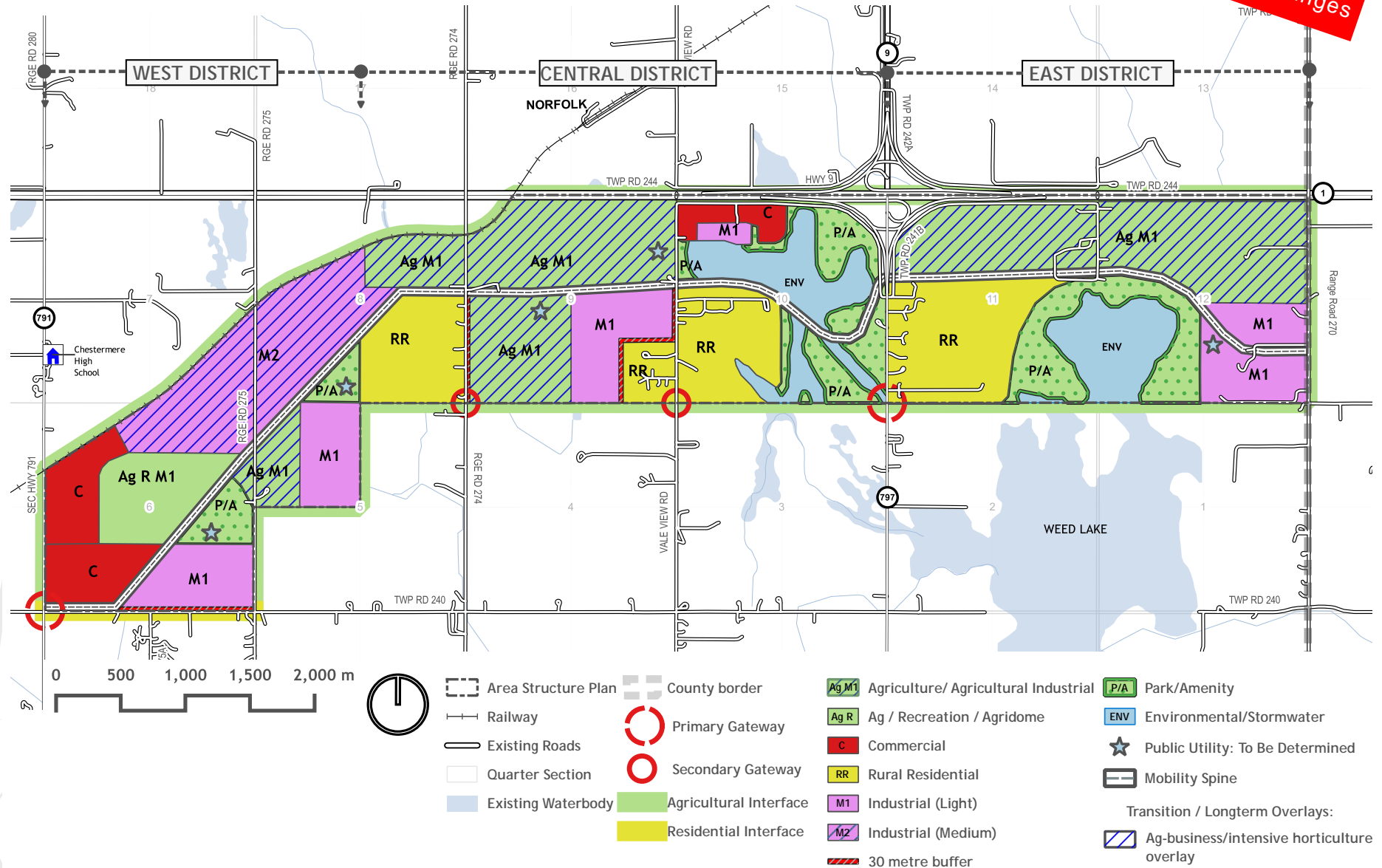
MAP 7 | INTERFACES AND GATEWAYS

**EAST HIGHWAY 1 ASP**  
 Gateways & Interfaces



MAP 7 | INTERFACES AND GATEWAYS

Map changes



# PLAN POLICIES: OPEN SPACE & ENVIRONMENT



# 13.0

## PARKS & ENVIRONMENTALLY SENSITIVE AREAS

Open space, parks, pathways, and trails contribute to community building by preserving rural landscapes and providing residents with opportunities for passive and active recreation. Communities need to have a wide range of accessible, connected, inviting, and safe parks and open spaces to meet the diverse needs of residents, businesses, schools, and other institutions. Pathways that connect neighbouring municipalities are also important to provide regional connections to adjoining areas and amenities.

The overall intent is to enhance the environmental assets on the site and maintain the rural quality of landscape. These policies are applicable to wetlands and any other environmentally sensitive areas on the site. At the same time, existing significant tree stands and related habitat in other parts of the site should be retained on site if possible and developed as part of a natural areas and trail network.

### OBJECTIVES

- Promote, conserve, and enhance an interconnected open space system.
- Ensure that open space and parks have an ecological, social, cultural, recreational, and / or aesthetic function and that each space operates in a sustainable manner.
- Provide for an interconnected regional and local network of pathway and trail connections.
- Provide opportunities for passive recreation and alternative transportation modes within industrial and business areas.

- Provide for the enhancement of wetlands and wetland values.
- Ensure wetlands are assessed through the local plan preparation process.

### TECHNICAL REVIEW

A Biophysical Inventory (BI) scan was done by Trace Associates Inc for Terradigm Development Consultants Inc in January 2021. This BI reviewed the biophysical features of the ASP Study Area and identified the key environmental and natural features, including two Environmentally Sensitive Areas (ESAs) including Weed Lake, as well as a number of potential ESAs (wetlands over 1 ha and modified grassland). A large portion of the ASP Study Area has been cultivated, either as cropland or as hayland, and these areas are not expected to contain environmentally significant features. No steep slopes, escarpments or other biophysical features were identified as constraints. The full Biophysical Inventory Report makes the following recommendations to help maintain the form and function of key environmental and natural features:

- Conduct field inventories to establish baseline conditions of the key environmental and natural features.
- Establish appropriate setbacks from confirmed Environmentally Sensitive Areas.
- Apply best management practices when constructing near these areas including Erosion and Sediment Control Measures, wildlife sweeps and temporary fencing, to avoid or minimize impacts.

## POLICIES

### 13.1 OPEN SPACE AND TRAILS

- 13.1.1 An interconnected system of open space shall be provided in the Plan area that is in general accordance with **Map 8: Parks and Trails Concept**.
- 13.1.2 Open space shall be provided through such means as:
  - a. The dedication of reserve lands, environmental reserves, and public utility lots;
  - b. The provision of environmental reserve easements, conservation easements, or other easements and rights-of-way;
  - c. Government lands for public use;
  - d. Privately owned land that is accessible to the public;
  - e. Publicly owned stormwater conveyance systems;
  - f. Land purchases, endowment funds, land swaps, and donations; and/or
  - g. Other mechanisms as may be approved by the County.
- 13.1.3 Open space shall be planned and integrated into the Plan area so that the function of each space will provide a positive and safe social, cultural, and/or recreational experience for the community.
- 13.1.4 Open space shall have an ecological, social, cultural, recreational, and/or aesthetic function that is sustainable.

### *Parks, Pathways, Trails, and Sidewalks*

- 13.1.5 The network of pathways, trails, and sidewalks should promote active transportation (e.g. walking and cycling); the network should provide active transportation connections between all areas within the Plan.
- 13.1.6 The design and construction of parks, pathways, trails, and associated amenities shall be of high quality and adhere to the County's Servicing Standards and the County's Parks and Open Space Master Plan design criteria.
- 13.1.7 Local plan preparation shall align with the County's Active Transportation Plan.
- 13.1.8 Local plan preparation should provide for a pathway, trail, and sidewalk network that generally aligns with the network shown on **Map 8: Parks and Trails**, and:
  - a. Provide active transportation connections within, and external to, the local plan area;
  - b. wherever possible, be located within, or align with, a park, wetland, stormwater conveyance system, natural water course, riparian area, or natural area;
  - c. incorporate Crime Prevention Through Environmental Design (CPTED) features; and
  - d. contribute to the regional trail and pathway system and, where required, connect with other municipalities' active transportation and pedestrian networks.
- 13.1.9 Where the regional pathway, trail, and sidewalk network cannot be located within a park, wetland, stormwater conveyance system, natural water course, riparian area, or natural area, it may be located within a road right-of-way in accordance with applicable County standards or in municipal reserve land adjacent to a road.

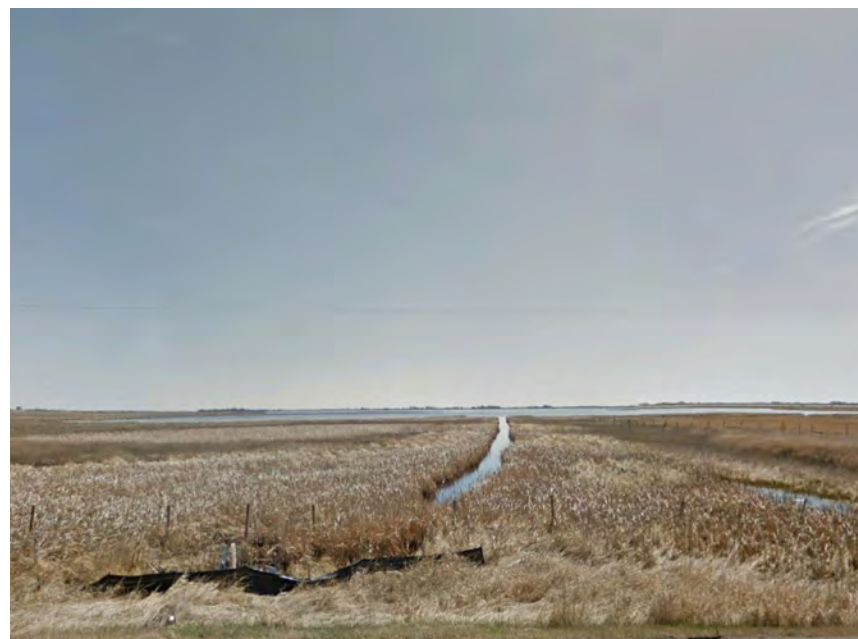
## 13.2 WETLANDS

- 13.2.1 Wetland protection shall be guided by County and Provincial Policy.
- 13.2.2 The County shall require the use of the Provincial system to determine wetland classification and relative wetland value.
- 13.2.3 Local plans shall identify the classification and value of wetlands within the local plan area boundary. This shall be done as part of a wetland assessment, to be provided at the local plan preparation stage.
- 13.2.4 Local plans shall determine, through consultation with the Province, whether wetlands are Crown owned land.
- 13.2.5 Wetlands, not claimed by the Crown, that have a high relative value should be dedicated as environmental reserve, environmental reserve easement, or enhanced as storm water management areas.
- 13.2.6 Where wetlands are not retained, developers shall provide for appropriate replacement or compensation, in accordance with Provincial policy.

## 13.3 RIPARIAN AREAS

- 13.3.1 Riparian area protection shall be guided by County and Provincial Policy.
- 13.3.2 The riparian setback area from a protected watercourse should be determined using the Province's Stepping Back from the Waters: A Beneficial Management Practices Guide For New Development Near Water Bodies in Alberta's Settled Region, or a similar provincial document which may replace this document.

- 13.3.3 The riparian setback area shall be protected as environmental reserve, environmental reserve easement, municipal reserve, or by other means satisfactory to the County.
- 13.3.4 Building and development in the riparian setback area shall be in accordance with the County's Land Use Bylaw and the County's Riparian Setback Policy.
- 13.3.5 The riparian setback area uses may include parks, pathways, and trails.
- 13.3.6 Public roads and private access roads are allowed in the riparian setback area but should be located, designed, and constructed so as to minimize disturbance to the riparian area.

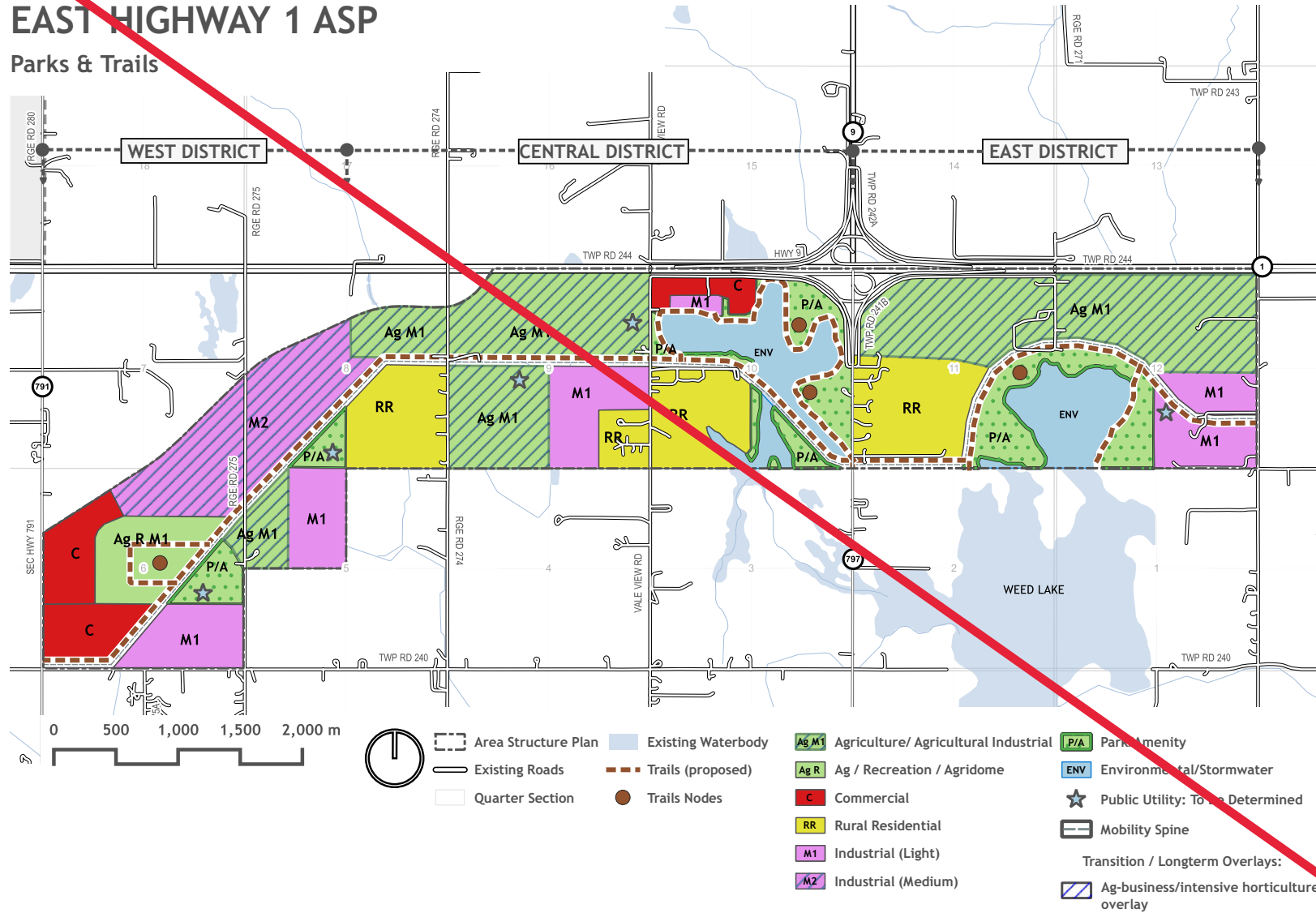


*Image courtesy of Google Earth - Facing east from Highway 797 towards Weed Lake*

MAP 8 | PARKS AND TRAILS CONCEPT

# EAST HIGHWAY 1 ASP

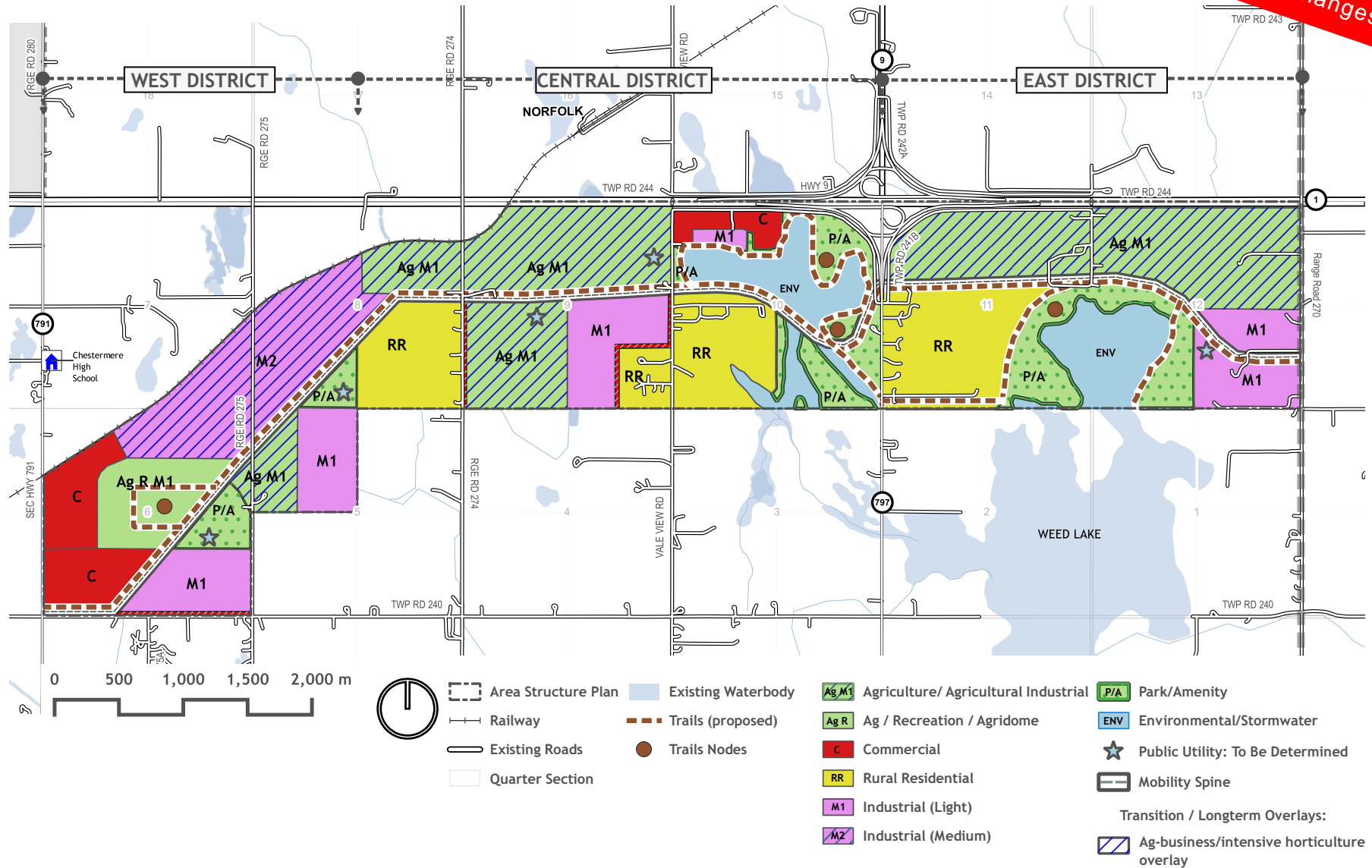
## Parks & Trails



- Area Structure Plan
- Existing Waterbody
- Ag M1 Agriculture/ Agricultural Industrial
- Ag R Ag / Recreation / Agridome
- C Commercial
- RR Rural Residential
- M1 Industrial (Light)
- M2 Industrial (Medium)
- P/A Park Amenity
- ENV Environmental/Stormwater
- Public Utility: To Be Determined
- Mobility Spine
- Transition / Longterm Overlays: Ag-business/intensive horticulture overlay
- Trails (proposed)
- Trails Nodes

MAP 8 | PARKS AND TRAILS CONCEPT

Map changes





# 14.0

## RESERVES

### OVERVIEW

Reserves and environmental reserves are lands dedicated to the County as public land during the subdivision process. Reserves enhance the community by providing land for parks, schools, and recreational amenities. Environmental reserves protect the community and natural environment by preventing development in hazardous areas such as ravines and floodways.

### OBJECTIVES

- Provide for the dedication of reserves to meet the educational, recreational, cultural, social, and other community service needs of the community.
- Provide for the taking of money in place of land for municipal reserve, school reserve, or municipal school reserve.
- Provide direction on the timing of reserve dedication.
- Provide for the identification and protection of environmentally significant land or hazard land through the dedication of environmental reserve or environmental reserve easements.

### 14.1 POLICIES

- 14.1.1 Reserves owing on a parcel of land shall be provided as:
- a. municipal reserve, school reserve, or municipal and school reserve;
  - b. money in place of reserve land; or
  - c. a combination of land and money.
- 14.1.2 Municipal reserve, school reserve, or municipal and school reserve shall be provided through the subdivision process to the maximum amount allowed by the Municipal Government Act.
- 14.1.3 Voluntary dedication of reserve land beyond the maximum amount allowed by the Municipal Government Act may be considered if it is demonstrated that the additional reserve will benefit the community and result in no additional acquisition costs to the County.
- 14.1.4 All, or a portion of, reserve land requirements may be deferred by registering a deferred reserve caveat if it is determined that the reserve could be provided through future subdivision.
- 14.1.5 The acquisition, deferral, and disposal of reserve land, and the use of money in place of reserve land, shall adhere to County Policy, agreements with local school boards, and the requirements of the Municipal Government Act.

- 14.1.6 The dedication of reserves should meet the present or future needs of the Plan area by considering the recommendations of this Plan, the Parks and Open Space Master Plan, Recreation and Culture Master Plan, local plan, school boards, and / or recreation boards.
- 14.1.7 The amount, type, location, and shape of reserve land shall be suitable for public use and readily accessible to the public.
- 14.1.8 Where an identified park, trail, and pathway system (**Map 8: Parks and Trails Concept**) or land for recreational or cultural amenities cannot be provided through the dedication of municipal reserves or private easement, consideration should be given to acquiring land through the use of:
  - a. money in place of reserve land;
  - b. money from the sale of surplus reserve land; or
  - c. other sources of identified funding.

## 14.2 POLICIES

- 14.2.1 Lands that qualify as environmental reserve should be dedicated as environmental reserve or environmental reserve easement through the subdivision process, as per the Municipal Government Act.
- 14.2.2 Environmental reserves should be determined in accordance with the MGA by conducting:
  - a. Biophysical Impact Analysis Report;
  - b. Geotechnical Analysis; and / or
  - c. other assessments acceptable to the County.

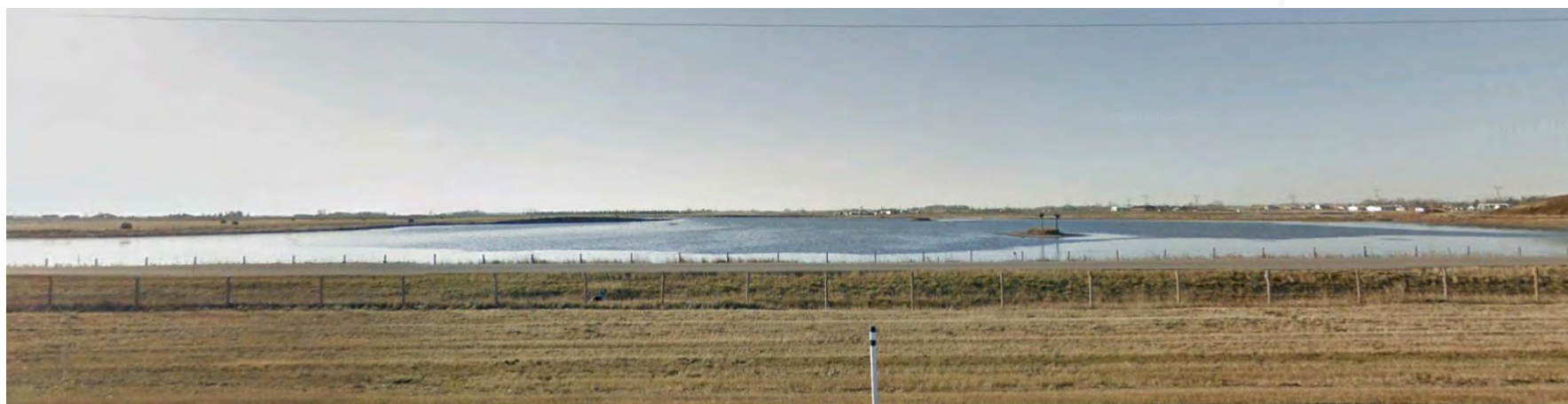


Image courtesy of Google Earth - Facing south from Highway 1 towards wetlands

# 15.0

## AGRICULTURAL INDUSTRIAL

The East Highway 1 ASP encourages the continued use of land for agriculture, while creating opportunity for agricultural diversification and innovation in the future. Adjacent industrial and commercial uses are intended to support agricultural areas as they evolve and intensify. The policies support the retention and development of agriculture uses as described in the Rocky View County Plan, providing direction on developing adjacent to agricultural operations in a manner that minimizes land use conflict.

### OBJECTIVES

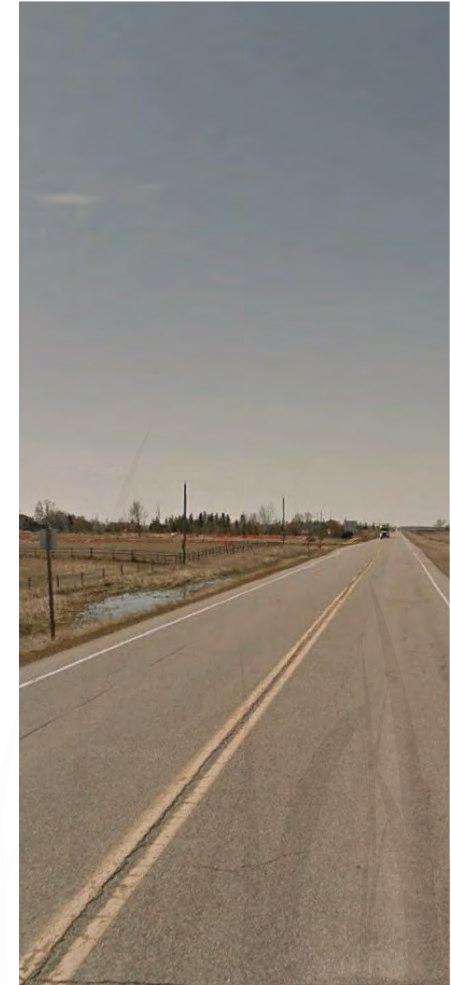
- Support agricultural operations until alternative forms of development are determined to be appropriate.
- Provide for appropriate development of farmsteads and first parcels out.

### 15.1 POLICIES

- 15.1.1 Existing agricultural operations within the Plan boundary are encouraged to continue until development of those lands to another use is deemed desirable and that use is determined to be in accordance with the policies of this Plan.
- 15.1.2 The creation of a single lot from an unsubdivided quarter section for the purposes of a farmstead, First Parcel Out subdivision, or other agriculture development should be supported without the requirement of a local plan when it is in accordance with the relevant policies of this Plan and the County Plan.

- 15.1.3 First parcel out lot subdivisions shall meet the requirements of the County.
- 15.1.4 Residential first parcels out shall be situated in a manner that minimizes the impact on future development of the site. Residential first parcels out:
- shall meet the site requirements of the County Plan;
  - shall meet the County's access management standards; and
  - should be located on the corners of the quarter section.
- 16.1.5 Applications for Confined Feeding Operations are not under the jurisdiction of the County; however they are not desired in the Plan area.

# PLAN POLICIES: MOBILITY, TRANSPORTATION & INFRASTRUCTURE



# 16.0

## TRANSPORTATION

### OVERVIEW

The transportation network must develop in a manner that is safe, functional, and efficient. The network should integrate development within the East Highway 1 area, and provide regional opportunities for active transportation and public transit. **Map 9: Existing Transportation & Mobility Network** shows the provincial, regional, and some local transportation networks in the Plan area, provides information on road classifications, active transportation routes and railway infrastructure. The East Highway 1 ASP proposes the development of a central utility corridor to facilitate vehicular movement within the site.

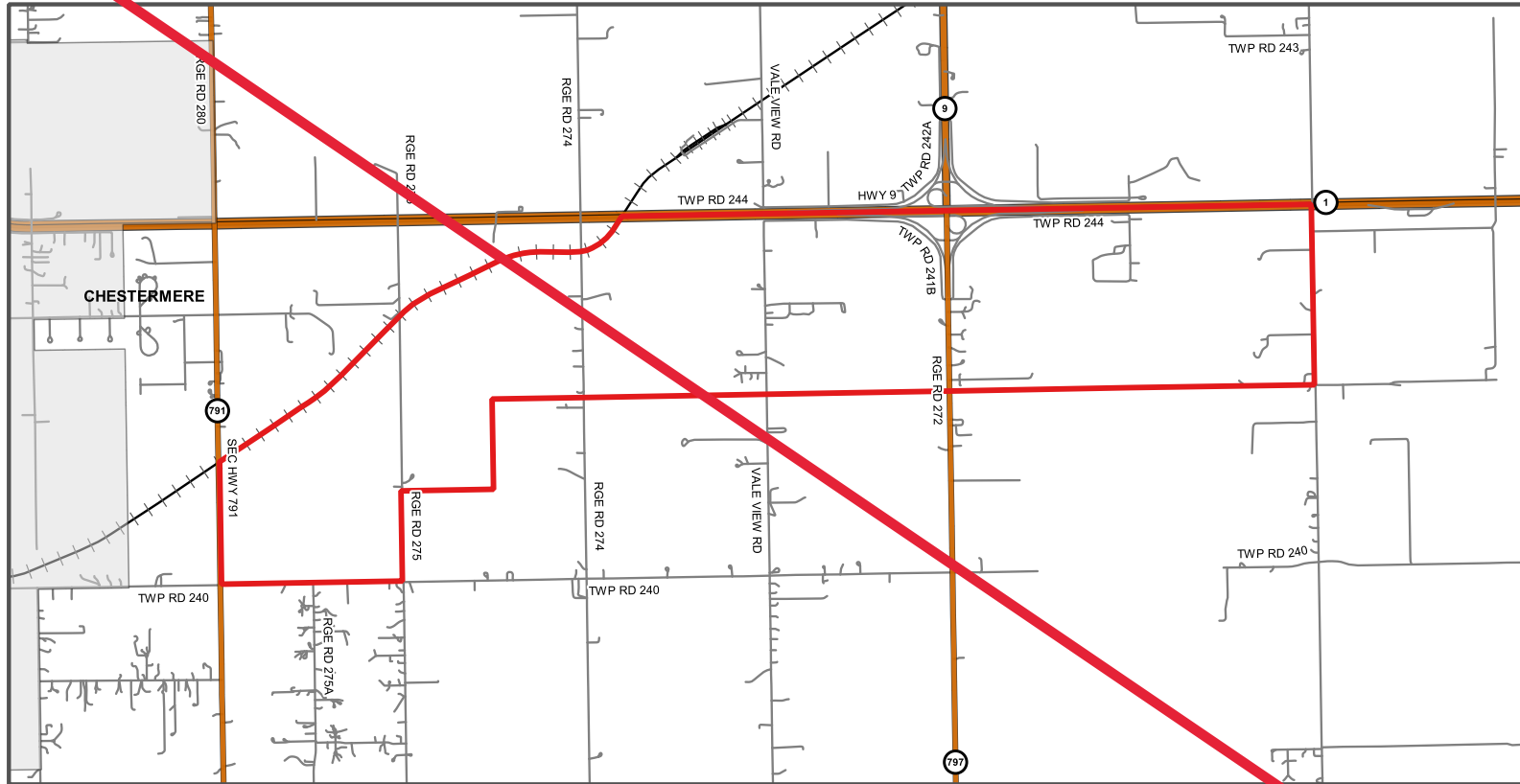
### OBJECTIVES

- Support a regional road network, based on the township and grid system, that:
  - » efficiently accesses and aligns with the provincial and regional highway network; and
  - » encourages the separation of residential, commercial, and industrial traffic.
- Provide for connections to a regional pathway and trail system.
- Provide for an internal road network that contributes to a high quality built environment and efficiently and safely aligns to the regional road network.

### TECHNICAL REVIEW

A Transportation Impact Assessment (TIA) was conducted by JCB Engineering in February 2021. To support the intensified land uses a central utility corridor is proposed to parallel Highway 1 through approximately the middle of the East Highway 1 ASP. The main roadway would have connections at Highway 791, Highway 797 and Range Road 270, and would provide an expressway route across the full length of the East Highway 1 ASP. Access to the individual lots within the East Highway 1 ASP would be via frontage roadways parallel to the expressway, so access would be controlled from the public roadways into the development but also within the development to promote safe and efficient transport of goods and people. It is proposed that roundabouts will be used as the traffic control through the East Highway 1 ASP to minimize stop-and-go traffic, and there could be future connections to lands to the east and west when those lands are developed. The proposed transportation network offers the best scenario for safe and efficient vehicular movement within and around the Plan area.

MAP 9 | EXISTING TRANSPORTATION & MOBILITY NETWORK



East Highway 1 Area Structure Plan

Existing Transportation Network

- Area Structure Plan
- Primary Highway
- Secondary Highway
- Local Road
- Railway
- City

0 1 2 3 km

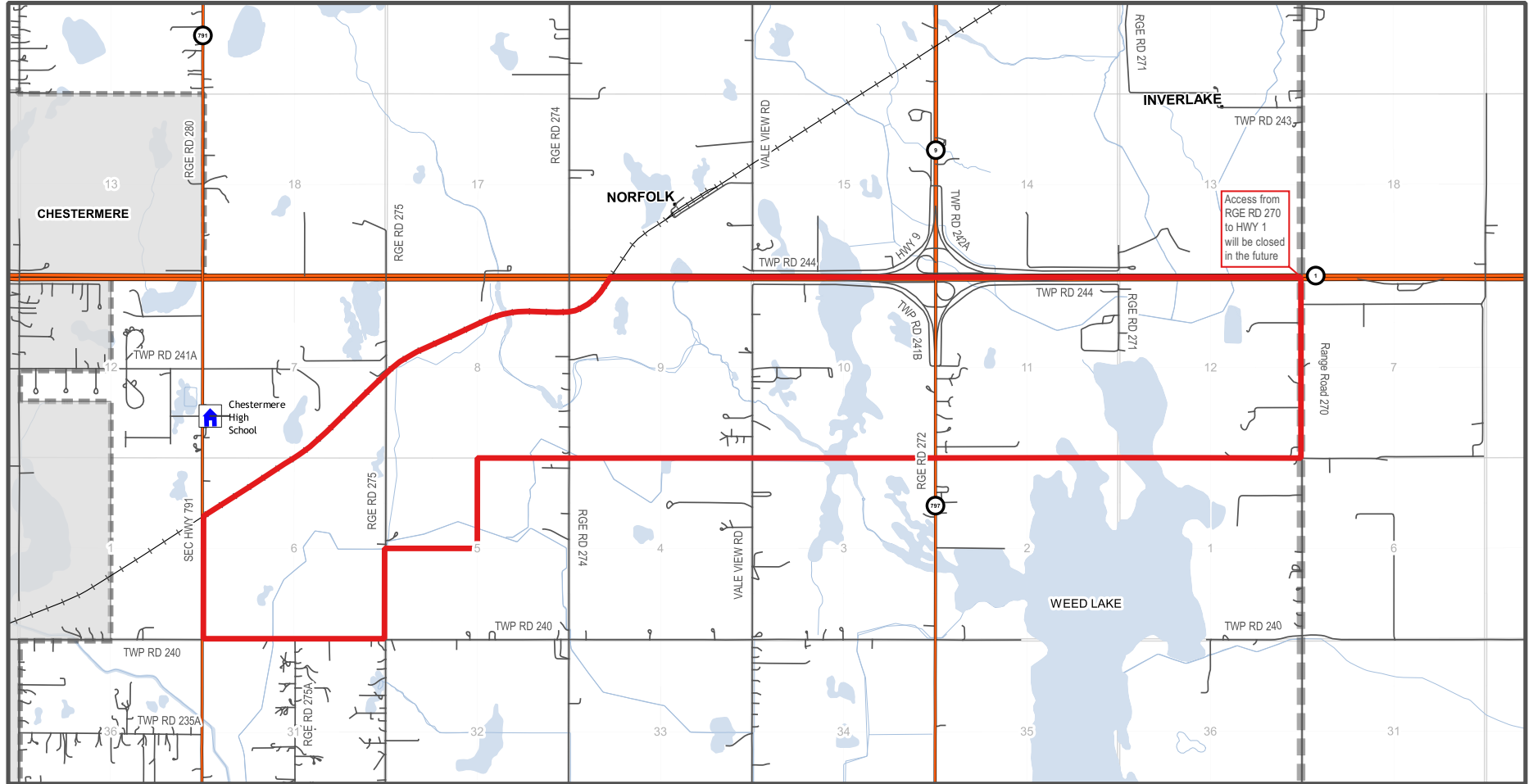


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ROCKY VIEW  
COUNTY

**MAP 9 | EXISTING TRANSPORTATION & MOBILITY NETWORK**



**Existing Transportation**

- Area Structure Plan
- Primary Highway
- Secondary Highway
- Local Road
- Access Railway
- Rocky View County
- City Boundary

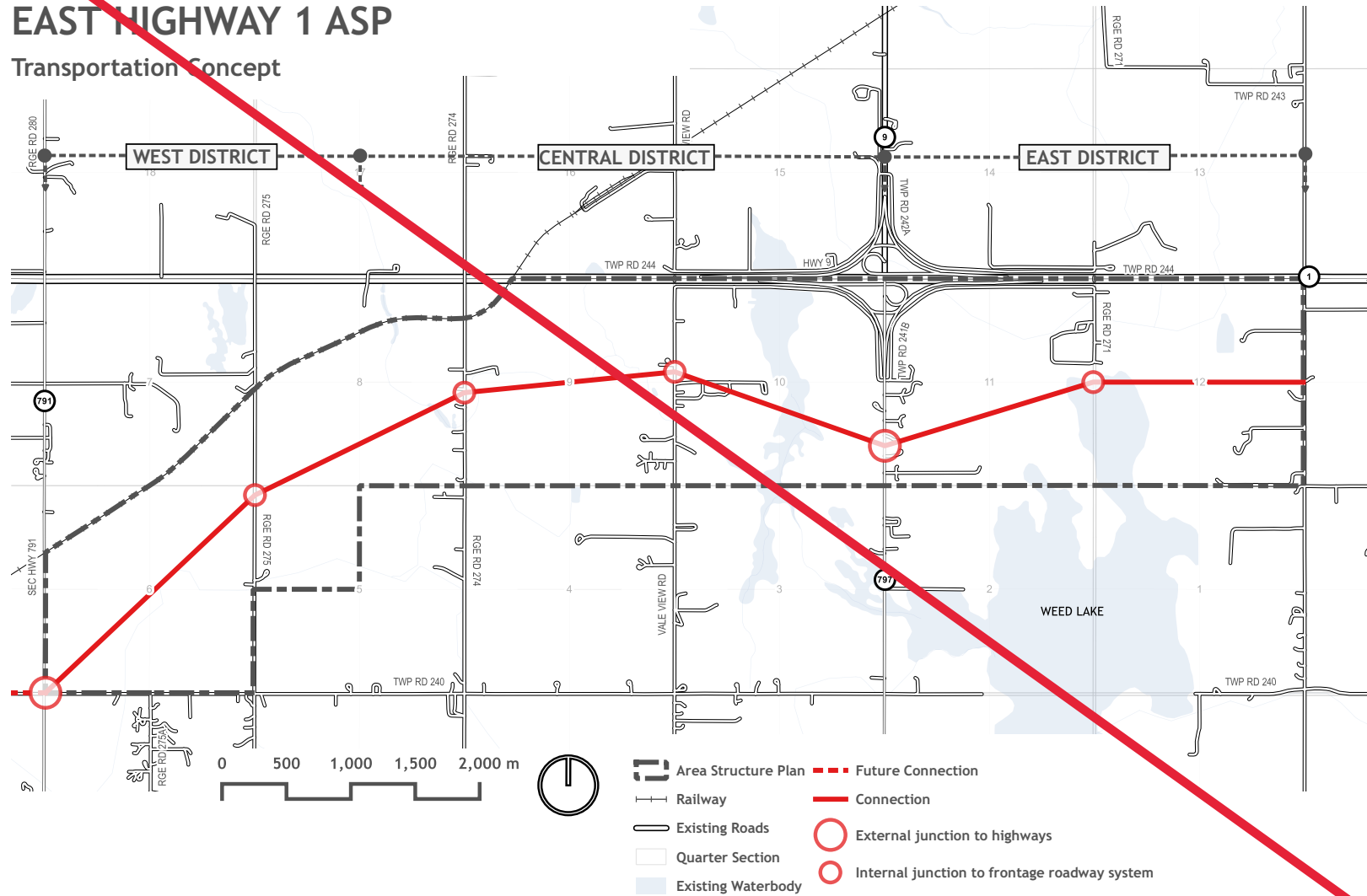
0 1 2 3 km



MAP 10 | TRANSPORTATION & MOBILITY CONCEPT

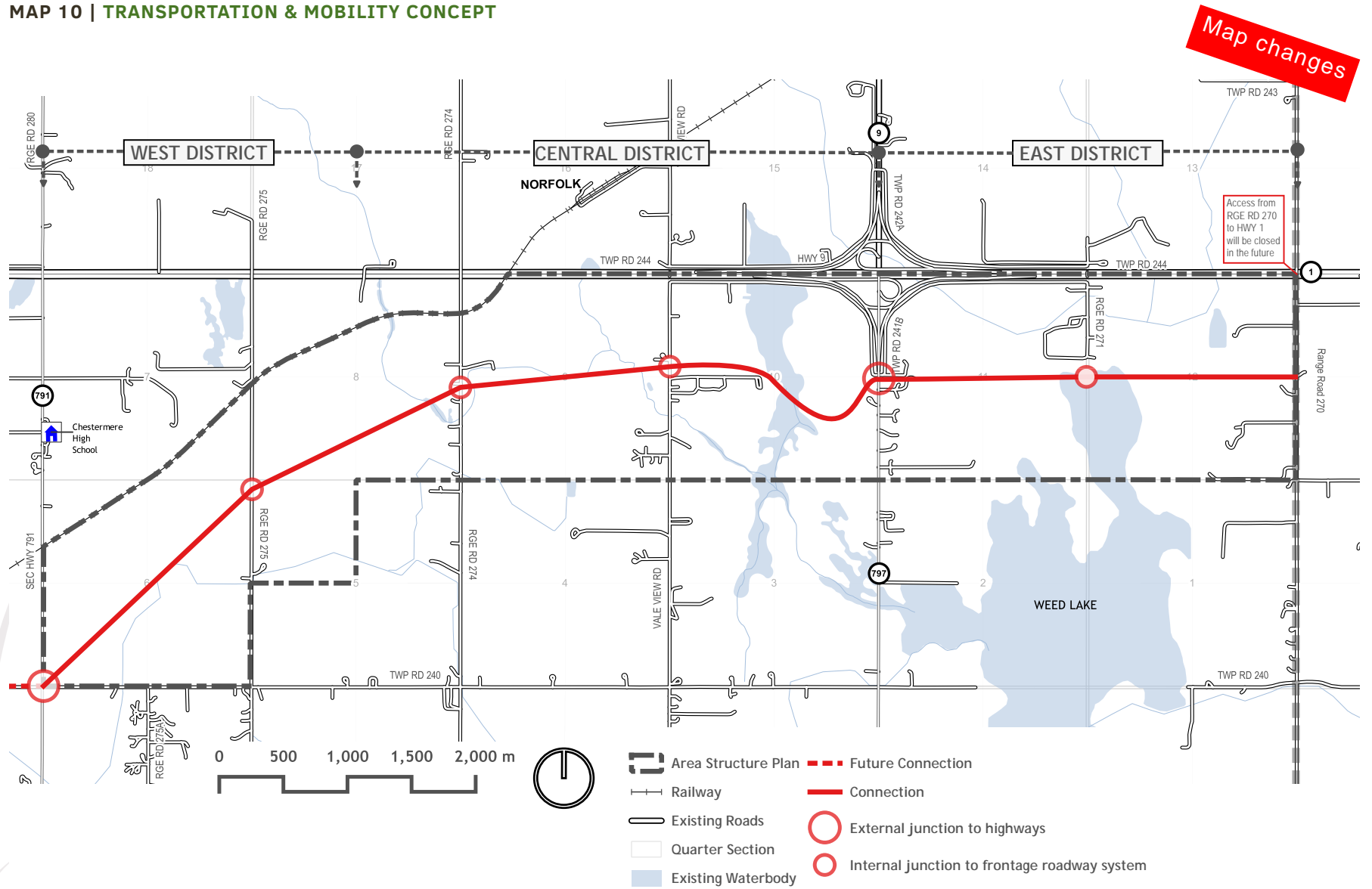
# EAST HIGHWAY 1 ASP

## Transportation Concept





MAP 10 | TRANSPORTATION & MOBILITY CONCEPT



## POLICIES

### 16.1 GENERAL

- 16.1.1 A Transportation Impact Assessment shall be required as part of the local plan preparation.
- 16.1.2 The regional transportation system should be developed in general accordance with **Map 10: Transportation & Mobility Concept**. The classifications of the grid road network may be refined through further transportation analysis and / or at the local plan stage.
- 16.1.3 The County shall collaborate with Wheatland County, and the Province regarding regional road connections and interchanges.
- 16.1.4 The County should collaborate with adjacent municipalities to ensure connections of streets, pedestrian, and bicycle networks align and transition smoothly across municipal boundaries.
- 16.1.5 The County encourages and supports the inclusion of a pedestrian and bicycle network as part of the East Highway 1 Transportation Network.

### 16.2 LOCAL TRANSPORTATION NETWORK - GENERAL

- 16.2.1 The design and construction of roadways within the local transportation network shall utilize sound access management principles and shall be in accordance with the County Servicing Standards.
- 16.2.2 The designation and design of local roads within the transportation network, including classification, street sizing, and intersection / access spacing, shall be determined at the time of local plan preparation. Local roads shall be designed in accordance with the urban or rural cross section requirements established by the County.

### 16.3 LOCAL ROADS - INDUSTRIAL AND COMMERCIAL

- 16.3.1 The type of road cross section (urban or rural) within industrial areas shall be determined at the time of local plan preparation.
- 16.3.2 Industrial areas should provide internal pathways and pathway connections to the regional trail network.
- 16.3.3 All roads within commercial areas should be designed to an urban road standard. Commercial development shall provide for safe and efficient pedestrian and bicycle circulation between buildings, sites, and, where applicable, adjacent areas.

# 17.0

## UTILITY SERVICES

### OVERVIEW

Well-designed and effective utility services are the foundation of a well-planned community and competitive ag-business area. Development within the East Highway 1 ASP will connect to Rocky View County utility system for wastewater and an onsite water treatment facility for the potable water system. The utility systems for the Hamlet of Langdon and surrounding area are supplied by Langdon Waterworks for potable water and Rocky View County for transmission lines and associated facilities to dispose of wastewater. Private companies provide shallow utilities such as gas, electricity, and telecommunications to the area. **Map 11: Water Infrastructure**, shows the alignments of existing and proposed water transmission lines, water treatment facility, pump stations, and raw and treated water reservoirs in the East Highway 1 ASP area. The potable water system could have a potential to service adjacent lands outside the ASP as policy changes allow. **Map 12: Sanitary Infrastructure** shows existing and proposed wastewater transmission lines, lift stations, and sanitary catchment areas in the ASP area. The costs to service the ASP area should be shared by all benefiting

### OBJECTIVES

- Ensure potable water and wastewater systems are provided to the Plan area in a safe, cost effective, and fiscally sustainable manner.
- Identify and protect utility service routes.
- Support water conservation.
- Ensure shallow private utility systems are provided to new development.
- Ensure fire suppression and water supply infrastructure is provided to deliver the appropriate level of fire protection within the Plan area.

### TECHNICAL REVIEW - WATER

The water infrastructure at full buildout will consist of an onsite water treatment facility, raw water reservoir, treated water reservoir, pump station, 525mm and 450mm water transmission mains to the Central and West Districts and East Districts respectively. The potable water source will come from an existing Alberta Transportation borrow pit that could accommodate a raw water reservoir located along the west side of Highway 797 towards the centre of the site. Alberta Environment approvals will be required to accommodate the projected East Highway 1 ASP water demands (see Water System Concept Plan and separate detailed Servicing Study by IDEA Group, March 2021).

## TECHNICAL REVIEW - SANITARY SEWER

The sanitary infrastructure at full buildout will consist of two sanitary lift stations conveying wastewater through a 400mm sanitary force main running along Highway 797 from the East Highway 1 ASP area to the Langdon Wastewater Treatment Plant. Upgrades will be required to the existing Langdon Wastewater Treatment Plant to accommodate the projected ASP sanitary flows. (see Sanitary Sewer System Concept Plan and separate detailed Servicing Study by Idea Group, March 2021)

The ASP area sanitary servicing concepts offer opportunities to accommodate additional areas outside of the ASP area to the east and north west of the ASP area as shown on **Map 12: Sanitary Infrastructure**. There are opportunities for interim temporary wastewater lift stations, transmission lines, tanks, truck in and truck out, septic mound and field options to bring on the development parcels within the ASP area to market early. An interim temporary wastewater service connection to the existing East Balzac Sanitary force main could be considered as the force main has capacity at this time as the Balzac area is not at full buildout.

## POLICIES

### 17.1 UTILITIES - LOCATION & PHASING

- 17.1.1 Utility service development should support an orderly, logical, and sequential pattern of development.
- 17.1.2 The provision, alignment, and capacity of the water distribution system shall be in general accordance with **Map 11: Water Infrastructure**.
- 17.1.3 The provision, alignment, and capacity of the sanitary sewer system shall be in general accordance with **Map 12: Sanitary Infrastructure**.

- 17.1.4 The location and size of utility rights-of-way and easements, and related line assignments, should be determined at the local plan stage to the mutual satisfaction of the County, the developer, and the utility companies.
- 17.1.5 Utility rights-of-way and easements shall be provided to accommodate County utilities and shallow utilities at the subdivision or development permit stage, as deemed necessary by the utility provider.
- 17.1.6 Full piped servicing shall be installed by the developer to the Plan Area following approval of the first Local Plan that includes lands within phase 3 (Central District of the ASP) or if the intermunicipal regional servicing needs full piped services whichever comes first. Interim servicing solutions in compliance with this ASP will be required prior to the completion of full piped servicing to the Plan Area.

### 17.2 WATER

- 17.2.1 All new development shall connect to the County's potable water system once available.
- 17.2.2 Notwithstanding policy 17.2.1 (above) developments may be permitted to provide individual potable water solutions on a temporary basis in accordance with County policy if the following conditions are met:
  - a. The County's potable water system is not yet available to the site;
  - a. The developer enters into a deferred services agreement and connects to services when available;
  - a. The developer agrees that no compensation will be provided to the developer for the costs incurred for the construction of the temporary servicing solution.
  - a. The proposed temporary solution meets provincial

regulations; and  
a. The development is not a heavy water user.

17.2.3 A Water Use Assessment shall be required with local plan preparation, subdivision applications, and / or development permit applications to determine water demand and infrastructure required to meet that demand.

17.2.4 Development and buildings relying on potable water provided by the County utility system shall use low flow fixtures and appliances.

17.2.5 The County encourages the reduction and reuse of water in accordance with provincial laws and regulations.

### **17.3 WASTEWATER**

17.3.1 All new development shall be required to connect to the County's wastewater system once available.

17.3.2 Notwithstanding policy 17.3.1 (above) developments may be permitted to provide individual wastewater solutions on a temporary basis in accordance with County policy if the following conditions are met:

- a. The County's wastewater system is not yet available to the site;
- a. The developer enters into a deferred services agreement and connects to services when available;
- b. The developer agrees that no compensation will be provided to the developer for the costs incurred for the construction of the temporary servicing solution.
- c. The proposed temporary solution meets provincial regulations; and
- d. The development is not a heavy wastewater user.

17.3.3 A Wastewater Servicing Study shall be required with local plan preparation, subdivision applications, and / or development permit applications to determine wastewater demand and infrastructure required to meet that demand.

17.3.4 Sump pumps and stormwater drainage systems shall not be connected to the wastewater system.

### **17.4 SHALLOW UTILITIES**

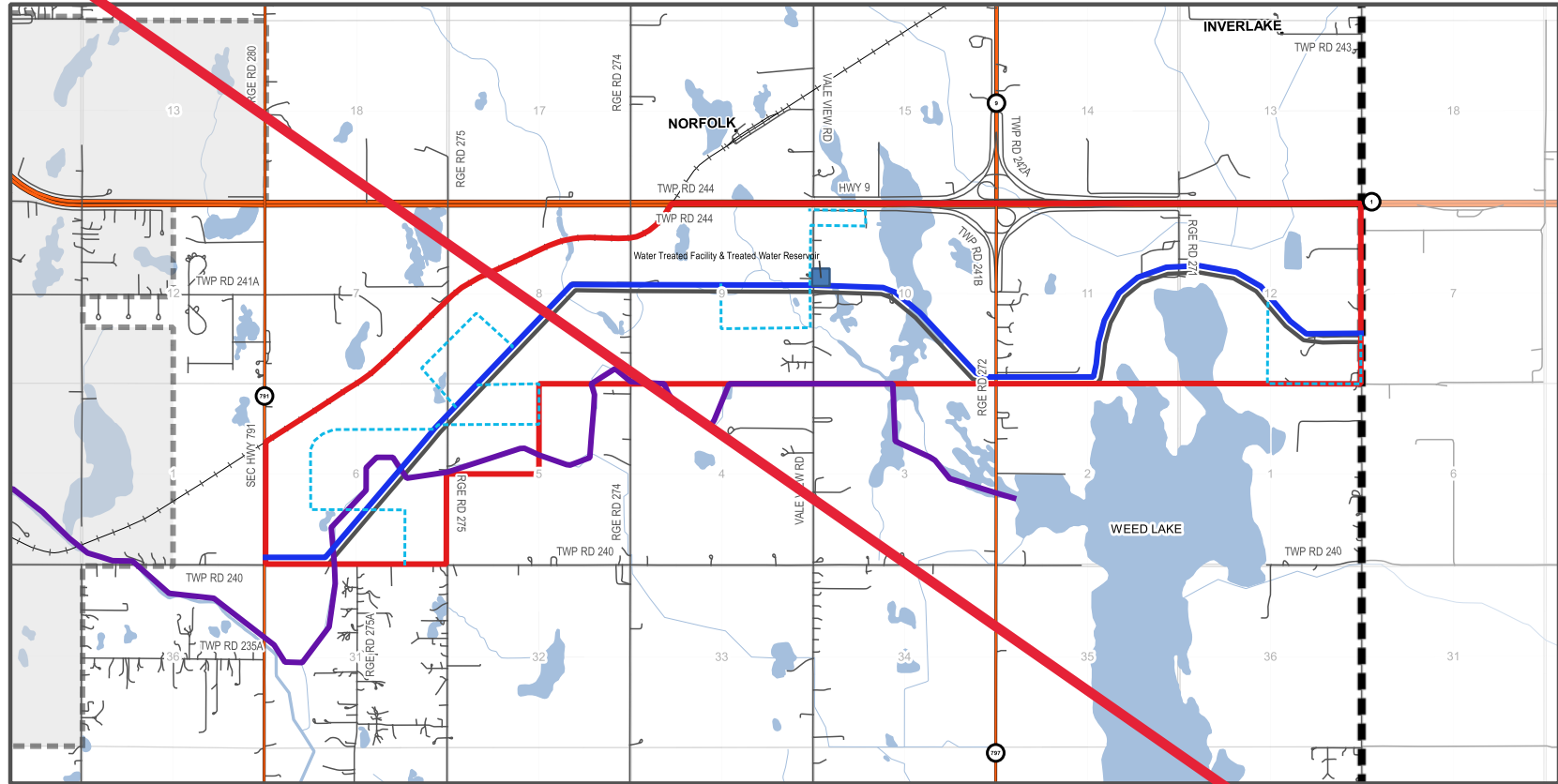
17.4.1 All new development shall be serviced with shallow utilities at the expense of the developer.

### **17.5 EMERGENCY SERVICE INFRASTRUCTURE**

17.5.1 All industrial and commercial buildings are required to provide fire suppression systems and shall be in compliance with the County's Fire Suppression Bylaw.

17.5.2 All water systems serving developments within the Plan area shall be designed to provide adequate water pressure and volume to combat fires.

MAP 11 | WATER INFRASTRUCTURE



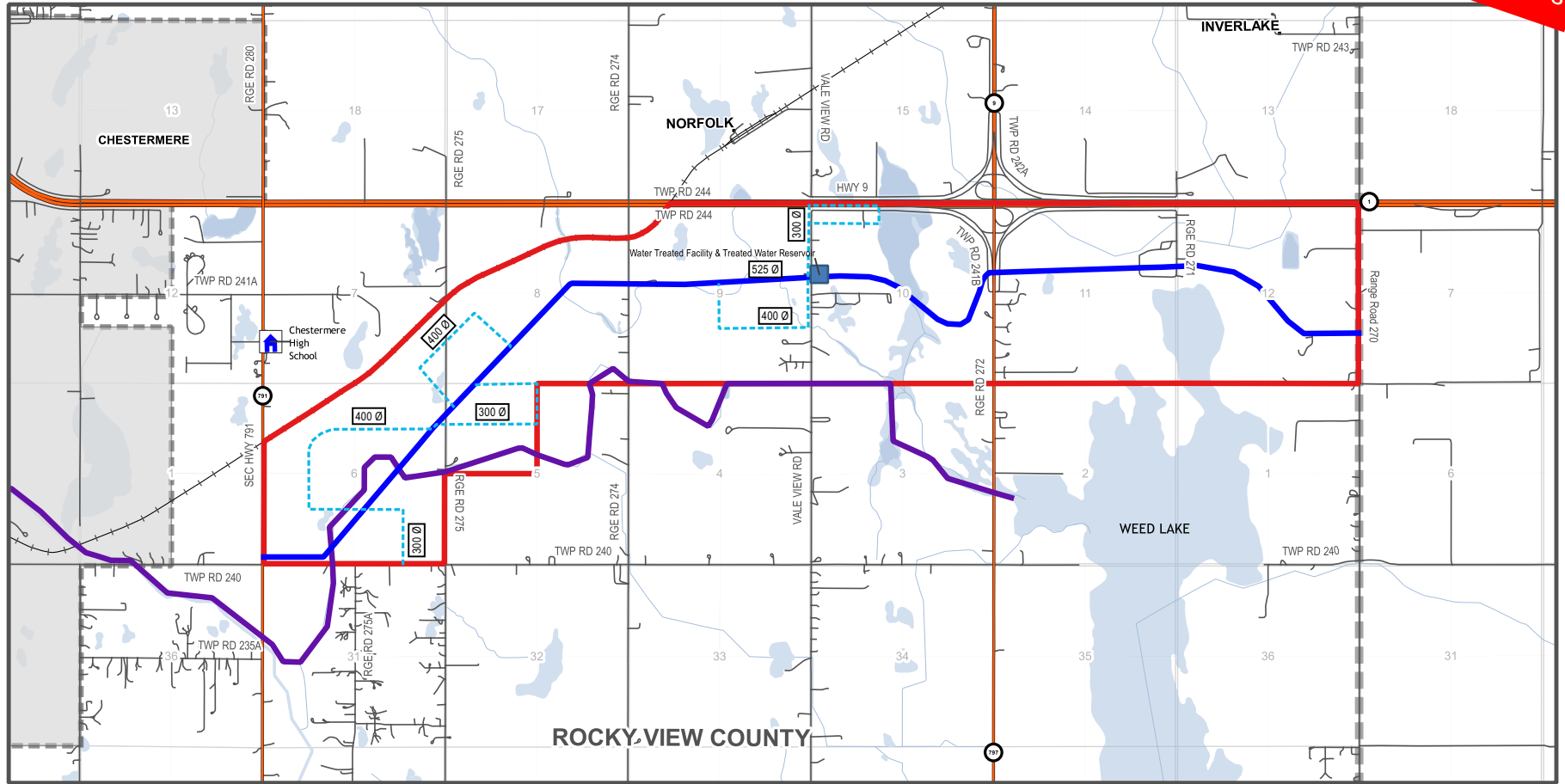
Proposed Water Infrastructure

- Area Structure Plan
- Rocky View County
- Existing Langdon Ditch
- Proposed Water Main
- Conceptual Water Main
- Water Facility



MAP 11 | WATER INFRASTRUCTURE

Map changes

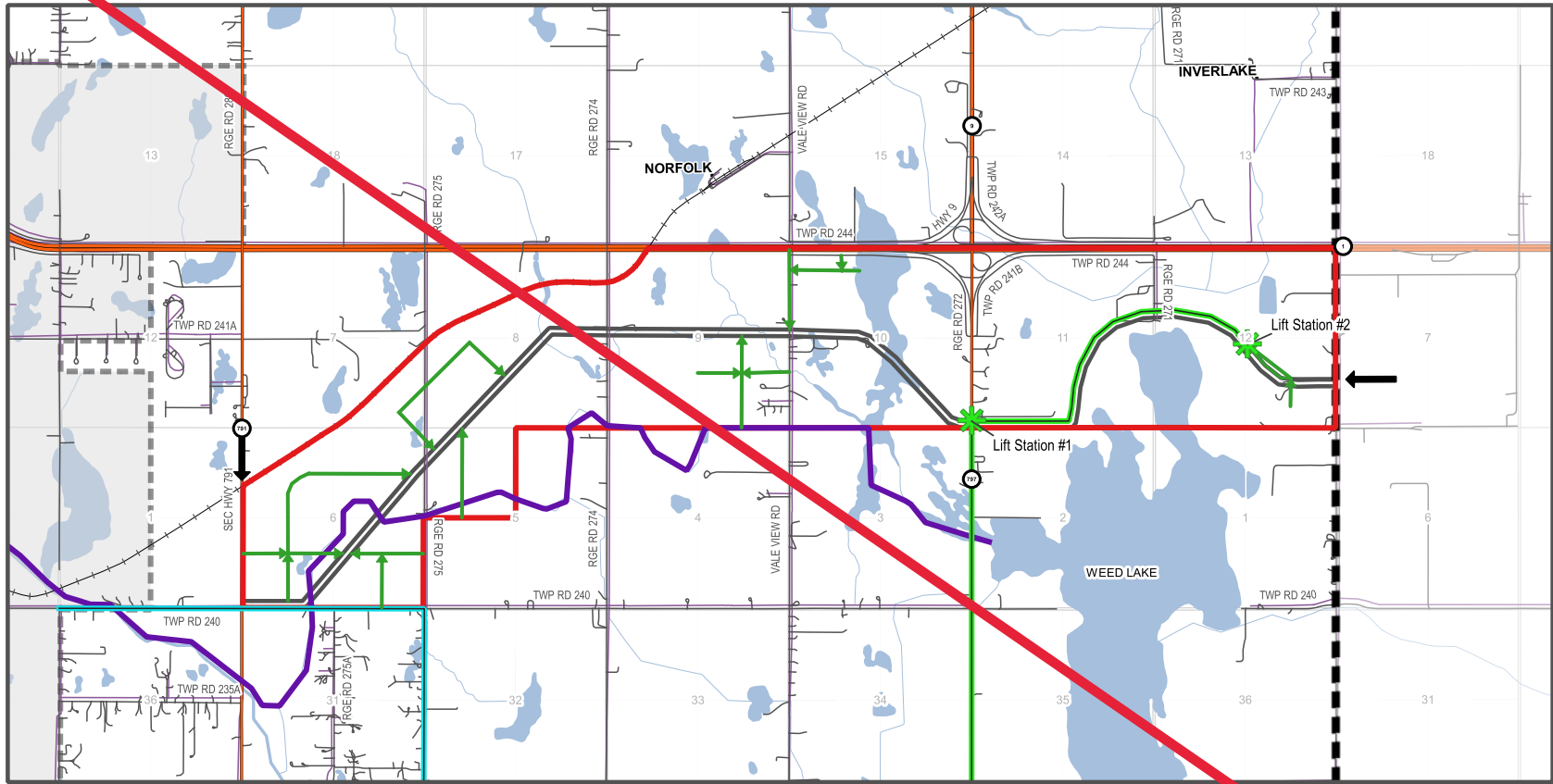


Proposed Water Infrastructure









- Area Structure Plan
- Proposed Water Main
- Rocky View County
- Conceptual Water Main
- Existing Langdon Ditch
- Water Facility



MAP 12 | SANITARY INFRASTRUCTURE



**Proposed Sanitary Infrastructure**

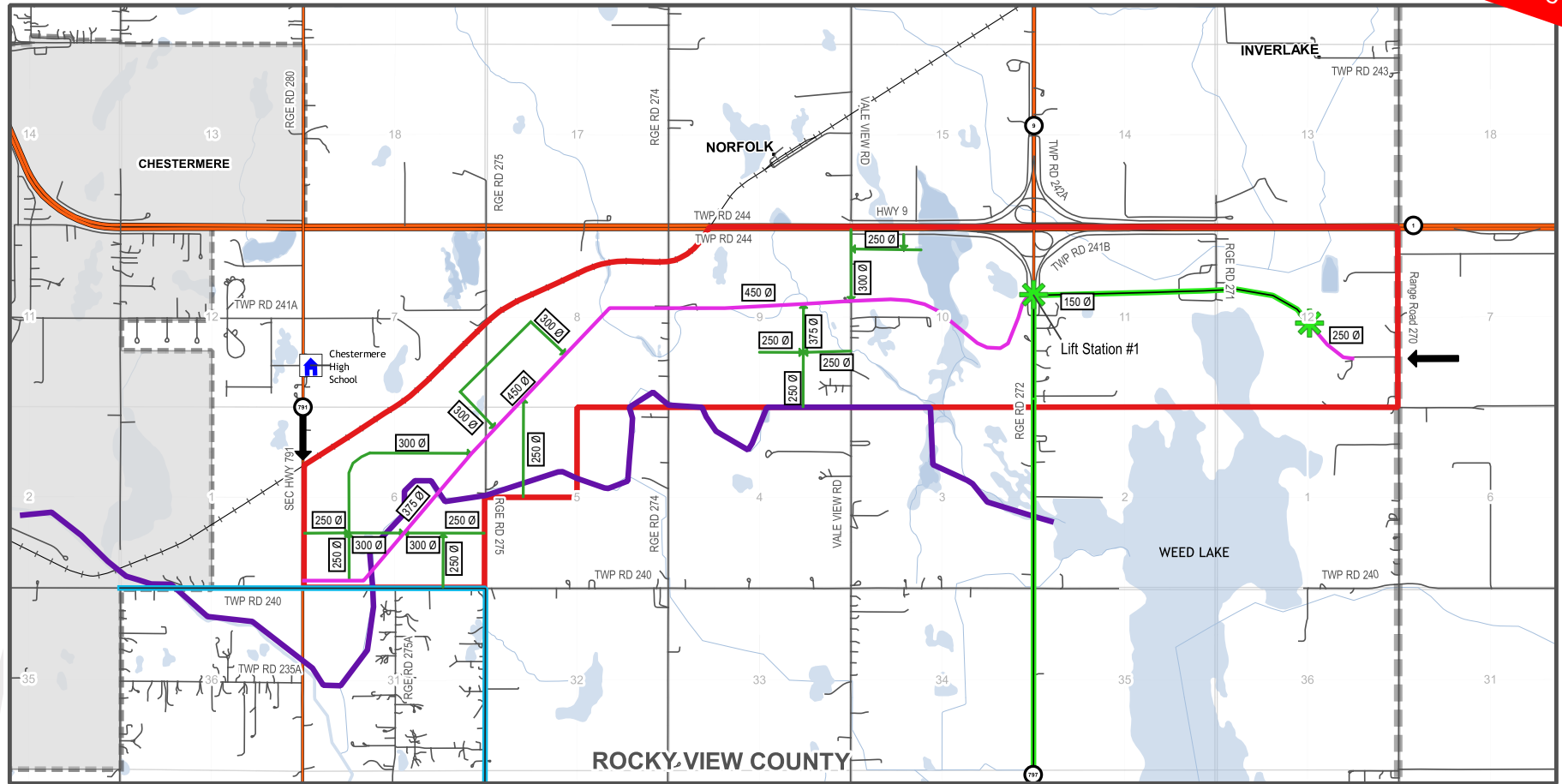
- |  |  |
|--|--|
|  Area Structure Plan              |  East Balzac Sanitary Forcemain |
|  Rocky View County                |  Proposed Sanitary Forcemain    |
|  Conceptual Sanitary Gravity Main |  Existing Langdon Ditch         |
|  Proposed Lift Station            |  Possible External Served Areas |





MAP 12 | SANITARY INFRASTRUCTURE

Map changes



Proposed Sanitary Infrastructure

- Area Structure Plan
- Rocky View County
- Conceptual Sanitary Main
- Sanitary Gravity Main
- ★ Proposed Lift Station
- East Balzac Sanitary Forcemain
- Proposed Sanitary Forcemain
- Existing Langdon Ditch
- ➔ Possible External Serviced Areas

0 1 2 3 km



# 18.0

## STORMWATER

### OVERVIEW

The existing topography generally flows from northwest to the southeast towards the existing Langdon Ditch which flows to Weed Lake located southeast of the ASP area. There are no natural streams or rivers within the ASP area, but there is a significant amount of existing wetlands and water bodies closer to Weed Lake. The hydrology of the area will be impacted by the various development districts to varying degrees with the West District having more intensive land uses. The hydrology of the area of the more intensive land uses will result in an alteration of the flow paths, increased prevalence of impervious surfaces which reduces natural surface water absorption and increased sediment potential. A comprehensive stormwater management plan will need to be developed for the ASP plan area to manage surface water, prevent flooding, and ensure output meets water quality and quantity guidelines.

### OBJECTIVES

- Maintain the natural flow of the existing stormwater as much as possible while addressing and detaining additional water volume, rate, and maintaining or improving water quality.

### TECHNICAL REVIEW

An ASP level Stormwater Management Study has been prepared by IDEA Group in support of the ASP area in accordance with the Terms of Reference, and the Stormwater Management Guidelines for the Province of Alberta. The site is split into three existing drainage areas with the majority of the three district areas draining northwest to south east. The south east area of the West District drains to the south towards the existing municipal ditch. The northeast area of the East District drains to the north. The existing topography of the site will generally remain once the ASP area is fully built out with the major drainage areas being maintained post development. With the ASP plan area being split by the major transportation spine running through the site, a creek system will be developed within the ASP plan area to address both water volume, rate reduction, and water quality treatment utilizing a manmade creek system. The manmade creek system will mimic the natural creek characteristics and water treatment processes through biofiltration to address the stormwater management for the ASP plan area. The creek system will be split between a north and south Sunbelt Creek network that will meander through the development and collect surface runoff through the Sunbelt plan area and convey stormwater to stormwater ponds and ultimately release to the larger water bodies to the east of the plan area. Within the Sunbelt Creek system the stormwater volume and water quality will be address through a “kidney” system that collects the stormwater with the stormwater ponds which creates the water circulation within the ponds before it releases to downstream Creek. The creek will naturally expand and deepen in natural low lying areas throughout the ASP plan area to accommodate runoff volume and control discharge rates and clarify stormwater prior to discharging to the existing eastern water bodies.

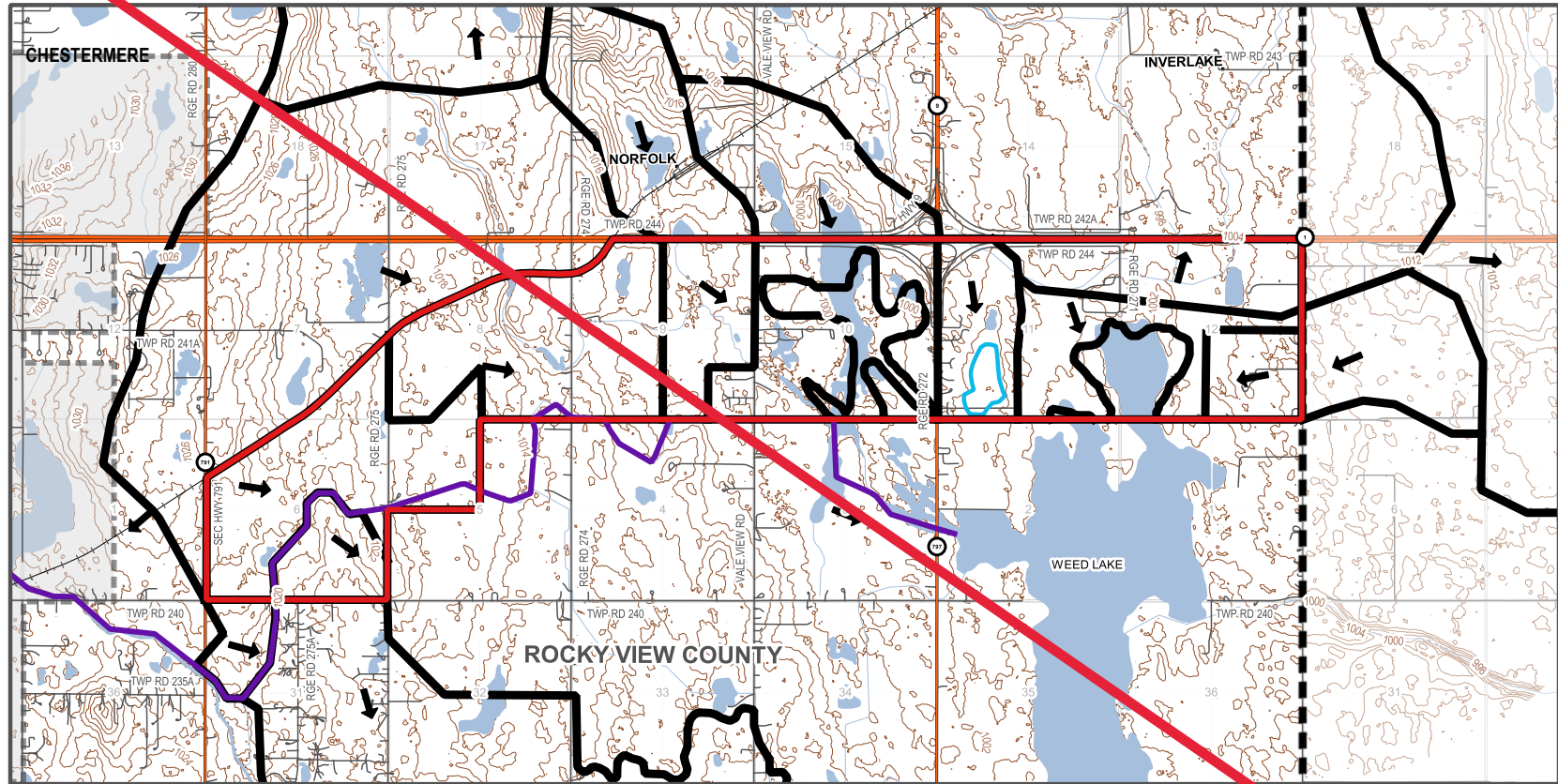
## 18.1 LOCAL STORMWATER MANAGEMENT

- 18.1.1 The location of the natural stormwater drainage conveyance system shall be protected and acquired as part of the development process, in general accordance with **Map 13: Stormwater Management Plan**.
- 18.1.2 Stormwater conveyance systems should develop in an orderly, logical, and sequential pattern of development.
- 18.1.3 Stormwater shall be conveyed downstream in a manner that protects downstream properties.
- 18.1.4 Where required, proponents of new development shall identify and secure, in consultation with the County, the downstream stormwater conveyance system.
- 18.1.5 Stormwater conveyance systems must provide a right-of-way of sufficient width to accommodate upstream stormwater flow.
- 18.1.6 Stormwater ponds or constructed wetlands should be located:
  - a. on an accessible public utility lot; and
  - b. outside of the riparian setback area.
- 18.1.7 Natural wetlands and / or natural drainage courses that are retained should receive treated stormwater through direct or indirect flow in order to maintain the value of the wetland and the drainage course.

## 18.2 STANDARDS AND DESIGN

- 18.2.1 Stormwater infrastructure shall be constructed, operated, and maintained in accordance with the County Servicing Standards, County Policy, and Provincial regulations. The stormwater management system should be designed to:
  - a. operate on a gravity basis;
  - b. wherever possible, use the stormwater drainage conveyance system, as generally shown on **Map 13: Stormwater Management Plan**;
  - c. accommodate stormwater flows from adjacent transportation networks;
  - d. preserve the value of existing wetlands; and
  - e. conform to an urban standard where a curb and gutter transportation system is provided.
- 18.2.2 Stormwater conveyance alignments and ponds are shown conceptually on **Map 13: Stormwater Management Plan**. Alternate and more cost effective alignments may be considered at the local plan stage if among other matters, it can be shown that the impact on wetlands within the identified conveyance system is reduced through the use of an alternative alignment.

MAP 13 | STORMWATER MANAGEMENT PLAN



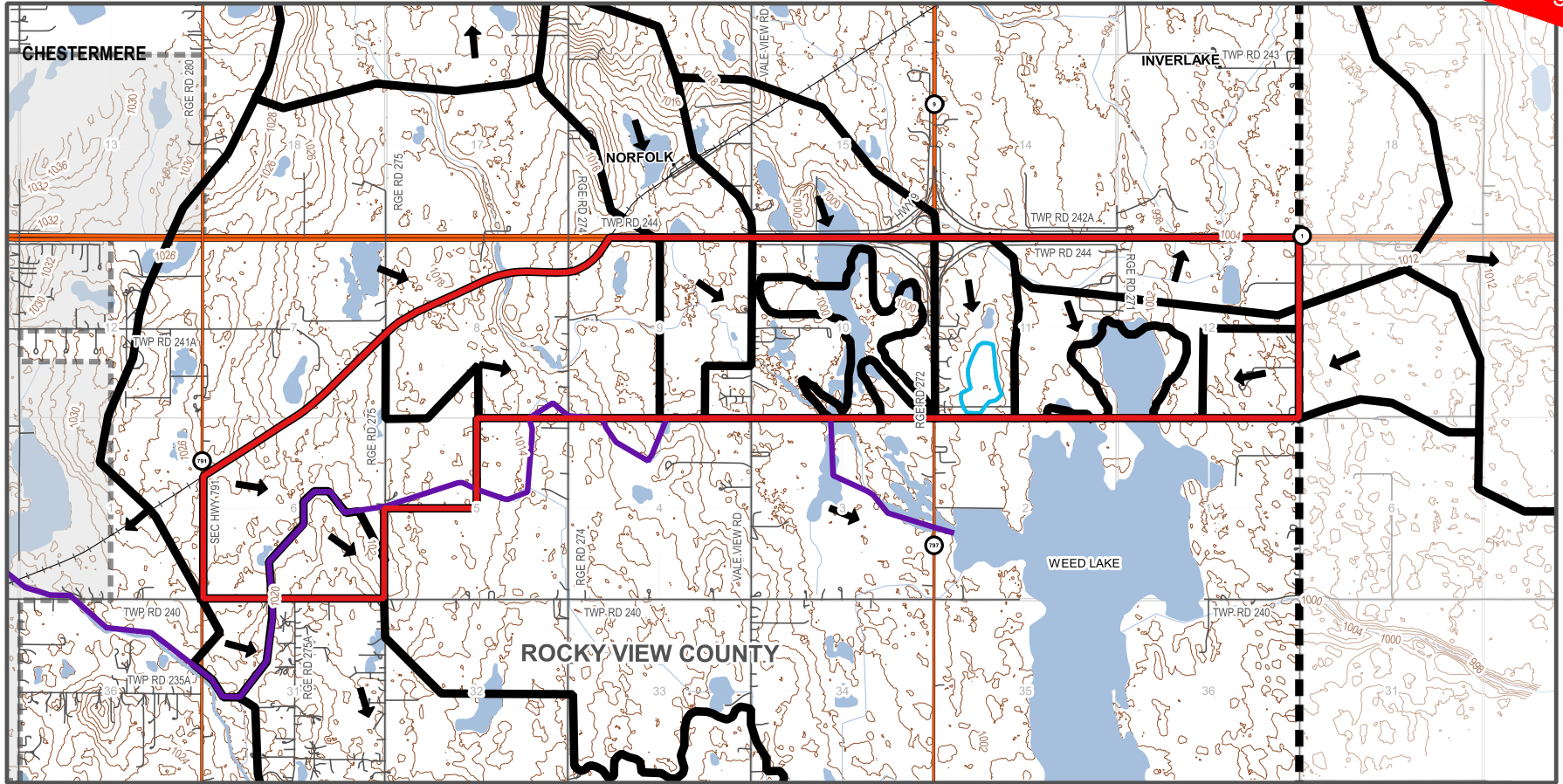
**Stormwater Servicing**

- Area Structure Plan
- Drainage Boundary
- Rocky View County
- Contours
- Existing Langdon Ditch
- Natural Depression
- Overland Drainage



MAP 13 | STORMWATER MANAGEMENT PLAN

Map changes



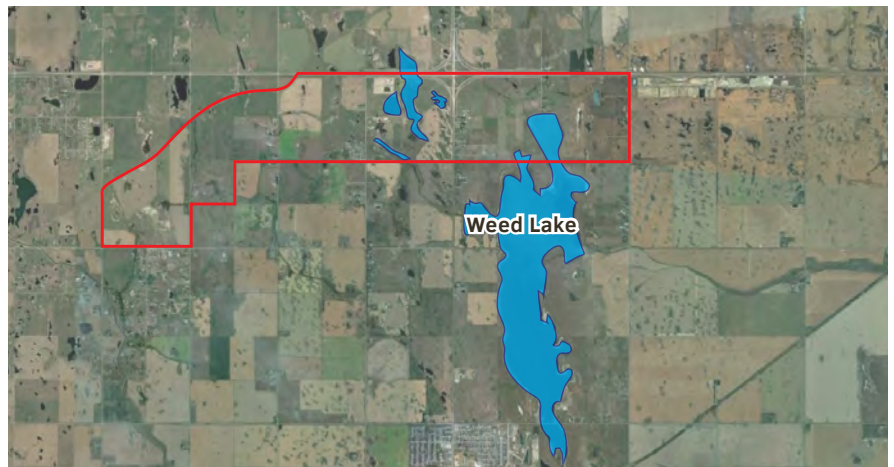
**Stormwater Servicing**

- Area Structure Plan
- Drainage Boundary
- Rocky View County
- Existing Langdon Ditch
- Contours
- Natural Depression
- Overland Drainage





*Image courtesy of Google Earth - Facing north from Highway 560 over Weed Lake*



## 19.0

## SOLID WASTE & RECYCLING

Solid waste policies address the management of solid waste through all stages of development; from construction and demolition to full build-out. The policies emphasize the reduction and diversion of waste through the recycling and reuse of materials. Each development stage has different solid waste requirements and the policies below provide guidance to developers and residents on effectively managing solid waste.

### OBJECTIVES

- Ensure local plans address solid waste management during all stages of development and are in alignment with the County's Solid Waste Master Plan and/or Solid Waste Servicing Strategy.
- Promote plans that support the 3R's and circular economy principles including waste minimization and keeping all materials at their highest and best use.

### 19.3 POLICIES

19.1.1 The developer shall be responsible for the management and disposal of solid waste generated through all stages of construction in accordance with County standards.

19.1.2 A local plan should:

- address solid waste management through all stages of development, including occupancy;
- align with the County's Solid Waste Master Plan and/or Solid Waste Servicing Strategy;
- embrace opportunities to redesign systems to eliminate waste and keep products and materials at their highest use;
- conform to any solid waste standards set by the County; and
- consider the 3Rs hierarchy of reducing, reusing, and recycling in all decisions regarding solid waste management.

19.1.3 All industrial, commercial, and institutional facilities and business owners shall be responsible for providing their own solid waste services.

19.1.4 **All industrial, commercial, and institutional facilities must consider the 3Rs hierarchy of reducing, reusing, and recycling in decisions regarding solid waste management.**

# 20.0

## EMERGENCY SERVICES

### OVERVIEW

Emergency services within the Plan area include fire and protective service needs.

### OBJECTIVES

- Ensure an appropriate and efficient level of fire and protective services is made available for current and future residents in order to provide for a safe and livable community.
- Ensure communities are designed and constructed to optimize the delivery of fire and protective services.

### 20.1 POLICIES

- 20.1.1 In association with County Fire Services, the RCMP, and other emergency service providers, an adequate level of service shall be provided to meet current needs, as well as future needs, based on projected population growth and demographic change in the Plan area.
- 20.1.2 Policing will be provided by the RCMP as per the Provincial Police Service Agreement, until such time as another policing solution is required or sought out.
- 20.1.3 All industrial and commercial buildings should provide fire suppression systems and they shall be in compliance with the County's Fire Suppression Bylaw and the Alberta Building Code.

- 20.1.4 Local plans shall address fire and protection response measures and on-site firefighting requirements through consideration of such factors as efficient road design, safe and efficient access for emergency service vehicles, wild land fire protection, and fire control measures.
- 20.1.5 Crime Prevention Through Environmental Design (CPTED) features should be considered and incorporated into the design and construction of all new development, wherever possible.
- 20.1.6 The County shall collaborate with CN to develop an Emergency Response Plan to mitigate any risks related to railway lands and train movements.



# 21.0

## OIL AND GAS

Oil and gas facilities, infrastructure, and operations are industrial land uses that have the potential to affect public safety, quality of life, and the natural environment. The co-existence of these oil and gas activities with other forms of development in the Plan area is an important consideration in the area's development. See **Map 14: Oil and Gas**.

### OBJECTIVES

- Ensure appropriate and safe land development in relationship to petroleum facilities and wells.
- Allow for the continued safe operation of petroleum facilities and wells.

### 21.1 GENERAL POLICIES

- 21.1.1 Applicants proposing to develop land in the vicinity of petroleum facilities and wells shall adhere to the setback requirements and policies of this Plan, and the Directives and Bulletins of the Alberta Energy Regulator.
- 21.1.2 At the time of subdivision or development, a restrictive covenant shall be registered that prevents the construction of any building within the setback area associated with an active, suspended, or abandoned well.
- 21.1.3 As part of a local plan preparation process, applicants shall obtain a Land Development Information package from the Alberta Energy Regulator and identify the locations of all petroleum wells and pipelines (abandoned and operating) in

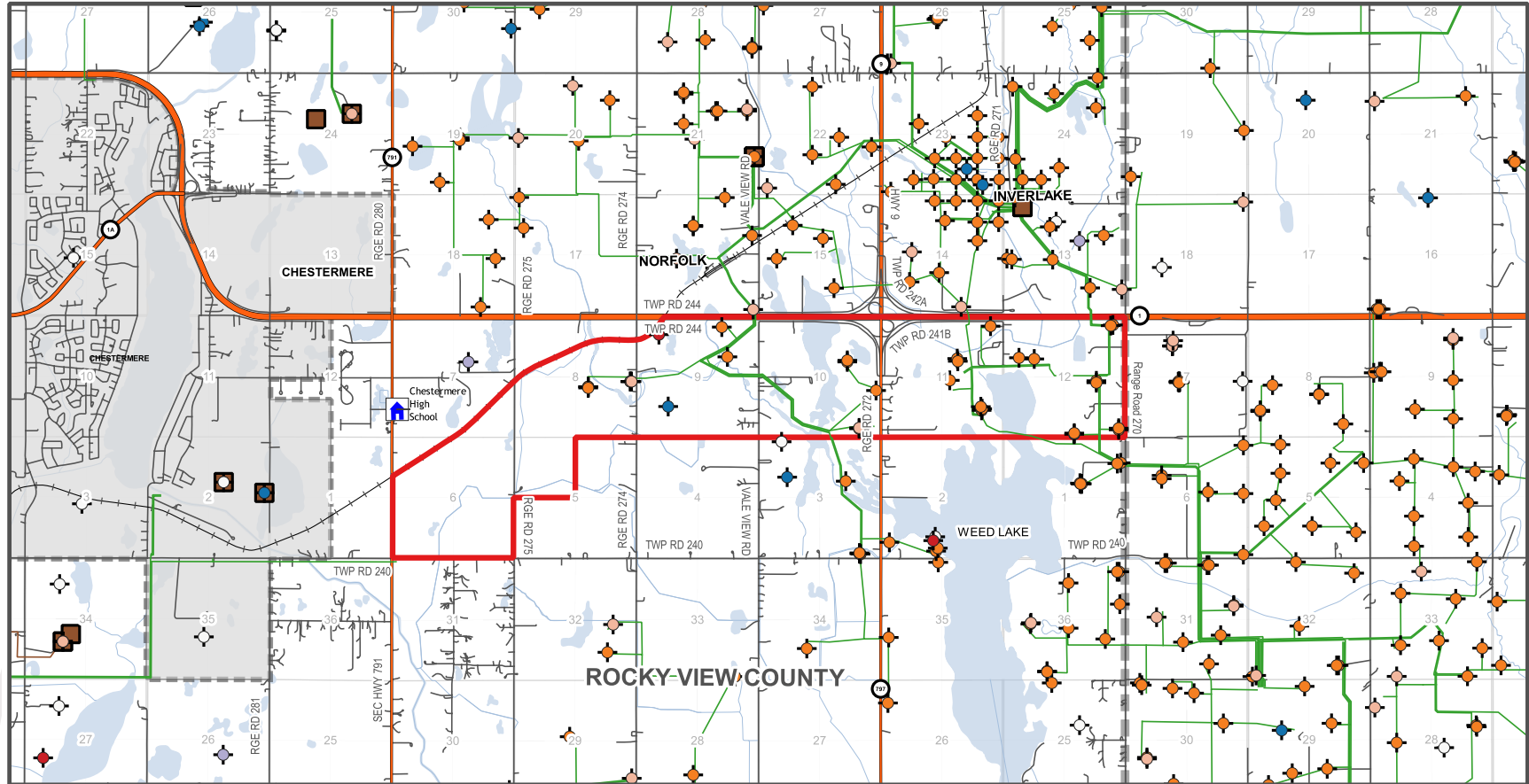
the local plan area. In addition, the applicant must determine if an Emergency Planning Zone has been established around a sour gas facility or well.




- 21.1.4 Prior to the preparation of a local plan to develop lands within 1.5 km of a petroleum facility that is situated within an Emergency Planning Zone, the developer shall consult with the County and the operator of the facility to determine how an Emergency Response Plan will be prepared, updated, or replaced.
- 21.1.5 The location, development setbacks, emergency planning zones, and emergency response planning regarding all petroleum facilities shall be identified in the local plan and included in any marketing information and other public communication materials for petroleum facilities.

### 21.2 ABANDONED OIL & GAS WELLS

- 21.2.1 All buildings located in proximity to an abandoned well site shall comply with the Alberta Energy Regulator setback requirements or provide a minimum building setback of 40 metres for residential development and 20 metres for all other development, whichever is greater.
- 21.2.2 Vehicular access to an abandoned well site shall:
- a. be determined through discussion with the abandoned well licensee;
  - b. be identified in the local plan; and
  - c. be protected by easements in favour of the County at the time of subdivision or development approval.





MAP 14 | OIL AND GAS





-  Area Structure Plan
-  Section
-  Oil and Gas Facility

**Well License Status**

-  Abandoned
-  Amended
-  Issued

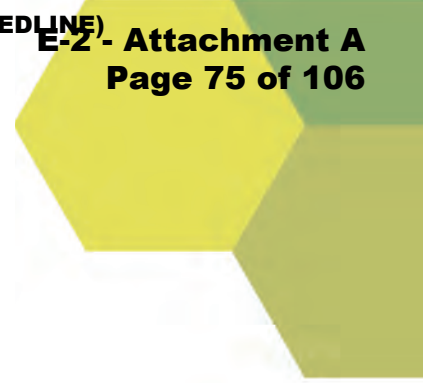
-  Reclamation Certified
-  Reclamation Exempt
-  Re-Entered
-  Suspension

**Pipeline Installations**

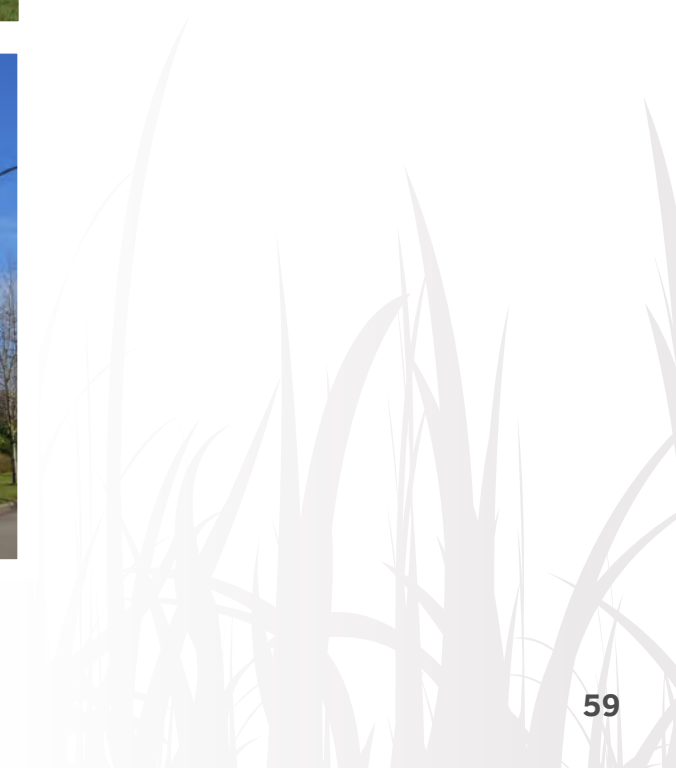
-  Pipeline-Natural & Sour Natural Gas
-  Pipeline-Oil-Well Effluent

0 1 2 3 4 5 km





# IMPLEMENTATION



# 22.0

## IMPLEMENTATION

The East Highway 1 Area Structure Plan outlines the vision for the future physical development of the Plan area and provides guidance with regard to infrastructure, land use, subdivision, and development. The purpose of this Section is to describe the Plan implementation process, to provide detail on the phases of development, and to specify requirements to ensure the Area Structure Plan policies and strategies are followed.

### OBJECTIVES

- Implement the Land Use Strategy and policies of the East Highway 1 Area Structure Plan in a way that is fiscally and environmentally sustainable.
- Ensure the cost of infrastructure development is identified and provided.
- Provide for the logical phasing of development.
- Ensure local plans adhere to the vision and policies of the Plan.
- Provide for the review and amendment of the Plan as required.

### 22.1 LOCAL PLANS, REDESIGNATION, SUBDIVISION AND DEVELOPMENT APPLICATIONS

Local plans are to be developed within the framework provided by this Area Structure Plan. In addition to the below policies, the standard technical requirements of a conceptual scheme or master site development plan are identified in the County Plan.

- 22.1.1 Applications for redesignation, subdivision, and / or development require the concurrent or prior adoption of a local plan, unless otherwise directed by the policies of this Plan or determined by the County not to be required.
- 22.1.2 Applications for a Development Permit in an area where a land use has been approved prior to the adoption of this Plan do not require a local plan.
- 22.1.3 Local plans shall address and adhere to the requirements of the East Highway 1 Area Structure Plan. In support of local plans and redesignation applications, the developer will be required to submit a rationale showing how their proposal is consistent with the vision and policies of the East Highway 1 Area Structure Plan.
- 22.1.4 Subdivision and development applications shall address and adhere to the requirements of the local plan and the policies of the East Highway 1 Area Structure Plan.
- 22.1.5 Where a local plan does not exist or is silent on a subject, the policies of the East Highway 1 Area Structure Plan shall prevail.

#### *Local Plan - Boundaries*

The boundaries of local plans should be based on the natural and physical conditions in the Plan area, as well as other factors such as the availability of servicing, parcel layout, and proposed transportation improvements. Potential Local Plan areas are outlined in **Map 15: Local Plan Areas**. Each Local Plan area could consist of one of three districts.

### *Local Plan - Requirements*

- 22.1.6 A local plan shall be required to support applications for subdivision and development in accordance with County Policy. The local plan should:
- a. provide detailed planning and design policies and guidelines;
  - b. address the County's Commercial, Office, and Industrial Design Guidelines and document how the local plan meets those guidelines;
  - c. provide architectural and site guidelines in order to provide a consistent, thematic design to the commercial area;
  - d. where applicable, coordinate with Wheatland County to ensure effective transition across municipal boundaries;
  - e. where necessary, provide for current and future access requirements to the transportation network; and
  - f. where necessary, ensure vehicle and pedestrian connections are in general accordance with other local plan areas, and in accordance with this Plan.
  - g. incorporate policies that provide for green building techniques and energy efficient design.
- 22.1.7 All planning or development applications, and any associated infrastructure construction, should meet the technical requirements of the County Plan, County Land Use Bylaw, East Highway 1 Area Structure Plan and associated technical studies, relevant local plan, County Servicing Standards, County Policy, and provincial and federal requirements.

### *Local Plan - Reserve Analysis*

- 22.1.8 A reserve analysis shall be required with the preparation of a local plan to determine the amount, type, and use of reserves owing within the local plan area.
- 22.1.9 The reserve analysis shall include a determination of:
- a. the total gross area of the local plan;
  - b. the type and use of reserves to be provided within the local plan area;
  - c. other reserves owing on an ownership basis;
  - d. the location of the reserve types and amounts in relation to the local plan area's overall open space system, with this information to be shown on a map; and
  - e. the amount of residual reserves to be taken as money in place of land.

### *Local Plan - Infrastructure Costs and Levies*

The East Highway 1 ASP recognizes development implementation will require infrastructure improvements within and external to the Plan area. The costs incurred by development of lands will be covered through a variety of revenue sources including developer improvements, development levies, County improvements, and user fees.

The need, cost, and timing of infrastructure vary with the type of infrastructure improvement. Offsite Levies for transportation, water, wastewater, and stormwater servicing will be developed for the East Highway 1 Area Structure Plan. All levies are subject to periodic review and include development costs associated with internal and external improvements to service the Plan area. Non-levy costs and improvements will be determined through periodic review of the master servicing documents and at the local plan preparation stage.

It is important to note that infrastructure costs do not represent the full costs to service the Plan Area. Complete community costs also include costs associated with program and service delivery to business owners (e.g. community recreation, fire and property protection, parks maintenance, waste and recycling operations, etc.), which serve community needs and are an essential part of a community.

**22.1.10 Full piped servicing shall be installed by the developer to the Plan Area following approval of the first Local Plan that includes lands within phase 3 (Central District of the ASP) or if the intermunicipal regional servicing needs full piped services whichever comes first. Interim servicing solutions in compliance with this ASP will be required prior to the completion of full piped servicing to the Plan Area.**

22.1.11 As part of the local plan approval process the identification, timing, and funding of any required off-site improvements is required. Off-site improvements that are:

- a. internal to the Plan area will be determined to the satisfaction of the County; or
- b. external to the Plan area, including provincial or adjacent municipal infrastructure will be determined to the satisfaction of the County, in consultation with the relevant municipality and / or provincial department.

22.1.12 Developers relying on regional County utility services (water, wastewater, and / or stormwater) shall be required to front-end the costs of utility service upgrades where deemed necessary by the County.

22.1.13 Costs associated with transportation and / or utility service

improvements are the developer's responsibility.

22.1.14 Developers relying on transportation and / or utility infrastructure improvements (water, wastewater, and / or stormwater) provided by other developments shall be required to pay cost recovery as per the requirements of the applicable cost contribution agreement.

22.1.15 Development proponents shall be required to pay the Rocky View County:

- a. Water and Wastewater Off-Site Levy;
- b. Stormwater Off-Site Levy; and
- c. Transportation Off-Site Levy.

## 22.2 PHASING

The purpose of the phasing strategy is to provide for the logical and cost-effective progression of development. Development phases shall align with infrastructure investment as it builds out in the Plan Area and will be subject to market forces. See **Map 16: Phasing** for proposed site development phases.

22.2.1 Phasing of development in the East Highway 1 Area Structure Plan area should be done in a logical and cost effective manner and shall be guided by the creation of local plans.

22.2.2 Local plans shall specify further details on phasing of development and infrastructure.

## 22.3 MONITORING

The progress in implementing the East Highway 1 Area Structure Plan will be monitored. Where necessary, County Administration will make recommendations as to how to manage growth in the Plan Area or how the Plan may be updated to meet changing circumstances.

- 22.3.1 County Administration will report to Council on implementation of the East Highway 1 Area Structure Plan as part of Administration's yearly reporting on the overall implementation of the County Plan.

## 22.4 PLAN REVIEW AND AMENDMENT

The future land use and development outlined in the East Highway 1 Area Structure Plan is intended to address a 20-30 year build-out of the area. While the Area Structure Plan is sufficiently flexible to account for change, periodic review, and occasional amendment of the Area Structure Plan may be required.

Under normal circumstances, the County will undertake a Plan assessment every 10 years to determine if a full review is required, as per the County Plan. However, if the rate and extent of development were to change dramatically, the County may initiate a review earlier than 10 years.

- 22.4.1 The County may consider periodic review and occasional amendment of the East Highway 1 Area Structure Plan in accordance with the County Plan, County Policy, and the Municipal Government Act.

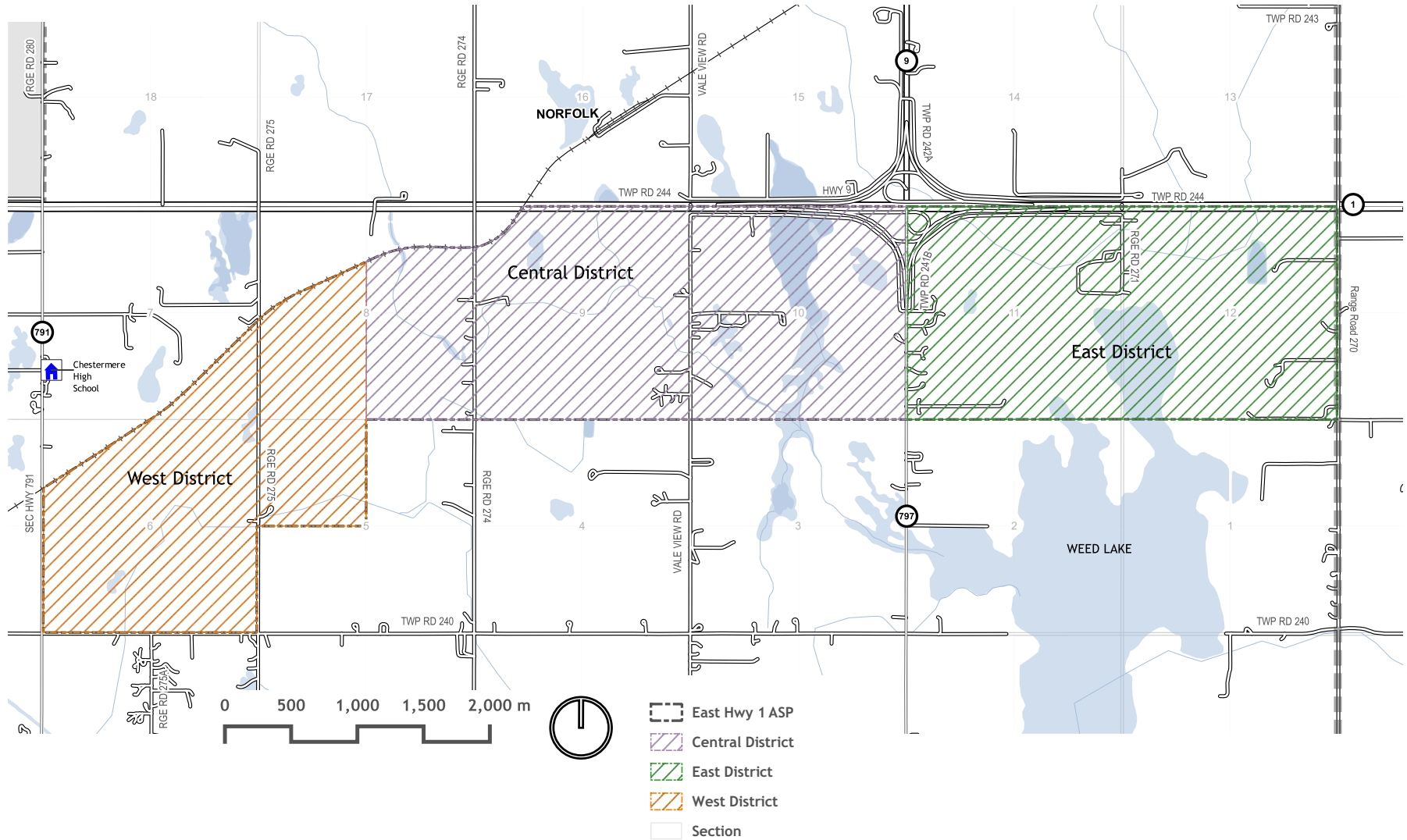
- 22.4.2 The East Highway 1 Area Structure Plan shall be subject to an assessment and possible review every 10 years.

### *Actions*

Actions are activities that need to be carried out by the County to achieve the goals, objectives, and policies of the Plan. The following are the recommended County actions to assist in the implementation of the East Highway 1 Area Structure Plan.

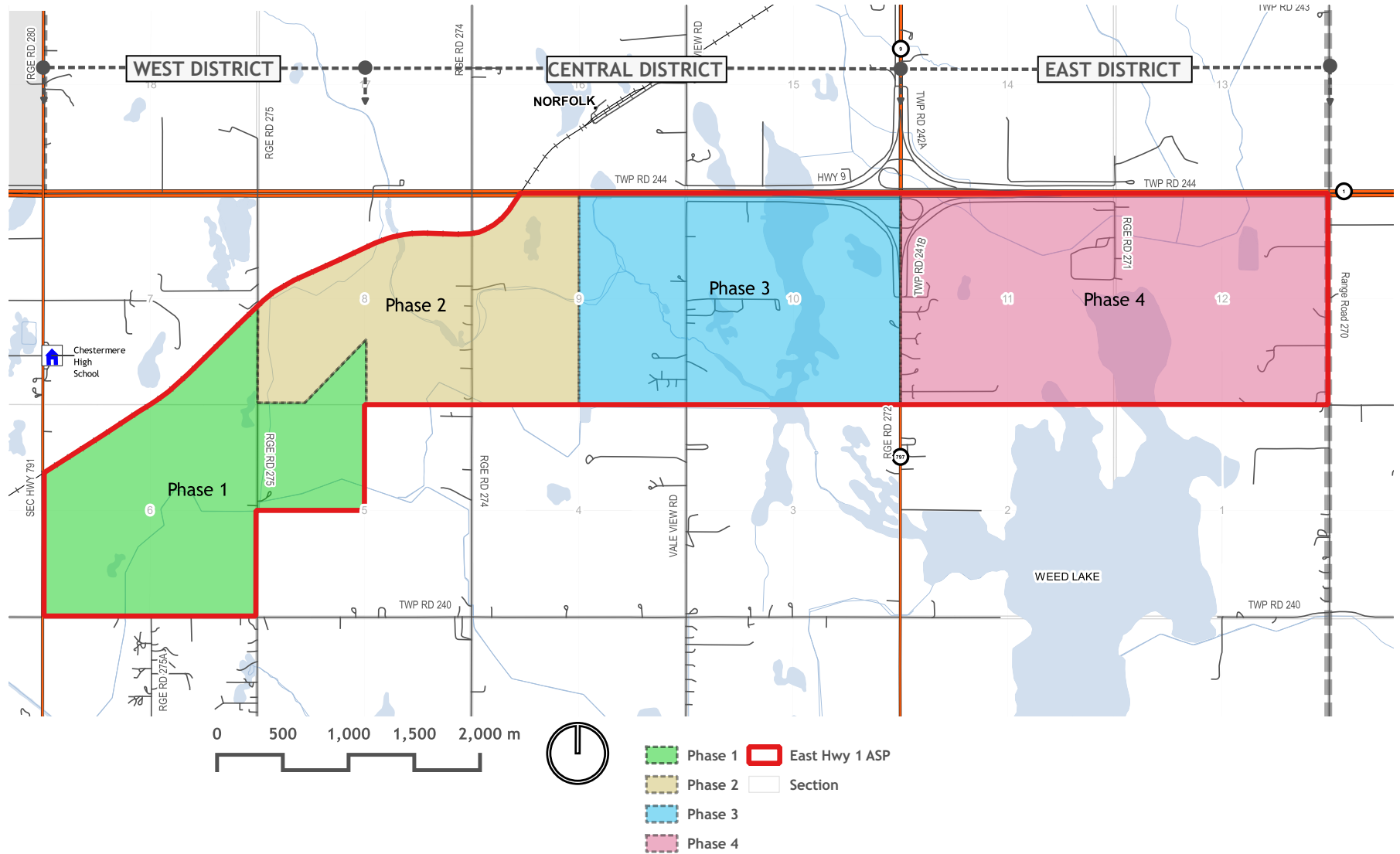
- 22.4.3 Monitor and report on the Plan implementation as part of the yearly County Plan reporting.

MAP 15 | LOCAL PLAN AREAS





MAP 16 | PHASING



# 23.0

## INTERMUNICIPAL COORDINATION AND COOPERATION

The Plan area is bordered by the Wheatland County to the east, connects directly to the City of Calgary via Highway 1, and is in close proximity to the **Town City** of Chestermere. The Plan acknowledges the land use intent of the County and provides for appropriate, compatible land use transitions at the interface areas. In addition, the Plan contains specific stormwater (Section 18), transportation (Section 16), open space (Section 13), business interface (Section 11), and intermunicipal (Section 23) policies that promote a coordinated and cooperative approach to planning. The County has initiated the process of developing an Intermunicipal Development Plan with Wheatland County.

### OBJECTIVES

- Ensure ongoing, meaningful consultation occurs between Rocky View County and surrounding areas on matters related to the implementation of the East Highway 1 Area Structure Plan.

### 23.1 LOCAL PLANS, REDESIGNATION, AND SUBDIVISION

Local plans are to be developed within the framework provided by this Area Structure Plan. In addition to the below policies, the standard technical requirements of a conceptual scheme or master site development plan are identified in the County Plan.

- 23.1.1 Rocky View County shall ensure that local plans and applications for redesignation and subdivision of lands in areas adjacent to Wheatland County address:
- a. regional drainage to ensure the protection of required drainage corridors;
  - b. alignment and connectivity of pathways, roadways, and utilities with the adjacent municipality;
  - c. land use compatibility with adjacent municipal land uses; and
  - d. other appropriate policies of this Plan.



# 24.0

## AGRICULTURAL INDUSTRIAL DESIGN GUIDELINES

The purpose of these design guidelines is to offer further direction for site development within the East Highway 1 area. These design guidelines will also assist in the development of a comprehensive site improvements strategy including the roads trail system connecting within and outside the site.

These design guidelines can be used by applicants, staff and Council to determine the general and specific merits of applications. Other more specific guidance through the Land Use Districts and regulations will have priority over this guide if there is any conflict.

### 24.1 GENERAL GUIDELINES

The objective is to integrate practical “green” and environmentally sensitive features into site and building design that enhance real estate value and reflect the rural agricultural character of the area. These features should be considered in design review and encouraged in implementation:

#### Site Development Strategies:

- Minimize construction disturbance by protecting areas (tree retention areas) with fencing and retaining vegetation shelter belts along property lines to retain the farmland character;
- Specify light coloured, high reflective roof materials to minimize the “heat island” effect.

#### Stormwater:

- Use permeable surfaces in the pedestrian walkway areas and parking areas to fit with the rural character;
- Use grassed swales and ditches for natural storm water management;
- Require storm management detention on site as appropriate in the specific areas outlined in this Area Structure Plan.

#### Water:

- Choose drought resistant native plantings for landscaping to reduce water use;
- Mulch planting beds to a depth of 50 mm to reduce water loss;
- Use recycled water/rainwater for irrigation (e.g., rain barrel program);
- Use native prairie grasses within the landscaped areas;
- Incorporate water use reduction features in buildings and on-site (e.g., include low-flow fixtures etc.).

#### Energy:

- Improve energy efficiency through design and building orientation (e.g., Minimum building envelope energy standard and south building orientation etc.);
- Include energy efficient fixtures in buildings;
- Use shade trees to shade buildings during summer months and reduce solar heat gain;

- Utilize programmable thermostats in individual living or commercial units;
- Use energy efficient lighting for internal and external lighting (e.g., LED);
- Use renewable energy features such as solar and geothermal energy.

**Construction Material and Waste Reduction:**

- Re-use existing building materials where possible;
- Use construction materials with recycled content where possible;
- Use construction materials to create variety and interest;
- Avoid using highly reflective materials especially towards traffic areas, sun-shade analysis could help choose appropriate materials;
- Retail top soil on site and balance cut and fill in grading;
- Ensure construction waste is recycled where feasible.

**Healthy Buildings, Healthy Landscapes and Practices:**

- Improve air quality by using materials that produce fewer off-gases for such elements as flooring and paint;
- Design windows so they can be opened for fresh air circulation;
- Install clarifiers or water/oil separators on each drain;
- Provide landscaping that includes wildlife habitat;
- Provide flexibility in design and universal accessibility of units so that occupancy can change over time (e.g., different industrial and business use flexibility).

## 24.2 INDUSTRIAL SITE PLANNING

### Design Intent

The intent is to create a light to medium industrial community connected by trails and easily accessible from the region. Attractive building proportions and materials will provide an inviting and valued place of business in the region.

Existing landscape features such as shelterbelts (vegetation screens and wind breaks) as well as unique rolling landforms should be retained and reinforced where possible to continue the legacy of the agrarian landscape. The rural road framework will be enhanced and improved by further roads and associated landscape improvements with the addition of each business or industrial frontage.

### Primary Gateways

Primary Gateways provide the entry points to the development area. These gateways should include Highway 791 at the western edge of the area. In addition, a primary gateway will be on Highway 797 in the central area.

- Monument signs, a maximum 1.5 meters high, should be used in accordance with the Sign By-law and in combination with earth berms and planting to appropriately announce the primary accesses.
- A signature “logo” should be developed for signage and area information that reflects the East Highway 1 future vision as one of the premier industrial centres.
- Direct lighting could be considered to complement the entrance without creating glare or other unnecessary impacts on adjoining uses.

### Secondary Gateways

Secondary gateways are those entrances distinct to the character areas that may include Vale View Road as well as Range Road 275 and Boundary Road on the east edge of the ASP area. Monument signage should be provided in accordance with local regulations, secondary to the primary gateway signs but still visible to drivers.

- Any signage should be integrated with appropriate trees and accent landscaping that flows with adjoining landscaping.
- Unobtrusive lighting should be provided that features the sign and creates no external glare.

### Streetscape Provisions

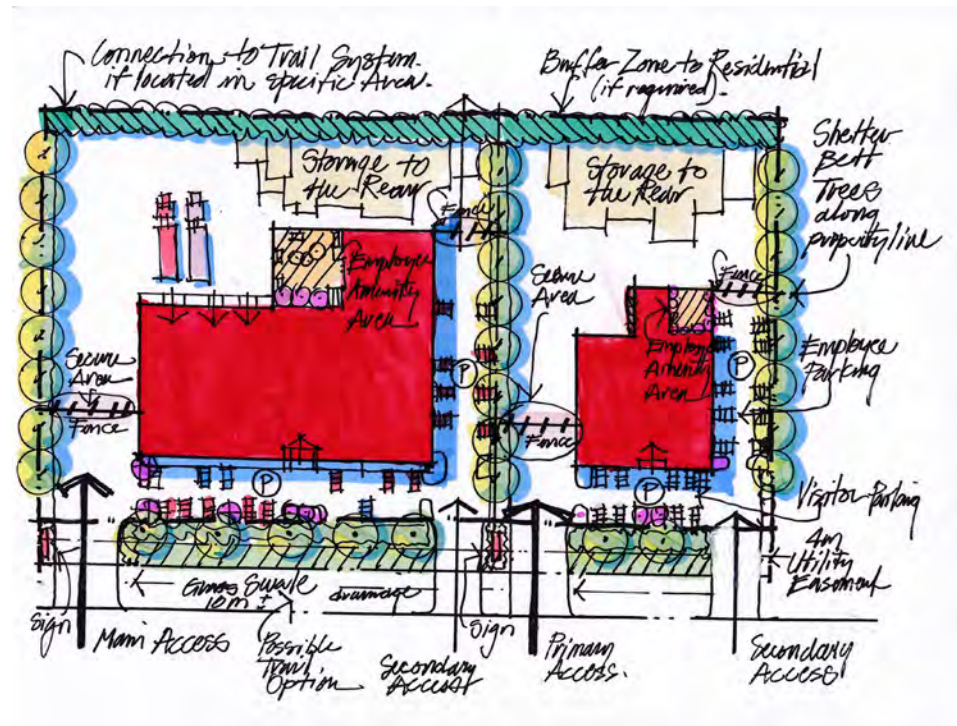
- A continuous 4.0 to 8.0 metre landscape strip should be provided along the property line with at least 20% covered by mass planting of hardy shrubs and native prairie grasses, especially at the entries and signage areas.
- The landscape strip should also have coniferous and deciduous tree planting in groups to further emphasize accent areas but not obstruct vision and access (with a 6cm caliper minimum size.) At minimum, a single row of trees spaced 10 meters apart is suggested within the landscape strip, along with low shrubs to screen temporary parking and drop-off areas.
- Lower shrubs and other accent planting as well as earth mounds (berms) should be used to enhance the green border and highlight entries, monument entry signs (ground mounted signs) and features on each site.
- Street lighting, where applicable, should be provided to engineering standards.
- Appropriate secondary direct and indirect landscape lighting and building lighting should be provided and not spill over onto adjacent sites or generate glare.



Potential streetscape (boulevard) character with tree planting and trails along generous landscape strips.

**Potential Industrial Streetscape Character**

- Opportunities for public art and historical references should be encouraged where appropriate, excepting major entrances where it conflicts with other informational signage.
- Crime Prevention Through Environmental Design (CPTED) principles should be followed to increase safety and security in and around buildings, especially during evening hours. Additional accent lighting for walkways and parking areas should be provided to increase visibility. Visibility can also be improved by low shrubs (maximum 0.6 meters high) and high canopied trees (2.4-meter minimum canopy height). Security fencing should be limited to the rear of the property.
- Opportunities for pedestrian interest along the site's frontage should be provided; landscape design should provide opportunities to sit and view or take part in walking or active recreation.
- Where applicable, boulevards should be planted with indigenous, drought tolerant planting, including street trees and shrubs, keeping in mind clear sight distances at intersections.



The majority of parking should be located to the side or rear of the building

**Vehicular Access, Off-Street Parking and Loading Areas**

- Limited parking should be permitted between street front and building. A drop-off lane could be permitted with temporary drop-off parallel parking within the front yard area provided a landscape strip is included.
- The balance of surface parking facilities should be located to the side or rear of the building.

**Landscaping and Screening of Parking Facilities**

- A layered landscape treatment should be provided to screen parking and loading areas while providing strategic visual access to signs, entries and access areas.

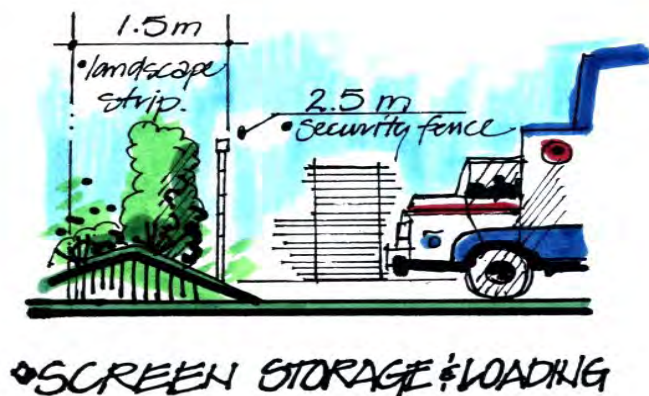
- Safety and security are important factors in the layout, size and characteristics of plant material and earth-mounding that effect visual access throughout the site.

**Buffer Zones**

- A minimum 10.0 m buffer zone should be provided to adjoining residential areas. No parking or storage are permitted in this area.
- The buffer zones should include at least a 2.0m high black or green wrapped chain link fence with a planted coniferous tree buffer and earth-mounding to conceal the fenced area.

### Loading and Outdoor Storage Areas

- Loading areas should be located to the rear (first preference) or sides (second preference) of the property and not easily visible from the adjoining street.
- Loading areas should be screened from view from the major arterial roads, highways and collector roads using layered landscaping and mounding, with the use of fences as a backdrop design element only.
- Screening should include fencing, plantings, and earth mounds that filter undesirable views.
- Security fences up to 2.5 meters high for storage and loading areas should be limited to green or black covered chain link fences or equivalents that are accompanied by appropriate hedging or other plant material in a minimum 1.5-meter landscape strip. No storage materials exceeding 2.5 meters high should be placed within 5 meters of the fence.



## 24.3 BUILDING ARCHITECTURAL CHARACTERISTICS

### Heights and Setbacks

- Buildings should generally be 12 to 15 m high considering additional floor to ceiling requirements for industrial uses;
- Buildings along roads could require a minimum setback equal to the height of the building but not less than 7.5 meters including the 4.0-meter utility setback. Consideration for views with appropriate setback and building step-back provisions.

### Building Width and Depth

- Neither the width nor depth of an individual building should exceed 80 meters.
- Additional width or depth may be considered where the proposal demonstrates visual interest and diversity.
- Where the need for longer, wider buildings can be demonstrated, design should include façade articulation, colour, and material variations.



*Buildings are generally two to three stories high*





Long buildings should be divided into segments for interest and visual quality



▷ BUILDING MASSING & FORM

**Massing and Form**

- On larger sites, higher building massing should step back from the street edge.
- Larger sites and larger buildings can accommodate a variety of building forms and massing options.
- Generic “Big Box” building designs that exhibit little façade interest and transparency to the street should be avoided.

**Main Entries to Streets**

- Main building entries should be clearly identifiable, visible, transparent, and accessible from the street.
- Pedestrian interest and comfort at entries should be provided through specifically designed seating, signage, lighting, and features that signal the building’s use.



Stepping of building can reduce building mass

### Building Articulation

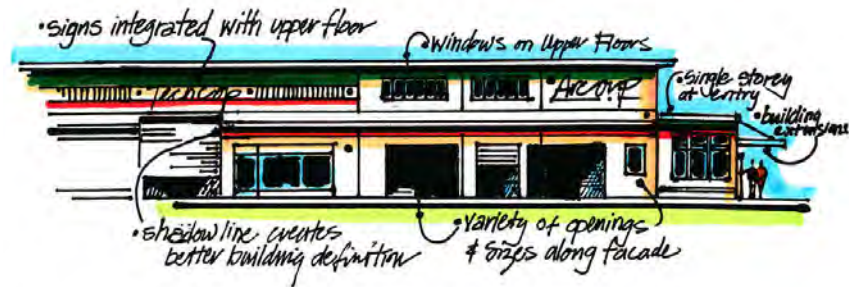
- Building articulation can be achieved utilizing glazing, canopy, and shading systems, as well as exposed structural components.
- Feature banding to break up perceived wall height might be used to assist in achieving horizontal articulation.
- Highly visible circulation and building systems are encouraged.
- Vertical service elements, such as stair wells, that are located to the perimeter of the building, may be used to assist in articulation, as well as express their function.
- Rooftop mechanical systems, and other appurtenances should be integrated into the form of the building or screened from view.

### Weather Protection

- Main building entries should provide generous weather protection that is designed to be an integral feature of the building's architectural character.
- Larger sites that are developed with more than one building should provide a weather protected walkway system to connect building entries within the site, and coordinated with adjacent sites if possible.
- Canopy and/or awning systems detailing should consider integrated signage, lighting, and display systems.
- Canopy and awning systems depth should be maximized to provide greater weather protection, as well as reduce the scale impact of larger buildings.
- Weather protection elements on overhangs may be considered in required yards and landscaped setbacks.
- Ground-oriented pedestrian "streets" through large footprint buildings are encouraged to create connections to on-site circulation routes and amenities. Such circulation through buildings should be clearly identified and designed for use by the public.

### Exterior Material and Color

- Lower shrubs and other accent planting should be used to enhance the green border and highlight entries and features on each site.
- Exterior building design should reflect the industrial character by utilizing appropriate, durable materials.
- Exterior materials that are encouraged are contemporary metal cladding systems; glass and steel; and architectural concrete.
- Vinyl and corrugated metal are discouraged as primary exterior materials but can be used as accent features on buildings. Earth tone colours such as greens and browns are encouraged as well as variations on bolder, primary colors to provide accent only.



### ◆ BREAK UP THE BOX - Building Articulation



### ◆ WEATHER PROTECTION & PEDESTRIAN SCALE

### Signage and Lighting

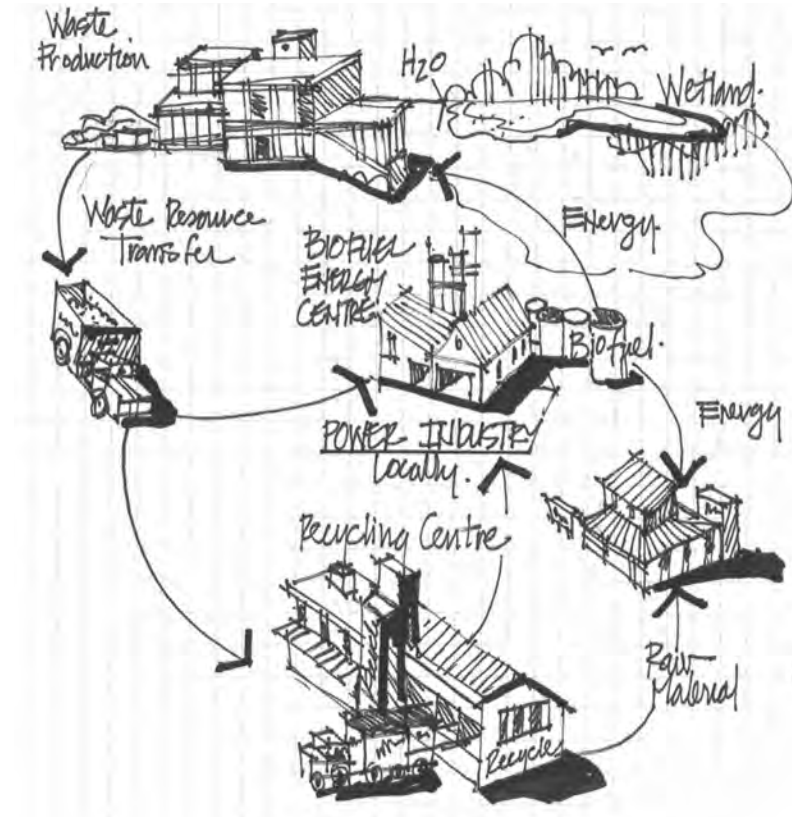
- Street and entrance lighting should be integrated into site design.
- Building and landscape lighting should be included with landscape features.
- Corporate signage should be subordinate to the design of the building.

### Eco-Industrial Recycling Energy and Waste Concept Potential

- Consideration should be given to a central eco-industrial centre that could advance recycling, materials exchange, shared emergency response, environmentally sensitive infrastructure and conservation.

### Winter Design Guidelines

- Consideration should be given to snow removal and storage making spaces that are accessible year-round.
- Buildings should be articulated to minimize shadowing and wind on pedestrian environments.
- Front canopies could protect sidewalk environments from snowfall accumulation during winter months while providing shade in the summer months.



Consider Eco-Industrial Hub for Recycling and other Sharing

## 24.4 COMMERCIAL AREAS

### Design Intent

The commercial areas will support the East Highway 1 districts as three complete rural communities. The design intent is to create two commercial nodes that service the West and Central Districts. It is important to maintain and enhance driver access and safety in and out of these commercial areas. Buildings should be generally one to two stories in these commercial areas and include a variety of commercial retail and office uses that support the agricultural businesses in the area. The area should complement the design of the surrounding industrial uses.

### Specific Design Guidelines

- Buildings should generally be one to two stories to maintain the existing character;
- Parking should be generally located in parking areas or 'courts' that are landscaped in front or to the side of the buildings;
- Incorporate tree planting to break up the parking into parking pods;
- Incorporate special features and pedestrian connections in the parking lots to make it safer and more pedestrian oriented;
- Signage should be limited to a specific signage placement zone at the property line and above the businesses in accordance with the sign bylaw;
- Landscaping should also be required to a similar standard as the industrial and business zones within a 4.0 to 8.0-meter landscape strip (including the 4.0-meter utility easement). To add to the agrarian theme, a low 1 m wood single rail wood fence (natural wood finish) is also encouraged along the property line as a standard detail and coordinated along the street.



*Local-Scaled Rural Commercial Development to support the Industrial Uses*

## 24.5 PARKS & ENVIRONMENTALLY SENSITIVE AREAS

### Design Intent

The overall intent is to enhance the environmental assets on the site and maintain the rural quality of the landscape. These guidelines are most applicable to the East Highway 1 significant wetlands and any other environmentally sensitive areas. At the same time, existing significant tree stands and related habitat in other parts of the site should be retained on site if possible and developed as part of a natural areas and trail network.

### Specific Design Guidelines

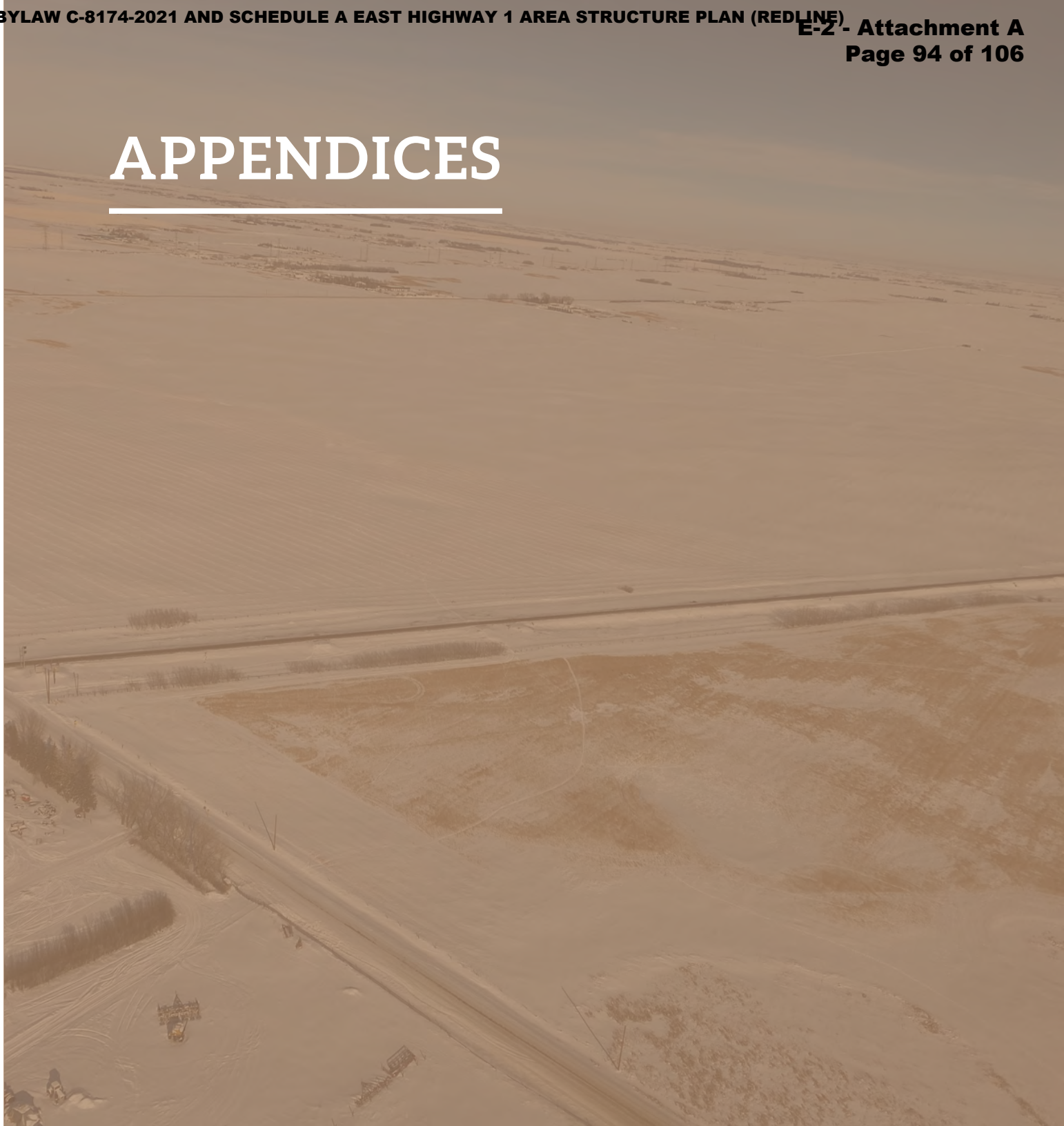
Wherever possible, development projects should conserve significant wetlands;

- Significant tree groupings should be retained if possible, following an assessment by a Certified Arborist;
- Applications affecting environmentally sensitive areas should be reviewed in consultation with Province of Alberta ministries (where applicable);
- Setbacks from wetlands watercourses should be determined in accordance with the Provincial riparian area requirements;
- Habitat protection boundaries should be formalized through Restrictive Covenants;



# APPENDICES

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# APPENDIX A | DEFINITIONS

**Conceptual Scheme** is a non-statutory plan, subordinate to an area structure plan. It may be adopted either by bylaw or by a resolution of Council. A conceptual scheme is prepared for a smaller area within an area structure plan boundary and must conform to the policies of the area structure plan. Conceptual schemes provide detailed land use direction, subdivision design, and development guidance to Council, Administration, and the public. If a conceptual scheme area is of sufficient size that further detail is required for specific areas and phases, the conceptual scheme may identify smaller sub-areas and provide detailed guidance at that level. These smaller sub-areas are referred to as 'development cells'.

**Local plan** is a term that refers to a conceptual scheme or master site development plan. A local plan will have unique planning requirements based on the planning direction provided in the area structure plan. Local plans must also address the general requirements for preparing a conceptual scheme or master site development plan identified in the County Plan.

**Master Site Development Plan** is a non-statutory plan that is adopted by Council resolution. A master site development plan accompanies a land use redesignation application and provides design guidance for the development of a large area of land with little anticipated subdivision. A master site development plan addresses building placement, landscaping, lighting, parking, and architectural treatment. The plan emphasis is on site design with the intent to provide Council and the public with a clear idea of the final appearance of the development.

**Open land** means publicly or privately owned land within a comprehensively designed Compact Country Residential neighbourhood or larger community, where the land is used for the primary purpose of conservation, recreation, or agriculture.

**Open space** means all land and water areas, either publicly owned or offering public access that are not covered by structures. Open space may include current and future parks, environmentally significant areas and other natural areas, pathways and trails, greenways, parks, land for schools and recreation facilities, utility corridors, golf courses, and cemeteries.

**Developable land** means privately owned land that has no natural or human caused constraints to residential development. Constraints to development include land determined to be unstable, hazardous, environmental reserve, contaminated, or regulatory setbacks as identified by the Provincial or Federal governments.

# APPENDIX B | PUBLIC OPEN HOUSE SUMMARY

**NEW**

## Public Engagement Summary

DATE	ENGAGEMENT
July 14, 2020	Terms of Reference Approved by Council Project Launch
January 2021	ASP project webpage launched
January 2021	Background Report posted on project webpage
April 12, 2021	Draft ASP posted on project webpage
April 22, 2021	Letter to neighbouring landowners sent with invite for May 6, 2021 open house
May 6, 2021	<b>Virtual Open House (42 attendees)</b>
May 28, 2021	Comprehensive Q&A posted to website and provided to Open House attendees
April - July 2021	One-on-one meetings with landowners
July 27, 2021	<b>Public Hearing</b>

A virtual open house was held on Thursday May 6, 2021 from 7:00pm - 8:30pm MST. There was a total of 42 attendees, including Rocky View County staff, developer representatives and technical experts. A representative from the City of Chestermere Planning Department was also in attendance.

The Open House agenda included: introductions, presentation by the developer team led by Michael von Hausen, and a question and answer session.









# APPENDIX C | POLICY DIRECTION

The East Highway 1 ASP has been prepared within the context of higher-level statutory plans, regional plans, and County policy. The ASP area falls within the boundaries of the following statutory documents:

## **CALGARY METROPOLITAN GROWTH PLAN**

On January 1, 2018, Rocky View County and nine other municipalities became part of a regional planning area defined as the *Calgary Metropolitan Region*. The Calgary Metropolitan Region Board Regulation (190/2017) enacted under the Municipal Government Act (MGA), directs that a regional growth plan and a supporting regional servicing plan shall be prepared to guide how the Calgary Metropolitan Region will develop in the future.

The Metropolitan Region Growth and Servicing Plans are due to be adopted by June 1, 2021. The Plan will address the following:

- Growth Areas
- Development density
- Transportation, recreation, utility, and transit corridors
- Servicing require to support the Growth Plan
- Environmentally sensitive areas

## **INTERIM GROWTH PLAN (IGP)**

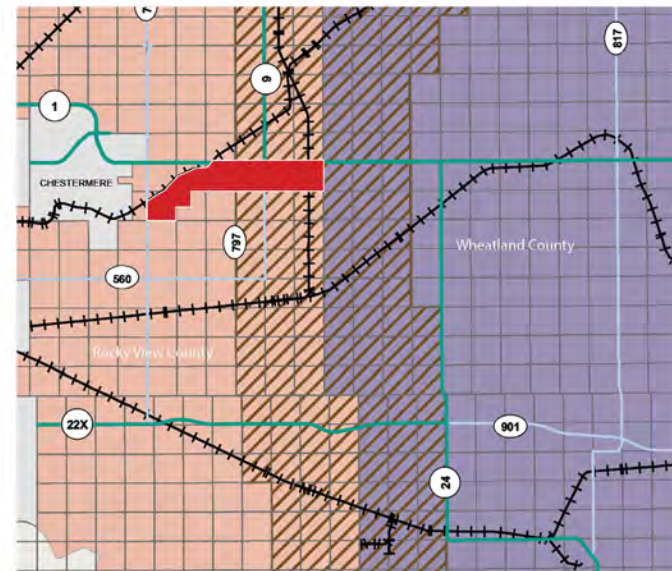
The interim Growth Plan is anticipated to be superseded by the Growth Plan, which is due to be adopted by June 1, 2021. The ASP requires approval from the CMRB, which is done through the Regional Evaluation Framework process. The IGP was approved by the Minister of Municipal Affairs on October 4, 2019. The IGP outlines three key goals:

1. Promote the integration and efficient use of Regional Infrastructure.
2. Protect water quality and promote water conservation.
3. Encourage efficient, strong, and sustainable growth.



The East Highway 1 ASP is consistent with the policies of the IGP, and supports the key goals.

## **ROCKY VIEW COUNTY-WHEATLAND COUNTY INTERMUNICIPAL DEVELOPMENT PLAN (IDP)**

The County is in the drafting phase of developing an Intermunicipal Development Plan (IDP) with Wheatland County. A portion of the East Highway 1 plan area is located within the *Rocky View County-Wheatland County IDP Study Area*.



Rocky View County / Wheatland County  
Intermunicipal Development Plan

 IDP Study Area  
 East Highway 1 ASP



**ROCKY VIEW COUNTY MUNICIPAL DEVELOPMENT PLAN (MDP)**

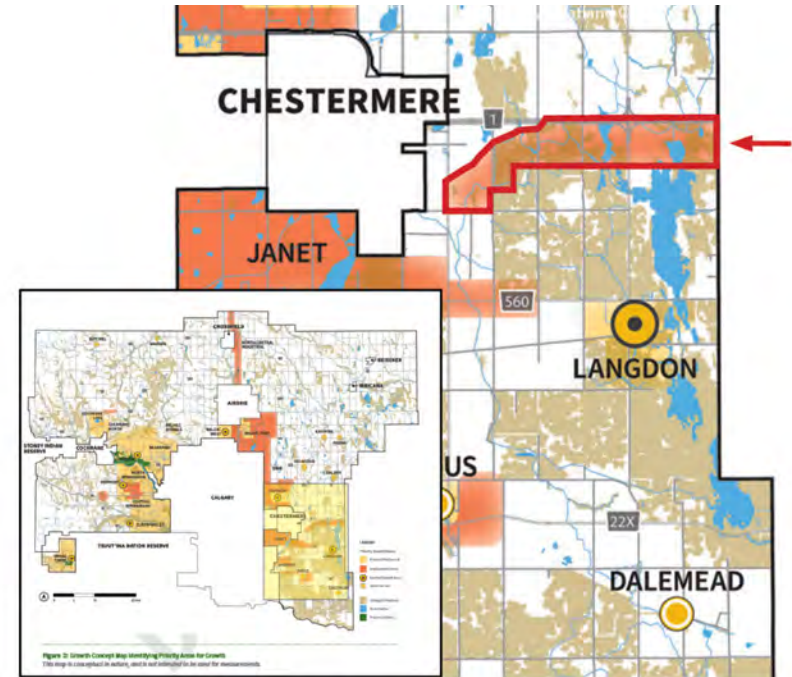
The County has a new draft MDP undergoing the final approval process. The draft MDP is guided by a Growth Concept Map that visually depicts the County’s future development areas, ecological features, hamlets, parks and water bodies. MDP Figure 2 identifies the Plan area as an Employment Area. It contains significant water bodies and ecological areas, however its Highway access and proximity to the ~~Town~~ City of Chestermere provides significant opportunities for employment relating to agricultural industrial and business uses.

**ROCKY VIEW - COUNTY PLAN**

The County Plan was adopted by council on December 2, 2014. The Master Plan was intended to guide development and servicing 10 years in the future. The Rocky View County MDP, once adopted, will supersede the County Plan as the primary long-term plan for the County.

**SOUTH SASKATCHEWAN REGIONAL PLAN**

The South Saskatchewan Regional Plan (SSRP) outlines strategic directions for the region over a ten year span of 2014-2024. The SSRP includes desired economic, environmental and social outcomes and objectives for the region. The South Saskatchewan region stretches from the southeast corner of Alberta northwest towards the M.D. of Bighorn.



Rocky View County - Draft Municipal Development Plan - Figure 2: Growth Concept Map



The MDP describes “Employment Areas” as:

“ These areas primarily contain commercial and industrial land uses and serve as major areas of employment in the County. Development will continue in existing growth areas, with new growth added in suitable locations to fulfill market demand. Most large scale industrial and commercial development will be directed to these areas.”



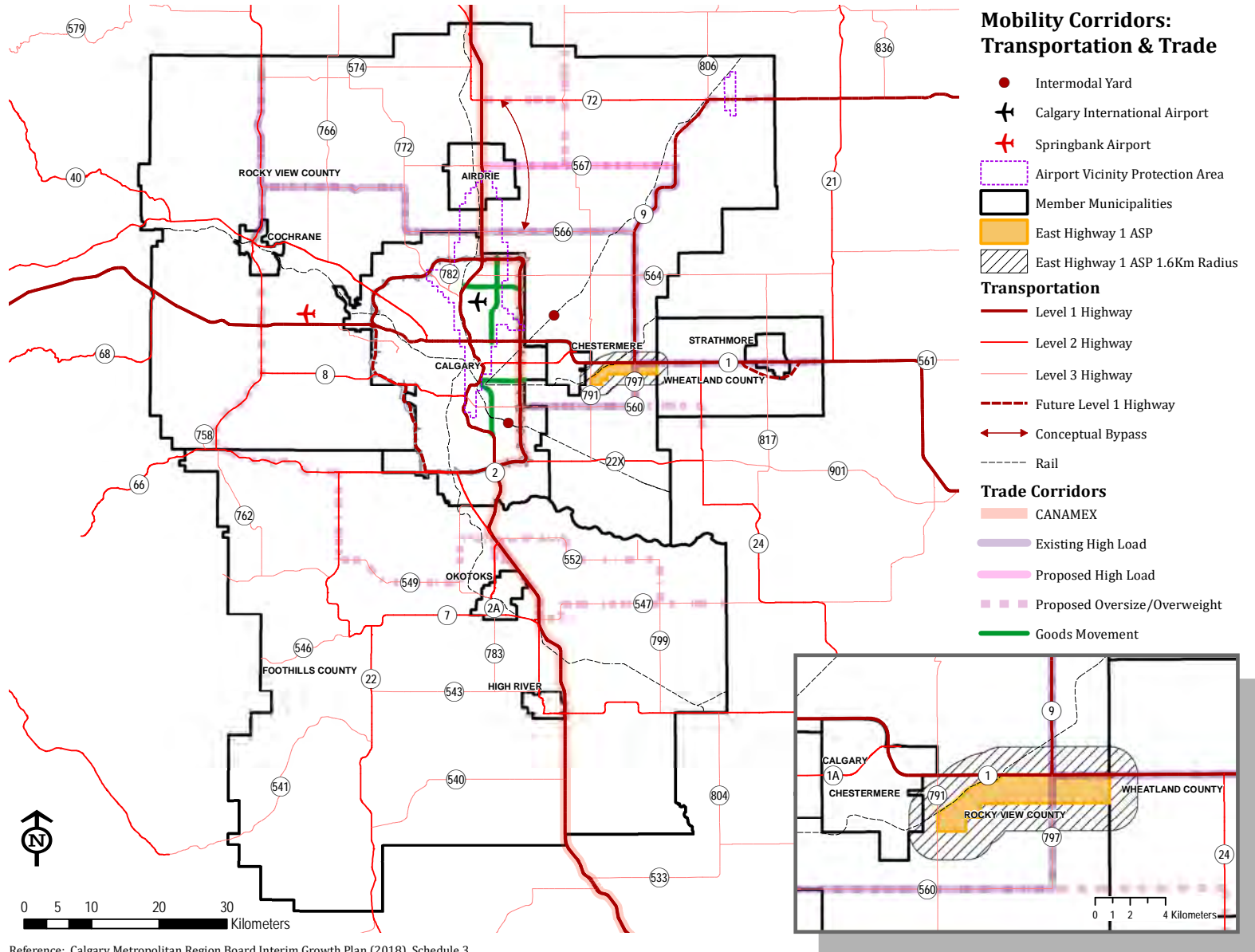
### COMMERCIAL AND INDUSTRIAL DESIGN GUIDELINES (ROCKY VIEW COUNTY)

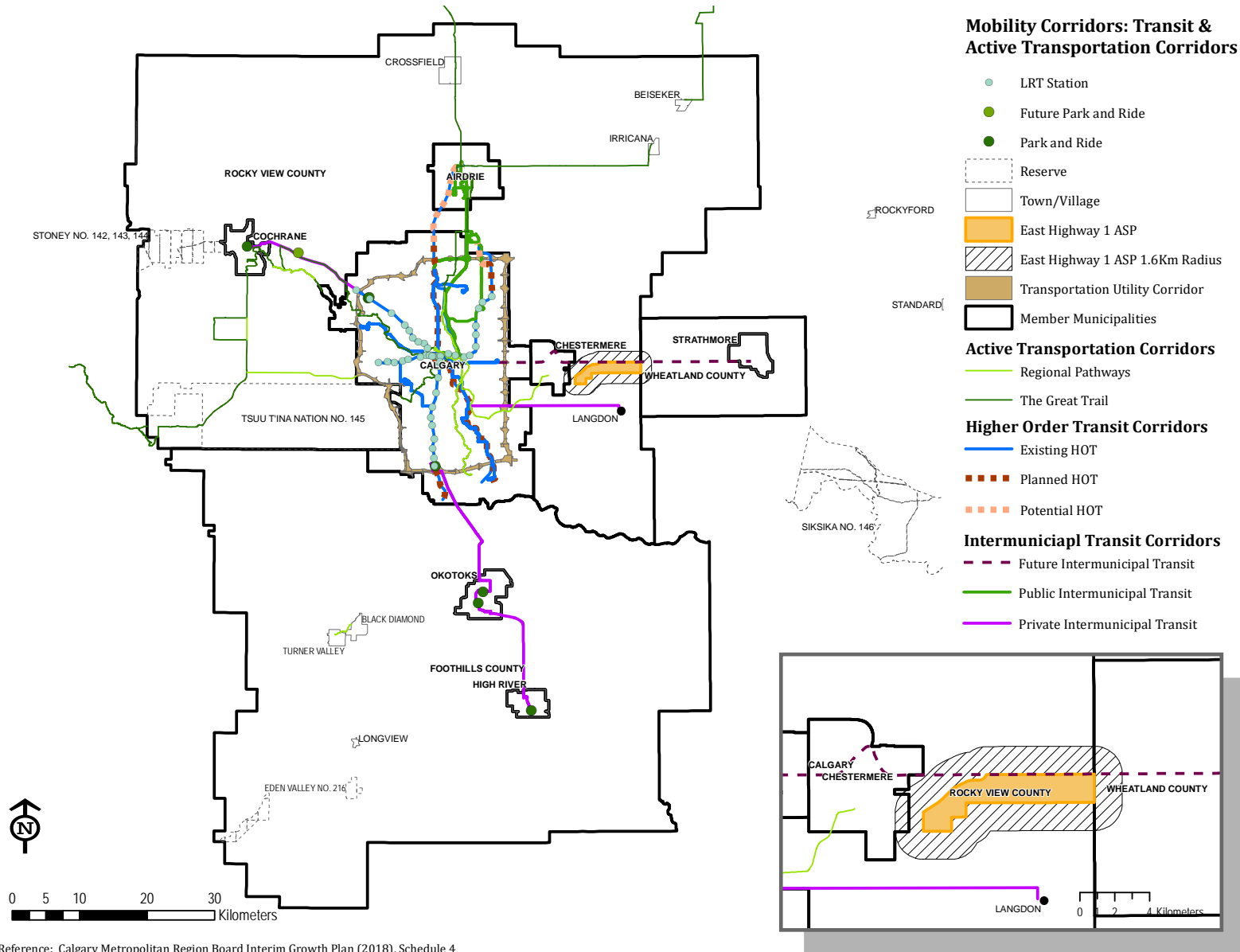


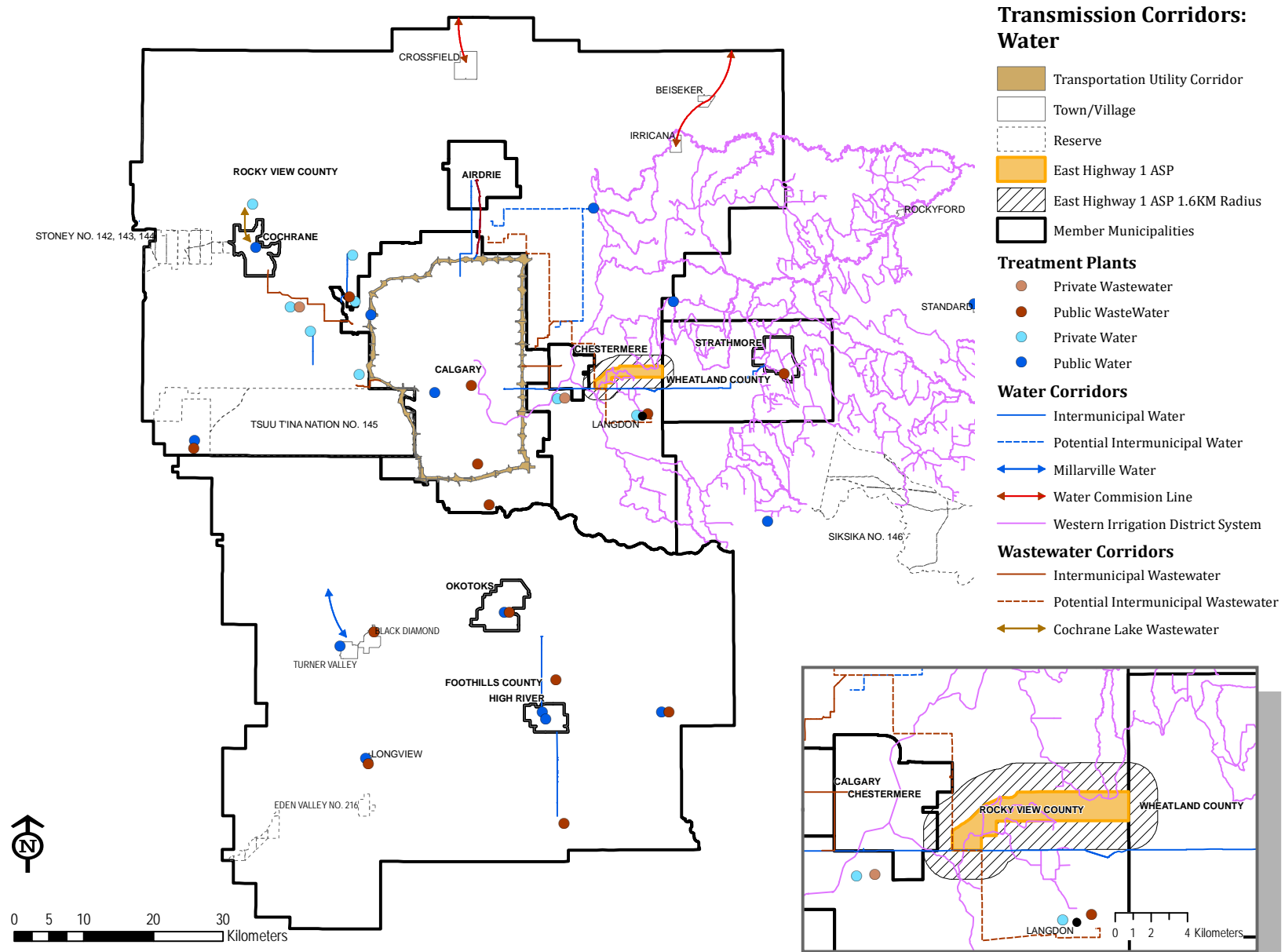
Rocky View County has developed *Commercial and Industrial Design Guidelines* to guide the quality of design for non-residential developments in the County. The Design Guidelines begin with outlining Design Principles followed by the Design Guidelines and specific guidelines for Special Design Areas. This ASP has been prepared to comply with these guidelines and subsequent development phases (local plan, subdivision and development) must also reference and comply with these guidelines to ensure aesthetically pleasing industrial design in the Plan area.

# APPENDIX D | REGIONAL CONTEXT MAPS

**NEW**







Reference: Calgary Metropolitan Region Board Interim Growth Plan (2018), Schedule 5



